

**PORT OF BROOKINGS HARBOR**  
**Regular Commission Meeting**  
**Wednesday, August 17, 2022 at 2:00pm**  
**Teleconference / Meeting Room**  
**16350 Lower Harbor Road Suite 202, Harbor OR, 97415**

**Teleconference Call-In Number: 1 (253) 215-8782**

**Meeting ID: 771 205 4017**

**Passcode: 76242022**

**(to mute/unmute: \* 6)**

**TENTATIVE AGENDA**

	<b>PAGE</b>
<b>1. CALL MEETING TO ORDER</b>	
• Pledge of Allegiance	
• Roll Call	
• Modifications, Additions, and Changes to the Agenda	
• Declaration of Potential Conflicts of Interest	
<b>2. APPROVAL OF AGENDA</b>	
<b>3. APPROVAL OF MEETING MINUTES</b>	
A. Approve Minutes of Regular Commissioner Meeting Wednesday July 20, 2022.....	2
<b>4. PUBLIC COMMENTS</b> – Limited to a maximum of three minutes per person. Comments by teleconference, please email your comments to <a href="mailto:danielle@portofbrookingsharbor.com">danielle@portofbrookingsharbor.com</a> prior to the meeting.	
<b>5. MANAGEMENT REPORTS / APPROVAL</b>	
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B. July 2022 Harbormaster Report.....	7
C. July 2022 Financial & Manager Report.....	10
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B. Southern Oregon Credit Service Request for Small Claims Approval.....	41
C. Boat Yard Warehouse Structural Analysis and Report Approval for Expense.....	42
D. New RV Park Fence Dividers.....	53
E. Permission to Display Bible Education Cart.....	56
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<b>7. INFORMATION ITEMS</b>	
A. Boat Yard – Dustless Blasting.....	144
B. Boat Shop & More Lease.....	160
C. USFWS Completes Sea Otter Study, Outlines Next Steps.....	176
<b>8. COMMISSIONER COMMENTS</b>	
<b>9. NEXT REGULAR MEETING DATE</b> – Wednesday September 21, 2022, at 2:00pm	
<b>10. ADJOURNMENT</b>	

A request for an interpreter for the hearing impaired, for those who want to participate but do not have access to a telephone, or for other accommodations for persons with disabilities should be made at least 48 hours in advance of the meeting to Port of Brookings Harbor Office at 541-469-2218.

**DRAFT MINUTES  
REGULAR MEETINGS OF THE BOARD OF COMMISSIONERS  
PORT OF BROOKINGS HARBOR DISTRICT**

**Wednesday, July 20, 2022**

*This is not an exact transcript. The audio of the session is available on the Port's website.*

The Port of Brookings Harbor District met in Regular session on the above date at 2:00pm. Open session at the Port Conference Room, 16350 Lower Harbor Road Suite 202, Harbor OR, 97415, teleconference and webinar.

**1. CALL MEETING TO ORDER**

Commission President Richard Heap called the Regular Meeting of the Port of Brookings Harbor of Commissioners to order at 2:00pm.

- **Commissioners Present:**  
Joseph Speir, Vice-President (Pos. #1); Sharon Hartung Secretary/Treasurer (Pos. #2); Larry Jonas (Pos. #3); Richard Heap, President (Pos. #4) and Kenneth Range (Pos. #5).
- **Management and Staff:**  
Gary Dehlinger, Port Manager; Travis Webster, Harbormaster; Danielle King, Safety/Administrative and Jack Akin, Port Engineer with EMC Engineering.
- There was no modifications or additions to the agenda.
- There was no declaration of potential conflicts of interest.

**2. APPROVAL OF AGENDA – Audio time 0:01:58**

- **A motion was made by Jonas and seconded by Speir to approve the agenda. The motion passed 5 – 0.**

**3. ELECTION OF COMMISSION OFFICERS – Audio time 0:02:15**

**A Motion was made by Jonas and seconded by Speir to approve Richard Heap to be President. A Motion was made by Range and seconded by Jonas to approve Joseph Speir to be Vice-President. A Motion was made by Jonas and seconded by Range to approve Sharon Hartung to be Secretary/Treasurer. The motions passed 5 – 0.**

**4. APPROVAL OF MEETING MINUTES – Audio time 0:03:46**

- A. Approve Minutes of Budget & Regular Commissioner Meeting Wednesday June 15, 2022.**
- B. Approve Minutes of Special Commissioner Meeting Tuesday, June 21, 2022.**

**A Motion was made by Speir and seconded by Hartung to approve the meeting minutes for the Budget and Regular Meeting, Wednesday, June 15, 2022 and Special Meeting, Tuesday, June 21, 2022. The motion passed 5 – 0.**

**5. PUBLIC COMMENTS – Audio time 0:04:56**

- There were four public comments. June McGraw emailed a public comment regarding the leashing dogs on Port Property and a dog attack that King read to the Board. Don Williams told the Board that his vessel was stolen from the basin and gave some suggestions regarding security and lighting. Gary Hartung mentioned a safety hazard regarding the anchors placed outside the boat yard warehouse. Al Cornell asked about the results of the water testing. The board had some questions for Mr. Williams regarding his vessel.

## 6. MANAGEMENT REPORTS

### A. June 2022 Safety & Security Report - Audio time 0:19:12

King reported on staff safety training, incidents, accidents, security issues that happened for the month of June and upcoming events happening at the Port.

### B. June 2022 Harbormaster Report - Audio time 0:21:10

Webster reported on projects that were completed in the month of June in the RV Park, marina, and equipment services. There was a discussion among the Board and staff regarding

### C. June 2022 Financial & Manager Report - Audio time 0:24:18

Dehlinger reported on the financials for the month of June. Dehlinger reviewed the Port items that happened in the month of June. There was a discussion among the Board and Staff regarding 1200Z permit. Jack Akin gave an update to the Board regarding the wastewater treatment plant, there was a discussion and questions from the Board.

**A motion was made by Speir and seconded by Hartung to approve the safety, security and environmental report, the harbormaster report, and the financial and manager report of June 2022 as discussed. The motion passed 5 – 0.**

## 7. ACTION ITEMS

### A. Boat Yard Building Plan – Audio time 0:45:52

Dehlinger reviewed with the Board what was discussed in the last meeting and what the Board had requested. Dehlinger stated that it is hard to get an accurate quote since the Board hasn't decided what kind of building we want. Dehlinger suggested to slow down to figure out what kind of building we want, ask an engineer to develop the drawings, then look for quotes, and ask for funding or seek out grants. Board allowed public comment. Range stated there is a third option and any building can be remodeled and would like to spend the money for an engineer to evaluate this structure as to what it would take to bring it in line aesthetically, structurally, and cosmetics of the exterior. Board discussion continued regarding the engineering report that was received by Jack Akin and his recommendations on proceeding forward with that building. Heap reviewed what the report had stated from what he remembers that the building needs to be brought back to code, basically you will be building a new building where the current one sits. Akin made it very clear to the Board that the structural engineer that created the report for the Port was not an associate of his at any time and this was the first time working with this person. His recommendations were not to demolish the building, his recommendations were if you wanted to keep the building that you have to make sure you are covering the Port for the safety infractions out there, it's a dangerous building. The building is failing at key stress points, you will spend a good deal of money trying to restore it, you will have to remove some material, and redo some construction errors made just to satisfy code.

**A motion was made by Range and seconded by Jonas to hire a structural engineer to look at our existing building and give us a cost, rough cost, what it would cost to structurally make it a viable building that meets code. The motion passed 3 – 2. Yes: Jonas, Range & Heap. No: Hartung & Speir.**

### B. Zola's on the Water Lease Amendment No. 2 – Audio time 1:16:53

Dehlinger reviewed the timeline and the actions from Zola's as to why the lease amendment is up for Board approval.

**A motion was made by Speir and seconded by Jonas to approve Zola's on the Water Commercial Lease Amendment No. 2. The motion passed 5 – 0.**

## 8. INFORMATION ITEMS

### A. Curry County Storm Drain Master Plan Draft April 2022 Review – Curry County Commissioners and Port Commissioners Meeting Date – Audio time 1:19:56

Dehlinger discussed scheduling a date and time with our Board to have a Board-to-Board meeting with Curry County Commissioners.

**B. Dog Leash Law Enforcement** – Audio Time 1:24:48

Jonas presented item and asked what has the Port done in the past regarding dogs, asked if staff could reach out to code enforcement to do a sweep of the Port. Webster went over how many signs we have, how many signs we have purchased, and what the Port has done regarding dogs.

**C. Small Debris Left Behind on the Jetty from Fireworks Show** – Audio Time 1:31:47

Jonas requested this item to be on the agenda but found out afterwards that the debris was picked up.

**D. Boat Ramp and Boat Parking Area** – Audio Time 1:32:41

Jonas stated that during the summer its busy and is the Port happy with how things are in the boat launch area? Board stated that they aren't sure what else we can do, we have signs, parking tickets, we monitor the area. Board allowed public comment. Board continued to discuss all the actions the Port has done to try and eliminate the amount of public parking in that area.

**9. COMMISSIONER COMMENTS** – Audio time 1:42:37

- No Commissioner comments.

**10. NEXT REGULAR MEETING DATE** – Wednesday, August 17, 2022, at 2:00pm.

**11. ADJOURNMENT** – Audio time 1:42:45

Having no further business, the meeting adjourned at 3:42 pm.

\_\_\_\_\_  
Richard Heap, President

\_\_\_\_\_  
Date Signed

\_\_\_\_\_  
Sharon Hartung, Secretary/Treasurer

\_\_\_\_\_  
Date Signed

*An audio recording was made of these proceedings. The recording and the full commission packet are available on the Ports website: [www.portofbrookingsharbor.com](http://www.portofbrookingsharbor.com).*

# SAFETY, SECURITY, AND ENVIRONMENTAL

## MONTHLY REPORT

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**Date:** August 17, 2022  
**Period:** July 2022  
**To:**  
**Issued By:** Danielle King, Safety, Security, & Environmental Coordinator

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### Safety

Port staff reviewed OSHA's new rules for wildfire smoke, the dangers of wildfire smoke, and how employees can protect themselves.

### Incidents

POBH recorded (5) incidents for the month of June bringing the year total to (57). Incidents included:

1. Vehicle was left on Port property; vehicle was removed by Wes's Towing.
2. Port damages from the July 4<sup>th</sup> accident has been submitted to our insurance company and Port legal was notified of the accident. Curry County Sheriff report is still under investigation.
3. Curry County Sheriffs contacted the Port for video footage of a dog encounter in the kite field parking lot.
4. A moorage holder went down to their boat in Basin I and their vessel was gone. Security cameras show an individual walk down to the basin, walk the docks (checking other vessels), get inside a vessel, and motored off in the boat to the end of the jetties. Motored back into the basin and tie up to the crab dock. Individual walked the jetty, walked around Zola's, and went back to the vessel on the crab dock, then motoring up the river. The boat was found later in the day under the bridge. Curry County Sheriffs asked for video footage and was able to identify the individual that stole the boat. This individual was trespassed from Port Property for 6 months.
5. An animal incident happened between two RV Park customers. One agreed to keep their distance for the remaining of their visit. Things continued to escalate over a few days, Animal Control was called to the scene but did not cite anyone since the case will be going to civil court.

### Security

Four Aces Security Solutions and POBH recorded (250) security issues for the month of July bringing the year total to (971). Issues included:

- (73) Overnight parking tickets.
- (126) Unauthorized visitors on Port Property after hours
- (27) Vehicles missing or unable to read boat launch ticket.
- (10) Parking violations in boat launch parking lot.
- (4) No camping
- (3) Unhitched trailer.
- (2) Emergency Vehicles to the RV Park
- (1) Non-operational vehicle
- (2) Vandalism
- (2) Parking violation throughout the Port

### Environmental / DEQ 1200-Z Industrial Stormwater

Monthly inspections were completed as required by our Stormwater Pollution Control Plan (SWPCP).

Discharge Monitoring Report (DMR) was submitted for the second quarter (April – June).

An oil sheen was found in Basin II. Port found the source of the sheen and was able to take care of the problem. The National Response Center and Oregon Emergency Response System was notified of the incident.

An oil sheen was discovered in Basin II, around N Dock. Port found the source of the sheen and was able to take care of the problem. The National Response Center and Oregon Emergency Response System was notified of the incident.

**Upcoming Events**

Pirates of the Pacific is August 11 – August 14 on the Boardwalk.

Art at the Port on the Boardwalk will be ending September 11.

South Coast Water Trails is September 17, meeting up at the Boat Launch Ramp.

# HARBORMASTER MONTHLY REPORT

**Date:** August 17, 2022  
**Period:** July 2022  
**To:** Gary Dehlinger, Port Manager  
**Issued By:** Travis Webster, Harbormaster

## RV Park

Staff stayed busy maintaining sites. Small repairs such as water spigots, cable ends and 30 amp plug ins were done. The bathroom door to the women's shower was slammed shut and bent the door latch. The door had to be cut out and replaced with a new door. New signs were added at each doggie pot station stating that dogs are to be on a leash.

## Occupancy Percent by Month & Year

	2019	2020*	2021	2022	Change from 2021	Forecast**
January	10.3	7.5	19	17	(2)	0
February	7.3	16	23	26	3	
March	16.8	16.4	39	29	(10)	
April	13.5	0	27.5	23	(4.5)	
May	26.4	5.7	43.1	31	(12.1)	
June	39.9	71.1	59.5	45	(14.5)	
July	61.3	84.7	85	72	(13)	
August	60.8	70	77			52
September	45.4	51	64			27
October	25.4	68	34			6
November	15.2	22	21			1
December	8.5	15	11			0

**Average    27.5            35.6            41.9**

\* April & most of May 2020 RV Park was closed due to COVID-19.

\*\* Forecast – Park allows for guests to reserve 6 months in advance.

## Marina

Staff has started brush removal around the barge basin. Staff will continue to make their way around the fuel dock and the transient dock through the summer. Quarterly dock inspections were conducted, and work orders were made for small repairs. Staff has started doing seaworthy inspections on vessels may be in question. Any problems or violations that are found given 30 to 60 days to make repairs and reschedule a new inspection.

**Boat Launches Paid through Launch Machine**

	2019	2020	2021	2022	Change from 2021
January	66	5	27	190	163
February	47	102	70	195	125
March	66	204	178	196	18
April	122	244	386	162	(244)
May	276	282	233	161	(72)
June	303	697	759	475	(284)
July	794	1095	826	982	156
August	875	768	716		
September	350	583	713		
October	518	713	518		
November	352	109	70		
December	53	40	60		
<b>Totals</b>	<b>3,822</b>	<b>4,842</b>	<b>4,556</b>	<b>2,361</b>	<b>(138)</b>

**Equipment Services Performed by Port Staff**

**Telehandler Jobs**

	2019	2020	2021	2022
January	4	2	0	8
February	1	6	3	2
March	6	4	6	5
April	7	10	5	7
May	6	3	7	6
June	3	0	3	4
July	1	5	0	1
August	3	4	1	
September	3	3	1	
October	10	6	5	
November	3	9	13	
December	15	5	3	
<b>Totals</b>	<b>62</b>	<b>57</b>	<b>47</b>	<b>33</b>

**Travel Lift Haul-Outs**

	2019	2020	2021	2022
January	2	1	0	2
February	2	5	1	6
March	4	5	6	6
April	7	5	6	7
May	13	9	5	8
June	16	15	12	6
July	15	14	7	8
August	8	4	7	
September	7	6	8	
October	9	8	4	
November	8	5	12	
December	5	1	0	
<b>Totals</b>	<b>96</b>	<b>78</b>	<b>68</b>	<b>43</b>



**Commercial Receiving Dock**

Quarterly inspections were completed on all receiving docks. Annual inspections of all dock hoists and Travelift were completed. All hoist in operation passed. Results will be sent to OSHA and office for our records.

**Commercial Retail Building**

No change to report at the boat yard building or hallmark dock. The main retail building has nothing to report, but we may need to start thinking about replacing the roof on at least one of the buildings soon. Both roofs are reaching the life span.

**Maintenance Crew**

Staff completed 100 work orders for the month of July. Landscaping was a large part of the month. Staff weeded retail area and rock beds along Lower Harbor Road, brush removal from banks, and site cleanup at check out time in RV park. July had a lot of people out fishing that resulted in 11 dump runs. About 22 cubic yards of fish carcasses.

# FINANCIAL SUMMARY & MANAGER REPORT

**Date:** August 17, 2022  
**Period:** Month End Report for July 2022  
**To:** Honorable Board President and District Board Members  
**Issued by:**

## July 2022 Financial Report - Overview

### Balance Sheet

End of the month unrestricted cash and equivalents totaled \$398,703. Restricted cash and equivalents totaled \$957,938 with Total Checking/Savings (cash) at \$1,356,640.

### July Profit & Loss

Total revenue from all funds was \$1,631,829 (cash carry over \$1,306,580). Total expense was \$323,759. The net income for July (including cash carry over) was \$1,308,069.

July Revenue Centers		Expenses	Net
Marina	\$66,940	\$72,053	(\$5,113)
Beachfront RV Park	\$74,739	\$40,028	\$34,711
Commercial / Retail	\$51,037	\$45,590	\$5,447
Fuel Dock	\$132,533	\$99,553	32,980

### Budget Performance FY 2022-2023

Total income is 17.6% with general fund revenues at 9.8% and total expense is 3.5% with general fund expenses at 7.4%.

### ATTACHMENTS

- Port Balance Sheet as of June 30, 2022, 2 pages
- Profit & Loss June 2022, 3 pages
- Profit & Loss Budget Performance, FY July 2022 thru June 2023, 4 pages
- June 2022 Check Register, 3 pages
- Vendor Summary for January through December 2022, 3 pages

Depreciation expense is not included in the budget or in our financial reports. If depreciation expense was included in the budget it would be difficult to balance the budget, and depreciation is not a cash expense, required under Generally Accepted Accounting Principles (GAAP), but not Governmental Accounting Standards Board (GASB).

**Port Items**

Met with Coast Guard spill reporting team from Portland. The team is making rounds to all ports in Oregon. They gave our port very high praise on dealing with spills and reporting. Our staff should be commended on their work keeping our local environment as clean as possible and following all protocols handling spills.

Met with County Sheriff on multiple issues at the Port. Port security cameras identified the individual that stole the boat in Basin 1 and the lighting was fine. Cameras also showed this individual did not try to leave the docks as mentioned last meeting. Our cameras recorded the incident with loose dogs at Kite Field, help identify the time and vehicle stealing a construction trailer from across Lower Harbor Road, identified when damage occurred to private property, recorded vehicle accident in retail parking lot and captured the incident at the RV Park. Cameras also showed how out-of-control the prohibited fireworks got during the 4<sup>th</sup> of July on the beach and on Port property. Cameras also continue to identify where oil slicks are coming from to give the Port the opportunity to stop them from happening again.

Closeout meeting with Oregon Department of Environmental inspector. Inspector gave high marks to the Port. No violations were noted during this inspection period (2019 – 2022).

**FEMA**

FEMA responded to our final budget submittal and is disputing the amount of sediment from the 2019 storms. Jack Akin/EMC Engineering is reviewing their data and will respond to their assertions.

# Port of Brookings Harbor Balance Sheet

Cash Basis

	Jul 31, 22
<b>ASSETS</b>	
Current Assets	
Checking/Savings	
100 · UNRESTRICTED CASH & EQUIVALENTS	
101 · GENERAL FUND CHECKING & LGIP	
10103 · General Funds Ckg Umpqua 3634	145,958.08
10104 · RCU Business Ownership 0687	18.10
10105 · RCU Business Savings 0600	10.00
10106 · General Fund LGIP 6017	214,474.26
10107 · Dredging Fund LGIP 6254	36,118.24
Total 101 · GENERAL FUND CHECKING & LGIP	396,578.68
10101 · Petty Cash	413.99
10102 · COUNTER CASH	
10102.1 · Office/Reception Cash Drawer	400.00
10102.2 · RV Park Cash Drawer	510.00
10102.3 · Fuel Dock Cash Drawer	800.00
Total 10102 · COUNTER CASH	1,710.00
Total 100 · UNRESTRICTED CASH & EQUIVALENTS	398,702.67
110 · RESTRICTED CASH & EQUIVALENTS	
104 · RESTRICTED MONEY MKT & CHECKING	
20104 · USDA BOND Umpqua MM 9529	2,520.87
30104 · Debt Service Umpqua MM 8627	2,516.15
40104 · Capital Projects Umpqua 8018	2,500.00
Total 104 · RESTRICTED MONEY MKT & CHECKING	7,537.02
105 · RESTRICTED LGIP	
20105 · USDA Bond Fund LGIP 6021	111,281.64
30105 · IFA Debt Service Fund LGIP 6020	46,462.14
50105 · Reserve Fund LGIP 6018	218,030.52
70105 · Capital Projects LGIP 6273	
70105.2 · Port Construction Fund	574,626.47
Total 70105 · Capital Projects LGIP 6273	574,626.47
Total 105 · RESTRICTED LGIP	950,400.77
Total 110 · RESTRICTED CASH & EQUIVALENTS	957,937.79
Total Checking/Savings	1,356,640.46
Accounts Receivable	
120 · ACCOUNTS RECEIVABLE	-20,069.17
Total Accounts Receivable	-20,069.17
Other Current Assets	
130 · DUE FROM TRANSFERS	
40130 · Due From Capital Projects	108,219.09
Total 130 · DUE FROM TRANSFERS	108,219.09
150 · Undeposited Funds	5,992.26
Total Other Current Assets	114,211.35
Total Current Assets	1,450,782.64
<b>TOTAL ASSETS</b>	<b>1,450,782.64</b>
<b>LIABILITIES &amp; EQUITY</b>	
Liabilities	
Current Liabilities	
Accounts Payable	
200 · ACCOUNTS PAYABLE	

# Port of Brookings Harbor Balance Sheet

Cash Basis

	Jul 31, 22
10201 · General Fund Accounts Payable	-29.98
<b>Total 200 · ACCOUNTS PAYABLE</b>	<b>-29.98</b>
<b>Total Accounts Payable</b>	<b>-29.98</b>
<b>Credit Cards</b>	
106 · RCU VISA ACCT	10,022.73
106.1 · RCU Business Ownership 0687	18.10
106.2 · RCU Business Savings 0600	10.00
<b>Total Credit Cards</b>	<b>10,050.83</b>
<b>Other Current Liabilities</b>	
100222 · Payroll Liabilities	
10222 · HealthCare Premium - Dependent	-845.27
<b>Total 100222 · Payroll Liabilities</b>	<b>-845.27</b>
10226 · Lodging Tax Payable	25,318.71
230 · DUE TO TRANSFERS	
40230 · Due To General Fund from CP	108,219.09
<b>Total 230 · DUE TO TRANSFERS</b>	<b>108,219.09</b>
<b>Total Other Current Liabilities</b>	<b>132,692.53</b>
<b>Total Current Liabilities</b>	<b>142,713.38</b>
<b>Total Liabilities</b>	<b>142,713.38</b>
<b>Equity</b>	
300 · Fund Balance	
301 · Unappropriated Balance	
10301 · General Fund Unappropriated Bal	532,465.33
20301 · Revenue Bond Unappropriate Bal	102,351.92
30301 · Debt Service Unappropriated Bal	22,758.51
40301 · Capital Project Unappropriated	40,430.77
50301 · Reserve Fund Unappropriated Bal	402,738.52
70301 · Port Const. Fund Unappropriated	569,448.67
<b>Total 301 · Unappropriated Balance</b>	<b>1,670,193.72</b>
302 · Appropriated Carryover	
10302 · General Fund Appropriated Carry	-532,465.33
20302 · Revenue Bond Appropriated Carry	-102,351.92
30302 · Debt Service Appropriated Carry	-22,758.51
40302 · Capital Proj Appropriated Carry	-40,430.77
50302 · Reserve Fund Appropriated Carry	-402,738.52
70302 · Port Const. Fund Appropriated	-569,448.67
<b>Total 302 · Appropriated Carryover</b>	<b>-1,670,193.72</b>
<b>Total 300 · Fund Balance</b>	<b>0.00</b>
<b>Net Income</b>	<b>1,308,069.26</b>
<b>Total Equity</b>	<b>1,308,069.26</b>
<b>TOTAL LIABILITIES &amp; EQUITY</b>	<b>1,450,782.64</b>

**Port of Brookings Harbor  
Profit & Loss**

Cash Basis

July 2022

	Jul 22
Income	
400 · REVENUES	
401 · GENERAL FUND REVENUES	
10411 · Cash Carry Over	334,747.33
10413 · Property Tax Prior	1,993.38
10414 · Interest General Fund	265.08
10418 · Miscellaneous	702.38
	337,708.17
Total 401 · GENERAL FUND REVENUES	
402 · GENERAL FUND PROGRAM REVENUES	
10421 · MARINA	
10421.2 · MOORAGE	
10421.3 · Commercial Slip Rent	5,522.46
10421.4 · Recreational Slip Rent	38,217.45
10421.5 · Transient	1,481.19
10421.6 · Other Moorage	315.00
	45,536.10
Total 10421.2 · MOORAGE	
10422 · Boat Launch	4,955.00
10423 · STORAGE	
10423.1 · Gear Storage	4,861.04
10423.2 · Boat Storage	3,091.00
	7,952.04
Total 10423 · STORAGE	
10424 · ADMINISTRATIVE FEES	597.41
10425 · MARINE SERVICES	
10425.1 · Travelift	3,192.00
10425.2 · 12 K Telehandler	924.00
10425.3 · Other Sales & Fees	1,086.60
10425.4 · Public Hoist	35.00
	5,237.60
Total 10425 · MARINE SERVICES	
10426 · EVENTS ON PORT PROPERTY	2,662.00
	66,940.15
Total 10421 · MARINA	
10427 · BEACHFRONT RV PARK	
10427.1 · Space Rental	67,208.19
10427.2 · Other Sales & Fees	7,530.99
	74,739.18
Total 10427 · BEACHFRONT RV PARK	
10428 · COMMERCIAL RETAIL	
10428.1 · Retail Property	34,278.48
10428.2 · Docks	13,553.67
10428.3 · CPI and Other Fees	3,204.65
	51,036.80
Total 10428 · COMMERCIAL RETAIL	
10429 · FUEL DOCK	132,532.87
	325,249.00
Total 402 · GENERAL FUND PROGRAM REVENUES	
420 · USDA REVENUE BOND FUND	
20411 · Cash Carry Over - USDA Revenue	102,841.78
20414 · Interest Revenue Bond Fund	117.73
20419 · Transfer to USDA Bond Fund	10,843.00
	113,802.51
Total 420 · USDA REVENUE BOND FUND	
430 · DEBT SERVICE FUND REVENUE	
30411 · Cash Carry Over - Debt Service	23,094.07
30414 · Interest Debt Service Fund	49.22
30419 · Transfer to Debt Service Fund	31,958.71
	55,102.00
Total 430 · DEBT SERVICE FUND REVENUE	

**Port of Brookings Harbor  
Profit & Loss**

Cash Basis

July 2022

	Jul 22
440 · CAPITAL PROJECTS FUND REVENUE	
40411 · Cash Carry Over - Capt Proj	2,500.00
<b>Total 440 · CAPITAL PROJECTS FUND REVENUE</b>	<b>2,500.00</b>
450 · RESERVE FUND REVENUE	
50411 · Cash Carry Over - Reserve Fund	215,799.89
50414 · Interest Reserve Fund	230.63
50419 · Transfer to Reserve Fund	2,000.00
<b>Total 450 · RESERVE FUND REVENUE</b>	<b>218,030.52</b>
460 · DEBT SERV. RV PARK IMPROV. FUND	
60419 · Transfer OR FFC 2020 Debt Serv.	4,809.87
<b>Total 460 · DEBT SERV. RV PARK IMPROV. FUND</b>	<b>4,809.87</b>
470 · PORT CONSTRUCTION FUND REVENUE	
70411 · Cash Carry Over - Port Const.	574,018.64
70414 · Interest Port Construction Fund	607.83
<b>Total 470 · PORT CONSTRUCTION FUND REVENUE</b>	<b>574,626.47</b>
<b>Total 400 · REVENUES</b>	<b>1,631,828.54</b>
<b>Total Income</b>	<b>1,631,828.54</b>
<b>Gross Profit</b>	<b>1,631,828.54</b>
<b>Expense</b>	
600 · GENERAL FUND EXPENDITURES	
10900 · Operating Transfers Out General	49,611.58
500 · PERSONNEL SERVICES	
10502 · Office Staff	20,212.14
10504 · Operations Staff	20,209.35
10506 · Overtime	206.19
10508 · Payroll Taxes/Costs/Benefits	
10508.1 · Paid Holidays	1,317.72
10508.2 · Sick Leave Benefit	859.52
10508.3 · Vacation	1,467.52
10508.4 · Payroll Taxes	4,606.12
10508.5 · SEP Retirement	4,166.49
<b>Total 10508 · Payroll Taxes/Costs/Benefits</b>	<b>12,417.37</b>
10510 · Health Care and Dental	8,921.11
10512 · Workers Compensation	20,639.83
<b>Total 500 · PERSONNEL SERVICES</b>	<b>82,605.99</b>
601 · GENERAL FUND Material & Service	
10601 · ADVERTISING & NOTIFICATIONS	725.50
10602 · REPAIRS & MAINTENANCE	
10602.1 · Equip. Repair/Maintenance	3,125.63
10602.2 · Supplies	24,021.72
10602.3 · Services	10,301.69
<b>Total 10602 · REPAIRS &amp; MAINTENANCE</b>	<b>37,449.04</b>
10603 · FUEL purchased for resale	66,952.23
10605 · UTILITIES	
10605.1 · Electric	7,911.38
10605.2 · RV Park Cable TV	595.08
10605.3 · Sanitary	4,060.60
10605.5 · Telecommunications	781.20
10605.6 · Waste Removal	9,415.88
10605.7 · Water	1,855.84
<b>Total 10605 · UTILITIES</b>	<b>24,619.98</b>
10606 · OFFICE EXPENSE	1,231.99

**Port of Brookings Harbor  
Profit & Loss**

Cash Basis

July 2022

	Jul 22
10607 · BANK SERVICE & FINANCE FEES	6,366.93
10608 · TRAINING & TRAVEL	0.00
10609 · PERMITS, LICENSES, TAXES & MISC	472.20
10610 · INSURANCE; PROP & CAS, BOND	10,328.58
10611 · PROFESSIONAL FEES	
10611.2 · Attorney	16,564.50
10611.3 · Engineering	9,925.87
10611.4 · Other Support/Consultant	871.31
Total 10611 · PROFESSIONAL FEES	27,361.68
Total 601 · GENERAL FUND Material & Service	175,508.13
Total 600 · GENERAL FUND EXPENDITURES	307,725.70
630 · DEBT SERVICE FUND EXPENDITURES	
801 · Principal	
30803P · 50 BFMII Travelift Principal	4,330.21
30804P · 2018 Genie Forklift Principal	1,235.10
Total 801 · Principal	5,565.31
810 · Interest Payments	
30813I · 50 BFMII Travelift Interest	328.79
30814I · 2018 Genie Forklift Interest	229.61
Total 810 · Interest Payments	558.40
Total 630 · DEBT SERVICE FUND EXPENDITURES	6,123.71
640 · CAPT. PROJ. EXPENDITURES	
740 · CAPT. PROJ. CAPITAL OUTLAY	
40702 · Land Improvement - Capt Proj	
40702.1 · Engineering/Consultants	5,100.00
Total 40702 · Land Improvement - Capt Proj	5,100.00
Total 740 · CAPT. PROJ. CAPITAL OUTLAY	5,100.00
Total 640 · CAPT. PROJ. EXPENDITURES	5,100.00
660 · DEBT SERV. RV PARK EXPENDITURES	
60806P · RV Park Improv. Loan Principal	3,280.70
60815I · RV Park Improv. Loan Interest	1,529.17
Total 660 · DEBT SERV. RV PARK EXPENDITURES	4,809.87
Total Expense	323,759.28
Net Income	1,308,069.26



**Port of Brookings Harbor  
Profit & Loss Budget Performance FY 2022-2023**

July 2022

	Jul 22	Budget	% of Budget
<b>Income</b>			
<b>400 · REVENUES</b>			
<b>401 · GENERAL FUND REVENUES</b>			
10411 · Cash Carry Over	334,747.33	490,000.00	68.3%
10412 · Property Tax Current	0.00	250,000.00	0.0%
10413 · Property Tax Prior	1,993.38	10,000.00	19.9%
10414 · Interest General Fund	265.08	2,000.00	13.3%
10417 · Assets Sales	0.00	10,000.00	0.0%
10418 · Miscellaneous	702.38	66,000.00	1.1%
10420 · Grants & Other Funding - GF	0.00	20,000.00	0.0%
<b>Total 401 · GENERAL FUND REVENUES</b>	<b>337,708.17</b>	<b>848,000.00</b>	<b>39.8%</b>
<b>402 · GENERAL FUND PROGRAM REVENUES</b>			
<b>10421 · MARINA</b>			
<b>10421.2 · MOORAGE</b>			
10421.3 · Commercial Slip Rent	5,522.46		
10421.4 · Recreational Slip Rent	38,217.45		
10421.5 · Transient	1,481.19	0.00	100.0%
10421.6 · Other Moorage	315.00		
<b>Total 10421.2 · MOORAGE</b>	<b>45,536.10</b>	<b>0.00</b>	<b>100.0%</b>
<b>10422 · Boat Launch</b>			
10422 · Boat Launch	4,955.00		
<b>10423 · STORAGE</b>			
10423.1 · Gear Storage	4,861.04		
10423.2 · Boat Storage	3,091.00		
<b>Total 10423 · STORAGE</b>	<b>7,952.04</b>		
<b>10424 · ADMINISTRATIVE FEES</b>			
10424 · ADMINISTRATIVE FEES	597.41	0.00	100.0%
<b>10425 · MARINE SERVICES</b>			
10425.1 · Travelift	3,192.00	0.00	100.0%
10425.2 · 12 K Telehandler	924.00	0.00	100.0%
10425.3 · Other Sales & Fees	1,086.60		
10425.4 · Public Hoist	35.00		
10425 · MARINE SERVICES - Other	0.00	0.00	0.0%
<b>Total 10425 · MARINE SERVICES</b>	<b>5,237.60</b>	<b>0.00</b>	<b>100.0%</b>
<b>10426 · EVENTS ON PORT PROPERTY</b>			
10426 · EVENTS ON PORT PROPERTY	2,662.00		
10421 · MARINA - Other	0.00	842,000.00	0.0%
<b>Total 10421 · MARINA</b>	<b>66,940.15</b>	<b>842,000.00</b>	<b>8.0%</b>
<b>10427 · BEACHFRONT RV PARK</b>			
10427.1 · Space Rental	67,208.19	0.00	100.0%
10427.2 · Other Sales & Fees	7,530.99	0.00	100.0%
10427 · BEACHFRONT RV PARK - Other	0.00	800,000.00	0.0%
<b>Total 10427 · BEACHFRONT RV PARK</b>	<b>74,739.18</b>	<b>800,000.00</b>	<b>9.3%</b>
<b>10428 · COMMERCIAL RETAIL</b>			
10428.1 · Retail Property	34,278.48	0.00	100.0%
10428.2 · Docks	13,553.67	0.00	100.0%
10428.3 · CPI and Other Fees	3,204.65	0.00	100.0%
10428 · COMMERCIAL RETAIL - Other	0.00	635,000.00	0.0%
<b>Total 10428 · COMMERCIAL RETAIL</b>	<b>51,036.80</b>	<b>635,000.00</b>	<b>8.0%</b>
<b>10429 · FUEL DOCK</b>			
10429 · FUEL DOCK	132,532.87	1,050,000.00	12.6%
<b>Total 402 · GENERAL FUND PROGRAM REVEN...</b>	<b>325,249.00</b>	<b>3,327,000.00</b>	<b>9.8%</b>
<b>420 · USDA REVENUE BOND FUND</b>			
20411 · Cash Carry Over - USDA Revenue	102,841.78	102,820.00	100.0%
20414 · Interest Revenue Bond Fund	117.73	840.00	14.0%
20419 · Transfer to USDA Bond Fund	10,843.00	130,120.00	8.3%
<b>Total 420 · USDA REVENUE BOND FUND</b>	<b>113,802.51</b>	<b>233,780.00</b>	<b>48.7%</b>

**Port of Brookings Harbor**  
**Profit & Loss Budget Performance FY 2022-2023**

July 2022

	Jul 22	Budget	% of Budget
<b>430 · DEBT SERVICE FUND REVENUE</b>			
30411 · Cash Carry Over - Debt Service	23,094.07	23,100.00	100.0%
30414 · Interest Debt Service Fund	49.22	616.00	8.0%
30419 · Transfer to Debt Service Fund	31,958.71	391,484.00	8.2%
<b>Total 430 · DEBT SERVICE FUND REVENUE</b>	<b>55,102.00</b>	<b>415,200.00</b>	<b>13.3%</b>
<b>440 · CAPITAL PROJECTS FUND REVENUE</b>			
40411 · Cash Carry Over - Capt Proj	2,500.00	2,500.00	100.0%
40416 · Government Funding			
40416.2 · FEMA Funding	0.00	3,580,000.00	0.0%
40416.3 · State Lottery Funding	0.00	0.00	0.0%
<b>Total 40416 · Government Funding</b>	<b>0.00</b>	<b>3,580,000.00</b>	<b>0.0%</b>
40419 · Transfer to Capital Project	0.00	0.00	0.0%
<b>Total 440 · CAPITAL PROJECTS FUND REVEN...</b>	<b>2,500.00</b>	<b>3,582,500.00</b>	<b>0.1%</b>
<b>450 · RESERVE FUND REVENUE</b>			
50411 · Cash Carry Over - Reserve Fund	215,799.89	215,700.00	100.0%
50414 · Interest Reserve Fund	230.63	1,500.00	15.4%
50419 · Transfer to Reserve Fund	2,000.00	26,000.00	7.7%
<b>Total 450 · RESERVE FUND REVENUE</b>	<b>218,030.52</b>	<b>243,200.00</b>	<b>89.7%</b>
<b>460 · DEBT SERV. RV PARK IMPROV. FUND</b>			
60419 · Transfer OR FFC 2020 Debt Serv.	4,809.87	57,720.00	8.3%
<b>Total 460 · DEBT SERV. RV PARK IMPROV. FU...</b>	<b>4,809.87</b>	<b>57,720.00</b>	<b>8.3%</b>
<b>470 · PORT CONSTRUCTION FUND REVENUE</b>			
70411 · Cash Carry Over - Port Const.	574,018.64	572,000.00	100.4%
70414 · Interest Port Construction Fund	607.83	1,170.00	52.0%
<b>Total 470 · PORT CONSTRUCTION FUND REVE...</b>	<b>574,626.47</b>	<b>573,170.00</b>	<b>100.3%</b>
<b>Total 400 · REVENUES</b>	<b>1,631,828.54</b>	<b>9,280,570.00</b>	<b>17.6%</b>
<b>Total Income</b>	<b>1,631,828.54</b>	<b>9,280,570.00</b>	<b>17.6%</b>
<b>Gross Profit</b>	<b>1,631,828.54</b>	<b>9,280,570.00</b>	<b>17.6%</b>
<b>Expense</b>			
<b>600 · GENERAL FUND EXPENDITURES</b>			
10900 · Operating Transfers Out General	49,611.58	605,324.00	8.2%
<b>500 · PERSONNEL SERVICES</b>			
10502 · Office Staff	20,212.14	289,000.00	7.0%
10504 · Operations Staff	20,209.35	267,800.00	7.5%
10506 · Overtime	206.19	8,000.00	2.6%
10508 · Payroll Taxes/Costs/Benefits			
10508.1 · Paid Holidays	1,317.72	0.00	100.0%
10508.2 · Sick Leave Benefit	859.52	0.00	100.0%
10508.3 · Vacation	1,467.52	0.00	100.0%
10508.4 · Payroll Taxes	4,606.12	0.00	100.0%
10508.5 · SEP Retirement	4,166.49	0.00	100.0%
10508 · Payroll Taxes/Costs/Benefits - Other	0.00	193,300.00	0.0%
<b>Total 10508 · Payroll Taxes/Costs/Benefits</b>	<b>12,417.37</b>	<b>193,300.00</b>	<b>6.4%</b>
10510 · Health Care and Dental	8,921.11	106,600.00	8.4%
10512 · Workers Compensation	20,639.83	22,000.00	93.8%
<b>Total 500 · PERSONNEL SERVICES</b>	<b>82,605.99</b>	<b>886,700.00</b>	<b>9.3%</b>
<b>601 · GENERAL FUND Material &amp; Service</b>			
10601 · ADVERTISING & NOTIFICATIONS	725.50	5,624.00	12.9%
10602 · REPAIRS & MAINTENANCE			
10602.1 · Equip. Repair/Maintenance	3,125.63	0.00	100.0%
10602.2 · Supplies	24,021.72	0.00	100.0%
10602.3 · Services	10,301.69	0.00	100.0%

**Port of Brookings Harbor**  
**Profit & Loss Budget Performance FY 2022-2023**

July 2022

	Jul 22	Budget	% of Budget
10602 · REPAIRS & MAINTENANCE - Other	0.00	635,490.00	0.0%
<b>Total 10602 · REPAIRS &amp; MAINTENANCE</b>	<b>37,449.04</b>	<b>635,490.00</b>	<b>5.9%</b>
10603 · FUEL purchased for resale	66,952.23	924,000.00	7.2%
10605 · UTILITIES			
10605.1 · Electric	7,911.38	0.00	100.0%
10605.2 · RV Park Cable TV	595.08	0.00	100.0%
10605.3 · Sanitary	4,060.60	0.00	100.0%
10605.5 · Telecommunications	781.20	0.00	100.0%
10605.6 · Waste Removal	9,415.88	0.00	100.0%
10605.7 · Water	1,855.84	0.00	100.0%
10605 · UTILITIES - Other	0.00	379,164.00	0.0%
<b>Total 10605 · UTILITIES</b>	<b>24,619.98</b>	<b>379,164.00</b>	<b>6.5%</b>
10606 · OFFICE EXPENSE	1,231.99	51,032.00	2.4%
10607 · BANK SERVICE & FINANCE FEES	6,366.93	64,134.00	9.9%
10608 · TRAINING & TRAVEL	0.00	8,453.00	0.0%
10609 · PERMITS, LICENSES, TAXES & MISC	472.20	46,089.00	1.0%
10610 · INSURANCE; PROP & CAS, BOND	10,328.58	126,314.00	8.2%
10611 · PROFESSIONAL FEES			
10611.1 · Accounting/Auditing	0.00	0.00	0.0%
10611.2 · Attorney	16,564.50	0.00	100.0%
10611.3 · Engineering	9,925.87	0.00	100.0%
10611.4 · Other Support/Consultant	871.31	0.00	100.0%
10611 · PROFESSIONAL FEES - Other	0.00	175,000.00	0.0%
<b>Total 10611 · PROFESSIONAL FEES</b>	<b>27,361.68</b>	<b>175,000.00</b>	<b>15.6%</b>
<b>Total 601 · GENERAL FUND Material &amp; Service</b>	<b>175,508.13</b>	<b>2,415,300.00</b>	<b>7.3%</b>
710 · GENERAL FUND CAPITAL OUTLAY			
10704 · Equipment	0.00	0.00	0.0%
710 · GENERAL FUND CAPITAL OUTLAY - Ot...	0.00	25,000.00	0.0%
<b>Total 710 · GENERAL FUND CAPITAL OUTLAY</b>	<b>0.00</b>	<b>25,000.00</b>	<b>0.0%</b>
920 · OPERATING CONTINGENCY	0.00	217,676.00	0.0%
<b>Total 600 · GENERAL FUND EXPENDITURES</b>	<b>307,725.70</b>	<b>4,150,000.00</b>	<b>7.4%</b>
620 · USDA REVENUE BOND EXPENDITURES			
20801P · USDA Revenue Bond Principal	0.00	83,913.00	0.0%
20810I · USDA Revenue Bond Interest	0.00	46,207.00	0.0%
<b>Total 620 · USDA REVENUE BOND EXPENDITUR...</b>	<b>0.00</b>	<b>130,120.00</b>	<b>0.0%</b>
630 · DEBT SERVICE FUND EXPENDITURES			
30802P · IFA PRINCIPAL			
30802.1 · OBDD #520139/Boardwalk Prin	0.00	0.00	0.0%
30802.2 · OBDD #525172/RV Park Prin.	0.00	0.00	0.0%
30802.3 · OBDD #525176/Green Bldg Prn	0.00	0.00	0.0%
30802.4 · OBDD #525181/EurekaFish Prn	0.00	0.00	0.0%
30802.5 · SPWF #L02009/Cold Strg Prin	0.00	0.00	0.0%
30802.8 · SPWF L02001/MarineFuel Dock Prn	0.00	0.00	0.0%
30802.9 · SPWF X03004/Eureka Fishery Prin	0.00	0.00	0.0%
30802P · IFA PRINCIPAL - Other	0.00	318,000.00	0.0%
<b>Total 30802P · IFA PRINCIPAL</b>	<b>0.00</b>	<b>318,000.00</b>	<b>0.0%</b>
801 · Principal			
30803P · 50 BFMII Travelift Principal	4,330.21	53,293.00	8.1%
30804P · 2018 Genie Forklift Principal	1,235.10	15,374.00	8.0%
<b>Total 801 · Principal</b>	<b>5,565.31</b>	<b>68,667.00</b>	<b>8.1%</b>
810 · Interest Payments			
30813I · 50 BFMII Travelift Interest	328.79	2,623.00	12.5%

**Port of Brookings Harbor**  
**Profit & Loss Budget Performance FY 2022-2023**

July 2022

	Jul 22	Budget	% of Budget
30814I · 2018 Genie Forklift Interest	229.61	2,210.00	10.4%
Total 810 · Interest Payments	558.40	4,833.00	11.6%
Total 630 · DEBT SERVICE FUND EXPENDITURES	6,123.71	391,500.00	1.6%
640 · CAPT. PROJ. EXPENDITURES			
40602 · Materials & Services Capt Proj	0.00	0.00	0.0%
740 · CAPT. PROJ. CAPITAL OUTLAY			
40702 · Land Improvement - Capt Proj			
40702.1 · Engineering/Consultants	5,100.00	0.00	100.0%
40702.2 · Supplies	0.00	0.00	0.0%
40702 · Land Improvement - Capt Proj - Ot...	0.00	3,580,000.00	0.0%
Total 40702 · Land Improvement - Capt Proj	5,100.00	3,580,000.00	0.1%
Total 740 · CAPT. PROJ. CAPITAL OUTLAY	5,100.00	3,580,000.00	0.1%
Total 640 · CAPT. PROJ. EXPENDITURES	5,100.00	3,580,000.00	0.1%
650 · RESERVE FUND EXPENDITURES			
50200 · RESERVE for FUTURE EXPENDITURE	0.00	243,200.00	0.0%
Total 650 · RESERVE FUND EXPENDITURES	0.00	243,200.00	0.0%
660 · DEBT SERV. RV PARK EXPENDITURES			
60806P · RV Park Improv. Loan Principal	3,280.70	39,900.00	8.2%
60815I · RV Park Improv. Loan Interest	1,529.17	17,820.00	8.6%
Total 660 · DEBT SERV. RV PARK EXPENDITURES	4,809.87	57,720.00	8.3%
670 · PORT CONST FUND EXPENDITURES			
70100 · PORT CONST. CAPITAL OUTLAY			
70700 · Land Improvement - Port Const.	0.00	0.00	0.0%
70100 · PORT CONST. CAPITAL OUTLAY - Ot...	0.00	573,170.00	0.0%
Total 70100 · PORT CONST. CAPITAL OUTLAY	0.00	573,170.00	0.0%
Total 670 · PORT CONST FUND EXPENDITURES	0.00	573,170.00	0.0%
930 · Fund Balances			
10930 · Unappropriated Balance GF	0.00	25,000.00	0.0%
20930 · Unappropriated Balance-USDA	0.00	103,660.00	0.0%
30930 · Unappropriated Balance Debt	0.00	23,700.00	0.0%
40930 · Unappropriated Balance Capt Pro	0.00	2,500.00	0.0%
Total 930 · Fund Balances	0.00	154,860.00	0.0%
Total Expense	323,759.28	9,280,570.00	3.5%
Net Income	1,308,069.26	0.00	100.0%

Port of Brookings Harbor

Check Registers

As of July 31, 2022

Cash Basis

Type	Num	Date	Name	Memo	Debit	Credit
<b>100 · UNRESTRICTED CASH &amp; EQUIVALENTS</b>						
<b>101 · GENERAL FUND CHECKING &amp; LGIP</b>						
<b>10103 · General Funds Ckg Umpqua 3634</b>						
Bill Pmt -Check	DEBIT	07/05/2022	US Bank Equipment Finance	Contract No. 500-0623925-000 RICOH IMC6000 Copier		223.20
Check	DEBIT	07/01/2022	ADP	Advice of Debit #609058988 ezLaborManager/ADP 300 Timeclock (3 Timeclocks)		180.35
Check	DEBIT	07/05/2022	Elavon	JUN 2022 MERCHANT SERVICE FEE ACCT#316		969.14
Check	DEBIT	07/05/2022	Elavon	JUN 2022 MERCHANT SERVICE FEE ACCT#873 Ventek Boat Launch		89.48
Check	DEBIT	07/05/2022	Elavon	JUN 2022 MERCHANT SERVICE FEE ACCT#951		2,175.78
Sales Tax Pay...	DEBIT	07/06/2022	Oregon Lodging Tax	BIN: 0294055-3 2nd QTR 2022 State Lodging Tax		2,314.81
Bill Pmt -Check	DEBIT	07/18/2022	Tyree Oil, Inc	Account # 56851 Fuel Purchase for Resale		21,121.35
Bill Pmt -Check	DEBIT	07/20/2022	Tyree Oil, Inc	Account # 56851 Fuel Purchase for Resale		19,278.50
Check	DEBIT	07/08/2022	ADP	Advice of Debit 609348693 Payroll Date: 06/29/2022		148.31
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-15GM0		140.06
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-166B5		226.24
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-169JZ		162.52
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16DGJ		342.72
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16GNS		155.27
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16J8M		81.52
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16LGC		177.11
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16N78		162.00
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16PMB		153.73
Check	DEBIT	07/13/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationSYLL6-16S3F		318.88
Check	DEBIT	07/13/2022	TD Ameritrade	Employer Contribution 07/13/2022 ConfirmationSYLL6-16ZNZ		189.05
Check	DEBIT	07/07/2022	BL/ RV Park	STRIPE Debit - Refunds issued 07/07/2022		449.14
Check	DEBIT	07/07/2022	Tyree Oil	Unauthorized Debit for Fuel Purchase...already paid via petty cash		0.02
Bill Pmt -Check	DEBIT	07/18/2022	Chevron Business Card	Account #: 0496007075666 Fuel Purchases for Port Vehicles/Equipment		754.13
Check	DEBIT	07/22/2022	ADP	Advice of Debit 610398039 Payroll Date: 07/13/2022		148.31
Bill Pmt -Check	DEBIT	07/25/2022	Tyree Oil, Inc	Account # 56851 Fuel Purchase for Resale		26,552.38
Check	DEBIT	07/20/2022	Umpqua Bank (Service fees)	STORE CURRENCY DEPOSITED & SOLD for JUNE 2022		3.76
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MLN4D		150.28
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMGJH		226.24
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/13/2022 ConfirmationT010B-MMKDZ		152.76
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMMMMF		342.72
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMPQ0		149.95
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMRS6		76.38
Check	DEBIT	07/20/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMTL5		180.88
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMW4H		162.15
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MMY1N		100.83
Check	DEBIT	07/27/2022	Edward Jones	Employer Contribution 07/27/2022 ConfirmationT010B-MN02W		318.88
Check	DEBIT	07/27/2022	TD Ameritrade	Employer Contribution 07/27/2022 ConfirmationT010B-MN1KX		196.32
Bill Pmt -Check	DEBIT	07/25/2022	Pacific Office Automation	Customer # 507410 Copier Lease & Maintenance		263.89
Check	DEBIT	07/29/2022	ADP	Advice of Debit #611053218 ezLaborManager/ADP 300 Timeclock (3 Timeclocks)		180.35
Check	DEBIT	07/27/2022	Bolen, Joe	Refund for Charter License - paid at Kiosk in error. Batch#316/610		300.00
General Journal	DEBT 07/01	07/01/2022		Transfer to Debt Service Fund for Travelift Payment		4,659.00
General Journal	DEBT 07/01	07/01/2022		Transfer to Debt Service Fund for Fork Lift Payment		1,464.71
General Journal	DEBT 07/01	07/01/2022		Transfer to Debt Serv. RV Park for Umpqua Bank Loan Acct#97748040835 Payment		4,809.87
General Journal	Dredge 7/1	07/01/2022		General Fund Internal Transfer from Umpqua General Fund to Dredging Fund LGIP 6254 2% Gross ...		3,337.00
General Journal	IFA 07/01	07/01/2022		Transfer to IFA Debt Service for 3rd QTR 2022 Pmt		25,835.00
General Journal	RES 07/01	07/01/2022		Transfer to Reserve Fund		2,000.00
General Journal	USDA 07/01	07/01/2022		To transfer to USDA Revenue Bond Fund for November 2022 Payment		10,843.00
General Journal	CP 07/08	07/08/2022		Transfer to Capital Projects for payment to EMC Engineering inv#91009-2188		5,100.00
General Journal	PAY 07/13	07/13/2022		Rec 07/13/2022 payroll		17,220.47
General Journal	TAX 07/13	07/13/2022		Rec 07/13/2022 payroll		7,019.27
General Journal	PAY 07/27	07/27/2022		Rec 07/27/2022 payroll		16,675.88
General Journal	TAX 07/27	07/27/2022		Rec 07/27/2022 payroll		6,646.44
Bill Pmt -Check	11046	07/08/2022	SDAO Spec. Dist. Assoc. OR - Workers ...	Cust #02-0016414 Worker's Comp. Coverage Period: 7/1/2022-6/30/2023 Policy/Inv#37W16414-85		20,639.83

Port of Brookings Harbor

Check Registers

As of July 31, 2022

Cash Basis

Type	Num	Date	Name	Memo	Debit	Credit
Bill Pmt -Check	11047	07/08/2022	Travel Information Council	State Hwy Signs on Hwy 101 Annual Renewal - RV Park		358.00
Check	11049	07/08/2022	Lourence, Minda	REFUND to Minda Lourence		50.00
Sales Tax Pay...	11050	07/08/2022	Curry County TLT	Curry County Lodging Tax 2nd QTR 2022		10,802.45
Bill Pmt -Check	11051	07/08/2022	Amazon Capital Services	Business Account #A2VUC5YWS42764 - Supplies/Materials		398.75
Bill Pmt -Check	11052	07/08/2022	BI-MART	Account #931481 Water & Supplies		13.76
Bill Pmt -Check	11053	07/08/2022	Country Media, Inc.	CUST# 38747 Curry Coastal Pilot Notices		367.50
Bill Pmt -Check	11054	07/08/2022	Curry Equipment	Account#1052 Equip Repair & Maint. Supplies		109.48
Bill Pmt -Check	11055	07/08/2022	Curry Transfer & Recycling	Account #2040-2434-001 Trash Dumpsters		9,415.88
Bill Pmt -Check	11056	07/08/2022	Desi's Tree Trimming	06/28/2022 - Labor to remove vegetation around Ice House		1,650.00
Bill Pmt -Check	11057	07/08/2022	Eaton Corporation	MPL6007 Eaton Power Pedestal Main Door		339.40
Bill Pmt -Check	11058	07/08/2022	EMC-Engineers/Scientists, LLC	41 Hrs Port Engineering & Reimbursement to Monosov Consulting - RE Wastewater Treatment Plant		9,925.87
Bill Pmt -Check	11059	07/08/2022	Fastenal Industrial Supplies	Customer No.ORBRK0013 Toiletries & Supplies		166.27
Bill Pmt -Check	11060	07/08/2022	Four Aces Security Solutions LLC	JUNE 2022 - Security Patrol - 34%Marina, 33%Beachfront RV Park, 33%Commercial Retail		2,781.00
Bill Pmt -Check	11061	07/08/2022	Gowman Electric, Inc.	CCB: 198999 Electrical Repair		292.35
Bill Pmt -Check	11062	07/08/2022	Grainger	VOID: ACCT# 822663001	0.00	
Bill Pmt -Check	11063	07/08/2022	Harbor Sanitary District	JUNE 2022 Sanitary Bill		4,060.60
Bill Pmt -Check	11064	07/08/2022	Pape Material Handling	Customer No. 1070715 Equipment Maintenance & Repair		2,361.10
Bill Pmt -Check	11065	07/08/2022	SDAO Spec. Dist. Assoc. OR - Healthcare	Customer #: 03-0016414 - HEALTHCARE PREMIUM		10,350.32
Bill Pmt -Check	11066	07/08/2022	SDAO Spec. Dist. Assoc. OR - Prop & C...	Policy#31P16414-203 Customer ID: 01-16414 - 2021 PROPERTY & CASUALTY POLICY		10,328.58
Bill Pmt -Check	11067	07/08/2022	Suburban Propane	06/28/2022- Delivery of 66.40 Gallons of Propane		334.85
Bill Pmt -Check	11068	07/08/2022	Grainger	ACCT# 822663001 SPILL KIT & Materials for Toilet & Urinal Repairs		416.60
Bill Pmt -Check	11069	07/15/2022	Amazon Capital Services	Business Account #A2VUC5YWS42764 - Supplies/Materials		141.64
Bill Pmt -Check	11070	07/15/2022	BOLI PWR	WH-81 BOLI PWR Report for Basin 2 Piling Replacement - Contractor: Billeter Marine LLC		250.00
Bill Pmt -Check	11071	07/15/2022	Fastenal Industrial Supplies	Customer No.ORBRK0013 Toiletries & Supplies		3,266.71
Bill Pmt -Check	11072	07/15/2022	Gowman Electric, Inc.	CCB: 198999 Electrical Repair		142.99
Bill Pmt -Check	11073	07/15/2022	Harbor Logging Supply, Inc.	Metal for ladder replacement at Bornstein Seafoods Inc		553.41
Bill Pmt -Check	11074	07/15/2022	Harbor View Windows, Heating & Air	06/20/2022 - HVAC Repair - Bell & Whistle & 06/23/2022 - HVAC Repair - Pithitude		460.25
Bill Pmt -Check	11075	07/15/2022	John Kellum/John's Portable Welding	6/27-6/29/2022 - Ladder Replacement (Labor) at Bornstein Seafoods & 7/6/2022-Wire Hoist motor		1,312.50
Bill Pmt -Check	11076	07/15/2022	Miller Nash LLP	Legal Services		16,564.50
Bill Pmt -Check	11077	07/15/2022	Thermo Fluids, Inc.	Account # PO24273 Removal of Used Oil and Oily Water		149.50
Bill Pmt -Check	11078	07/15/2022	Tidewater Contractors, Inc.	Customer Code: 000061 - Rock Order No. 13419 Concrete V Blocks Stamped for Port use		1,260.00
Bill Pmt -Check	11079	07/15/2022	Traffic Safety Supply Co.	Customer ID:C004722 - 25 DELINEATORS		825.54
Check	11080	07/20/2022	Shawn Hall	Reimbursement for KEEN Utility Work Boots		219.92
Bill Pmt -Check	11081	07/20/2022	Amazon Capital Services	Business Account #A2VUC5YWS42764 - Supplies/Materials		237.40
Bill Pmt -Check	11082	07/20/2022	BI-MART	Account #931481 Water & Supplies		35.90
Bill Pmt -Check	11083	07/20/2022	Billeter Marine, LLC-vendor	Payment Bond for Basin 2 Pile Replacement Project		1,546.50
Bill Pmt -Check	11084	07/20/2022	Curry Equipment	Account#1052 Equip Repair & Maint. Supplies		229.97
Bill Pmt -Check	11085	07/20/2022	Quill Corporation	ACCT#1932158 Office Supplies		327.98
Bill Pmt -Check	11086	07/20/2022	Thermo Fluids, Inc.	Account # PO24273 Removal of Used Oil and Oily Water		115.50
Bill Pmt -Check	11087	07/20/2022	Gowman Electric, Inc.	CCB: 198999 Electrical Repair		85.00
Bill Pmt -Check	11088	07/27/2022	5-R Excavation, LLC	CCB: 155657 - 06/08 Pump out catch basins at Boaryard & 06/16 Clean Boat Launch Ramp		830.00
Bill Pmt -Check	11089	07/27/2022	Amazon Capital Services	Business Account #A2VUC5YWS42764 - Supplies/Materials		275.24
Bill Pmt -Check	11090	07/27/2022	Anchor Lock & Key	07/22/2022 - LABOR to install keypad on shower door at Beachfront RV Park Restroom		110.00
Bill Pmt -Check	11091	07/27/2022	BI-MART	Account #931481 Water & Supplies		14.99
Bill Pmt -Check	11092	07/27/2022	Coos-Curry Electric Cooperative, Inc.	ACCT # 67601 Electrical Service		7,911.38
Bill Pmt -Check	11093	07/27/2022	Grants Pass Water Lab, Inc.	July 08, 2022 - Boat Wash System - Before testing 1200-Z Storm Water & 07/13/2022 - Icehouse Dr...		792.00
Bill Pmt -Check	11094	07/27/2022	Harbor Water District P.U.D.	06/22/2022 - 07/20/2022 SERVICE/WATER BILL		1,855.84
Bill Pmt -Check	11095	07/27/2022	John Kellum/John's Portable Welding	07/19/2022 - Weld & repair Handrail on N&O Dock Ramp		112.50
Bill Pmt -Check	11096	07/27/2022	Rogue Credit Union	Membership #306 Acct#6000189521 CC Ending#7681		6,365.34
Bill Pmt -Check	11098	07/27/2022	Gold Beach Lumber Yard, Inc.	Account #776 Hardware Supplies & Materials		985.19
Total 10103 · General Funds Ckg Umpqua 3634					0.00	317,070.31
Total 101 · GENERAL FUND CHECKING & LGIP					0.00	317,070.31
10101 · Petty Cash						

**Port of Brookings Harbor  
Check Registers**

As of July 31, 2022

Cash Basis

Type	Num	Date	Name	Memo	Debit	Credit
Total 10101 · Petty Cash						
Total 100 · UNRESTRICTED CASH & EQUIVALENTS					0.00	317,070.31
<b>110 · RESTRICTED CASH &amp; EQUIVALENTS</b>						
<b>104 · RESTRICTED MONEY MKT &amp; CHECKING</b>						
20104 · USDA BOND Umpqua MM 9529						
Total 20104 · USDA BOND Umpqua MM 9529						
<b>30104 · Debt Service Umpqua MM 8627</b>						
<b>60104 · OR FFC 2020 Debt Service</b>						
Check	DEBIT	07/15/2022	Umpqua Bank/OR FFC Agreement 2020	OR FFC Agreement 2020 Payment #24		4,809.87
General Journal	DEBT 07/01	07/01/2022		Transfer to Debt Serv. RV Park for Umpqua Bank Loan Acct#97748040835 Payment	4,809.87	
Total 60104 · OR FFC 2020 Debt Service					4,809.87	4,809.87
<b>30104 · Debt Service Umpqua MM 8627 - Other</b>						
Check	DEBIT	07/15/2022	Umpqua Bank/Loan#747041620	Genie Reach Forklift Loan#747041620 Payment #53		1,464.71
Check	DEBIT	07/22/2022	m2 Lease LLC	Customer #107104 Loan#110561 Pmt #89 - 50 BFMII Travelift		4,659.00
General Journal	DEBT 07/01	07/01/2022		Transfer to Debt Service Fund for Travelift Payment	4,659.00	
General Journal	DEBT 07/01	07/01/2022		Transfer to Debt Service Fund for Fork Lift Payment	1,464.71	
Total 30104 · Debt Service Umpqua MM 8627 - Other					6,123.71	6,123.71
Total 30104 · Debt Service Umpqua MM 8627					10,933.58	10,933.58
<b>40104 · Capital Projects Umpqua 8018</b>						
<b>40104.1 · Government Funds</b>						
General Journal	CP 07/08	07/08/2022		Transfer to Capital Projects for payment to EMC Engineering inv#91009-2188	5,100.00	
Bill Pmt -Check	425	07/08/2022	EMC-Engineers/Scientists, LLC	51 Engineering Hrs- PW162-1 FEMA 4432 & 4452		5,100.00
Total 40104.1 · Government Funds					5,100.00	5,100.00
<b>40104 · Capital Projects Umpqua 8018 - Other</b>						
Total 40104 · Capital Projects Umpqua 8018 - Other						
Total 40104 · Capital Projects Umpqua 8018					5,100.00	5,100.00
Total 104 · RESTRICTED MONEY MKT & CHECKING					16,033.58	16,033.58
Total 110 · RESTRICTED CASH & EQUIVALENTS					16,033.58	16,033.58
<b>TOTAL</b>					<b>16,033.58</b>	<b>333,103.89</b>

**Port of Brookings Harbor  
Purchases by Vendor Summary**

Cash Basis

January through July 2022

	<u>Jan - Jul 22</u>
5-R Excavation, LLC	5,435.07
Adobe	89.94
ADP	3,705.89
Amazon Capital Services	5,477.90
Amber Espinoza	585.00
Anchor Lock & Key	110.00
Aquarius Environmental, LLC	7,220.00
Asurion Wireless Insurance	19.00
Beautiful Blinds Shutters & Shades	835.00
BI-MART	420.79
Billeter Marine, LLC-vendor	1,546.50
Black & Rice LLP	817.00
Boardwalk Mail Service	117.00
Boat Launch Kiosk	120.00
Boat Shop & More LLC	920.00
BoatU.S. Boat Graphics & Lettering	217.15
BOLI PWR	250.00
BoomTech	77.47
Brookings Glass, Inc.	1,996.00
Brookings Harbor Chamber of Commerce	450.00
C Bar C Truck Sales	42.95
Cascade Auto Recycling, LLC	486.40
Chetco Automotive	97.36
Chevron Business Card	5,673.62
CHEVRON/Shell	60.73
ComplianceSigns.com	634.00
Coos-Curry Electric Cooperative, Inc.	67,076.79
Costco	104.47
Country Media, Inc.	1,015.05
Crescent ACE Hardware	564.27
Crescent City Harbor District	1,000.00
Crow/Clay & Associates, Inc	1,778.15
Curry County Business License	102.50
Curry County Community Development	205.00
Curry County Road Department	250.00
Curry Equipment	898.85
Curry Transfer & Recycling	37,806.68
Del-Cur Supply Co-op	7,266.23
Desi's Tree Trimming	1,650.00
DF Supply, Inc.	2,646.81
Dish Network	4,165.56
DMV2U/Dept. of Transportation	0.35
Eaton Corporation	339.40
EBay	34.74
Edwards Roofing	7,822.00
Elavon	15,910.53
EMC-Engineers/Scientists, LLC	123,880.45
Englund Marine Supply Co.	98.14
Fastenal Industrial Supplies	14,981.79
Ferguson Enterprises, Inc.	473.78
Firefly Reservations	1,393.00
Flags.com	924.00
Forte Clothing Company	2,143.00
Four Aces Security Solutions LLC	13,905.00
FRED MEYER	114.90
Freeman Rock, Inc.	1,709.34
Gaylord Klinefelter Contracting	1,880.00
George's Auto & Diesel Electric	57.25



# Port of Brookings Harbor Purchases by Vendor Summary

January through July 2022

Cash Basis

	Jan - Jul 22
Gerald W. Burns, CPA	3,000.00
Gold Beach Lumber Yard, Inc.	19,495.54
Gowman Electric, Inc.	1,945.34
Grainger	2,701.13
Grants Pass Water Lab, Inc.	5,752.00
Grating Pacific, LLC	1,132.00
Harbor Logging Supply, Inc.	3,358.53
Harbor Sanitary District	26,918.29
Harbor View Windows, Heating & Air	460.25
Harbor Water District P.U.D.	13,498.36
Hartwick Automotive	60.99
HD SUPPLY FACILITIES	814.08
Highway Specialities, LLC	1,809.60
Home Depot	685.86
homesquare	417.98
Honeybee Bakery	29.61
In-Motion Graphics and Design, LLC	528.00
Industrial Steel & Supply Co. Inc.	332.57
Intuit	1,149.99
John Kellum/John's Portable Welding	4,050.00
K&K Insurance Group, Inc.	400.00
Kaman Industrial Technologies	2,399.92
Kendrick Equipment USA LLC	3,797.19
Les Schwab Tire Center	208.98
Lithia Ford of Klamath Falls	46,441.32
Mascott Equipment	932.74
McLennan Excavation, Inc.	113,077.55
Metro Media	455.00
Miller Nash LLP	56,708.50
Motion Industries	1,404.21
My Parking Permit	416.50
NAPA Auto Part	567.72
NorLift of Oregon, Inc.	217.15
Northwest Pump & Equipment Co.	8,145.35
Office Depot	38.38
Oil Can Henry's	14.00
ONLINE Purchases	2,150.47
Orcal Security Consulting LLC	5,649.53
Oregon Alarm	7,525.00
Oregon Coast Magazine	675.00
Oregon Department of Agriculture	278.00
Pacific Office Automation	2,089.93
Pacific Rim Copy Center	1,127.00
Pape Material Handling	4,453.47
Pitney Bowes Global Lease	846.18
Pitney Bowes, Inc.	1,013.04
Platt	916.47
Pressure Washers Direct	49.99
Pump Pipe & Tank Services, LLC	4,524.02
Quill Corporation	1,240.67
Rentprep Enterprise/Fidelis Screening	498.75
RiteAid	14.38
SDAO Spec. Dist. Assoc. OR - Healthcare	72,520.70
SDAO Spec. Dist. Assoc. OR - Prop & Cas	73,446.10
SDAO Spec. Dist. Assoc. OR - Workers Comp	20,639.83
SimpliSafe	89.94
SmartSign	1,771.89
Spectrum Business 8752 19 060 0025169	731.54

**Port of Brookings Harbor  
Purchases by Vendor Summary**

Cash Basis

January through July 2022

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	<u>Jan - Jul 22</u>
Spectrum Business 8752 19 060 0226494	759.86
Spectrum Business 8752 19 060 0247029	799.86
Spectrum Business 8752 19 060 0251369	825.79
Stadelman Electric, Inc.	3,345.10
Strahm's Sealcoat & Striping, Inc.	335.00
Suburban Propane	515.36
SUPPLYHOUSE.COM	1,495.02
T. George Podell & Co., Inc./Hot And Mighty	2,159.44
Thermo Fluids, Inc.	1,136.22
Tidewater Contractors, Inc.	21,947.60
Traffic Safety Supply Co.	2,075.40
Travel Information Council	358.00
Tyree Oil, Inc	624,060.21
U Printing	234.95
ULine	2,000.44
United Rentals, Inc.	1,105.00
US Bank Equipment Finance	1,562.40
US Postal Service	198.00
US Relay/HD Relay	693.00
Valvoline	246.46
Ventek International	2,070.00
VERIZON WIRELESS	2,351.48
Vonage	1,643.41
WeatherTech	69.95
WEEBLY-CHARGE.COM	910.00
Wes' Towing	375.00
Ziply Fiber 541-412-7930-102902-5	273.79
Ziply Fiber 541-469-5867-121516-5	446.11
Zoom Video Communications Inc.	104.93
Zoro	124.81
<b>TOTAL</b>	<b><u>1,549,441.92</u></b>

## 2022 Commissioner Meeting Review

#	Meeting Date	Action Item	Information Item	Commission Vote Approve / Fail / Hold	Notes	
1	Tuesday, January 11, 2022		Non-Moorage Charter Fees			
2			Port Best Management Practices Amendment			
3				Oregon State Marine Board Maintenance Assistance Grant (MAG) Grant Application		
4				ODEQ Tier 2 Corrective Actions and Notification to Gear Storage Users		
5				Pelican Bay Arts Association Request for 5-year Agreement		
6				POBH Employee Handbook 2022		
7				Business Oregon FEMA Matching for DR-4432 and DR-4452		
8				Sale of Business – Boulder Fresh Crab Consent to Assignment and Assumption of Lease		
9				North Jetty Access		
10				Stormwater Test Results for December 13, 2021		
11				Boardwalk Condition and Modifications		
12				Fuel Dock – Fuel Tank Control Box Repair and Protective Structure		
13				South Coast Credit Accounts		
14				Vessel Miss Stacey		
15				Financial Consultant Contract		
16				Curry County Sheriff Substation Office MOU		
17				Zola's on the Water Late-Night Activities		
18				SDAO Annual Conference 2022		
19				Blue Fin Realty Lease Renewal Amendment No. 1		
20				Hallmark Receiving Dock Condition		
21				4th of the July Fireworks		
22				Basin 1 Storm Damage to Vessels		
23				Mountain View Custom Cycles LLC and Rebel Ink Tattoo Studio LLC and Barber Shop Lease		
24				Tidewinds Sportfishing Request for Signage Space		
25				2022 SDIS Property / Casualty Insurance Renewal and Longevity Credit and Rate Lock Guarantee		
26	Wednesday, January 19, 2022	Best Management Practices Amendment		Approved		
27			Oregon State Marine Board Maintenance Assistance Grant (MAG) Grant Ap	Approved		
28			Notification to Gear Storage Users	Approved		
29			Pelican Bay Arts Association Request for 5-year Agreement	Approved		
30			POBH Employee Handbook 2022	Hold	Under Port Legal Counsel Review	
31			Sale of Business Boulder Crab Shack Consent to Assign. and Assum. of Lease	Approved		
32			North Jetty Access and Crab Dock Removal	Approved	Close Jetty Access and remove dock when project warrants	
33			Boardwalk Condition and Modification	Approved	Separate damage section and extend handrailing	
34			CBN Enterprises	Approved	Allow Southern Oregon Credit Services / Collect Northwest to proceed with litigation	
35			Financial Consultant Contract	Approved		
36			Blue Fin Realty Lease Renewal Amendment No. 1	Approved		
37			Vessel Miss Stacey Update	Approved	Placing Lien on vessel, crab pots, gear and crab pot permit	
38				Budget Calendar for Fiscal Year 2022-23		
39				Tsunami January 15, 2022, Update		

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## 2022 Commissioner Meeting Review

#	Meeting Date	Action Item	Information Item	Commission Vote Approve / Fail / Hold	Notes
40	Friday, January 28, 2022	DEQ Tier 2 Corrective Action Report		Approved	
41			Moorage License Agreement – Recommended Revisions		
42	Wednesday, February 16, 2022	Non-Moorage Charter Fees		Approved	
43		Budget Officer Appointment		Approved	Approved Kim Boom
44			Next Workshop Meeting Date		
45	Thursday, February 24, 2022		Vessel Miss Stacey		
46			Derelict Vessels		
47			Moorage License Agreement Revisions		
48			Port Rates July 1, 2022, to June 30, 2023		
49			Boat Yard Warehouse Condition		
50			Budget Committee Applications – Selection Process		
51			Fuel Tank Control Box Protective Structure		
52			2022 Events at the Port		
53			RV Park Project Delay		
54			Delinquent Account Write Off Request		
55			Congressman DeFazio Earmark for WWTP Update		
56	Wednesday, March 16, 2022	Moorage License Agreement Revisions		Hold	Reviewing live-aboard and marine survey
57		Budget Committee Selection		Approved	Filled 3 vacancies
58		Delinquent Account Write Off Request		Approved	
59		Boat & Trailer Storage Area(s)		Approved	
60		Port Rates July 31, 2022 to June 30, 2023		Approved	
61		Commercial Fillet Station		Approved	
62		Port Manager Employment Agreement Amendment No. 2		Approved	
63			FEMA Project Update		
64			2022 Salmon Season Update		
65			Vessel Miss Stacey Update		
66			Stormwater Test Results		
67			Wastewater Treatment Plant		
68	Thursday, March 24, 2022	Special District Insurance Services Employee Health	Care Plan Renewal	Approved	
69		Budget Committee Members		Approved	Filled 1 vacancy
70		FEMA Project Preliminary Drawings & Update		Approved	
71	Monday, April 4, 2022	Wastewater Treatment Plant Information		Motions failed	
72			Stout Mountain Railway Proposal		
73			Tidewinds Sportfishing Signage Request		
74			POBH Employee Handbook 2022		
75			Boat Yard Warehouse Condition		
76			Hallmark Dock Condition		
77			Supplemental Budget		
78			SCKS Consent to Assignment		
79			Business Oregon FEMA Matching Project L22009 Contract		
80			Business Oregon FEMA Matching Project L22008 Contract		
81			Live-Aboard Policy Update		
82			Moorage License Agreement Revisions		

2022 Commissioner Meeting Review

#	Meeting Date	Action Item	Information Item	Commission Vote Approve / Fail / Hold	Notes
83			Non-Moorage Charter Boat Launch Fee		
84			Curry County Tourism and Promotions Committee		
85			MOU - Port and Curry County Sheriff Department		
86	Wednesday, April 20, 2022	Stout Mountain Railway Proposal		Fail	Possible at other Port areas
87		Tidewind Sportfishing Signage Request		Approved	Angle existing sign and add sign
88		POBH Employee Handbook 2022		Approved	
89		First Supplemental Budget FY 2021-22		Approved	
90		SCKS Consent to Assignment		Approved	
91		Business Oregon FEMA Matching Project L22009 Contract		Approved	
92		Business Oregon FEMA Matching Project L22008 Contract		Approved	
93		Moorage License Agreement Revisions		Approved	
94		Non-Moorage Charter and Guide Boat Launch Fee		Approved	Launch fee not included
95		MOU Curry County Sheriff Substation		Approved	
96		Live-Aboard Policy Revisions		Approved	
97		C.J. Huntsman Engagement Letter		Approved	
98		Wastewater Treatment Plant Timeline		Approved	EMC Engineering to start on design
99		RV Park Septic Tank on Drawing Clarification		Approved	Connect sewer to Harbor Sanitary
100		Commissioner and Staff Relations		Approved	
101			Charter and Guide Boat Sign Concept		
102			RV Park Change Order		
103			USACE Maintenance Dredging		
104			Fish Cleaning Building Repairs		
105	Friday, May 6, 2022	RV Park Change Order and Payment Request		Approved	C.O., time extension, payment
106		Crab Dock		Approved	Keep crab dock
107		SDAO Insurance Claim - Replacement of Broken Dock Pile		Approved	Repair pile and prepare contract
108			Commissioner and Staff Communications and Relations		
109	Tuesday, May 10, 2022	Budget Committee Meeting - FY 2022-2023 Budget Presentation		Approved	
110	Wednesday, May 18, 2022	Billeter Marine Public Improvement Contract			
111		Boat Yard Warehouse Engineering Report			
112		Richard Cortez Delinquent Account Write Off Request			
113		Charles Case Delinquent Account Write Off Request			
114		Charter and Guide Boat Sign			
115			April Stormwater Test Results and Tier 1 Report		
116	Wednesday, June 15, 2022	Budget Hearing		Approved	
117	Wednesday, June 15, 2022	Vessel and/or Trailer Storage Agreement		Approved	
118		Vessel Miss Stacey Moorage Renewal		Approved	
119		Charters and Guides Sign Agreement Form		Approved	
120			BOEM Wind Energy Farm Off the Coast of Brookings Oregon		
121			Pacific Seafood Request for Dock Hoist		
122			Zola's on the Water Concrete Patio outside Leased Premises		
123			Cable TV and Wi-Fi at Beachfront RV Park		
124			USDA Civil Rights Compliance Review & Response		
125			Boat Yard Building(s) and Port Office Proposal		
126			RV Park New Fence Dividers		

## 2022 Commissioner Meeting Review

#	Meeting Date	Action Item	Information Item	Commission Vote Approve / Fail / Hold	Notes
127			Summer Food Dine-In Bus Route		
128			Travel Lift Ramp Sediment Impacts		
129			Beach Cam for Website		
130	Tuesday, June 21, 2022	Approval of Resolution No. 2022-07 Regarding Offshore Wind		Approved	
131	Wednesday, July 20, 2022	Boat Yard Building Plan		Approved	To acquire second structural analysis of existing building and to bring it up to code
132		Zola's on the Water Lease Amendment No. 2		Approved	
133			Curry County Storm Drain Master Plan Draft April 2022 Review – Curry County Commissioners and Port Commissioners Meeting Date		
134			Dog Leash Law Enforcement		
135			Small Debris Left Behind on the Jetty from Fireworks Show		
136			Boat Ramp and Boat Parking Area		

30

## **ACTION ITEM – A**

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**DATE:** August 17, 2022  
**RE:** US Coast Guard Dock Lease Renewal Agreement Approval  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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### OVERVIEW

- The US Coast Guard leases 150 feet of Port owned dock at their facility. The lease is typically renewed every four years.
- This past year the government upgraded their bill paying system and it delayed their payment to the port (nearly a full year). Before the port can be paid, the lease renewal needs to be completed.
- This lease includes a 4.5% increase to the previous lease rate and this agreement allows for future CPI increases.

### DOCUMENTS

- Draft US Coast Guard Dock Lease No. HSCG89-22-1-0085, 8 pages

### COMMISSIONERS ACTION

- **Recommended Motion:**  
Motion to approve US Coast Guard Dock Lease No. HSCG89-22-1-0085.

**U. S. GOVERNMENT LEASE FOR REAL PROPERTY**

DATE OF LEASE: October 1, 2021

LEASE NO. HSCG89-22-1-0085

**PURPOSE and AUTHORITY.** Pursuant to specific Congressional appropriations and 14 U.S.C. §§ 2, 88, and 89, the United States Coast Guard has statutory authority to enforce federal laws and render aid to distressed persons, vessels, and aircraft on and under the Pacific Ocean and other waters over which the United States has jurisdiction. The parties understand that the sole purpose of this lease is to secure for the United States uninterrupted use of adequate ATON and cable space for USCG vessels.

**THEREFORE THIS LEASE,** made and entered into this date by and between **Port of Brookings Harbor**, whose address is **P.O. Box 848, Brookings, Oregon 97415** and whose interest in the property hereinafter described is that of **Owner/Lessor**, and the Commanding Officer, Civil Engineering Unit, Oakland, CA, on behalf of the **UNITED STATES OF AMERICA (Lessee)**, hereinafter called the "Government," pursuant to 14 U.S.C. § 907/908, for the consideration hereinafter mentioned.

**WITNESSETH:** The parties hereto for the considerations hereinafter mentioned, covenant and agree as follows:

1. **Space:** The Lessor hereby leases to the Government the following described premises: Six (6) foot by one hundred fifty-two (152) foot concrete mooring float, known as float "C" located in Lessor's Brookings, Oregon mooring basin as shown on Exhibit "A" attached and made a part hereof.

2. **Term: TO HAVE AND TO HOLD** the said premises with their appurtenances for the term beginning on October 1, 2021 and ending on September 30, 2022, subject to termination and renewal rights as may be hereinafter set forth.

3. **Rent:** The Government shall pay the Lessor total annual rent of \$11,580 in arrears, at the rate of \$965.00 per month in arrears, provided annual appropriations are granted by Congress. For each subsequent renewal year, the annual rent will be calculated based on the increase (over the immediate previous year) in the U.S. Department of Labor Consumer Price Index for All Urban Consumers - West Region. Electronic (ACH) rent payments shall be made payable to Lessor as explained in Paragraph 25.

4. **Termination:** The Government may terminate this lease at any time, without cause and without cost to the Government, by giving at least sixty (60) calendar-days' notice in writing to the Lessor and no rental shall accrue after the effective date of termination. Said notice to terminate shall be computed commencing with the day after the date of mailing.

5. **Renewal Options:** This lease may be renewed at the option of the Government, for the following terms and at the following rentals: Four (4) successive terms of one year occurring over the same time period (October 1 to September 30) in successive years. All other terms and conditions of this lease shall remain the same during any renewal term, and provided appropriations are granted by Congress. Notice to exercise an option must be given in writing to the Lessor at least thirty (30) days before the end of the term preceding the upcoming lease term or any subsequent renewal term, to be set as no later than August 30<sup>th</sup> of the then current lease year. Said notice shall be computed commencing with the day after the date of mailing.



**6. Additional Responsibility.**

- a. The government shall have the right to at USCG expense to install any security measures the Government deems necessary on USCG exclusive use areas, at USCG expense.
- b. Lessor will notify the Government, in writing, within thirty (30) days of any:
  - 1) Transfer of ownership of the described premises
  - 2) Change of payments, mailing address or ACH payments.

**7. Successors Bound:** The Lease shall bind, and inure to the benefit of, the parties and their respective heirs, executors, administrators, successors, and assigns.

**8. Subletting/Assignment:** Government may not sublet or assign lease premises without the prior written consent of the Lessor, which consent shall not be unreasonably withheld.

**9. Environmental Protection:** Government may not unlawfully pollute the air, ground or water, nor create a public nuisance. Lessee shall, at no cost to the Lessor, promptly comply with all applicable Federal, state, and local laws, regulations, and directives regulating the quality of the environment. This does not affect the Government's right to contest the validity of such laws, regulations or directives or to try to enjoin their applicability. Government shall use all required means to protect the environment and natural resources from any damage arising from Lessee's use of the facility and activities incident to its use. If any damage results to the environment or natural resources, Government shall restore the environment or damaged resources. Government agrees to comply with all applicable federal, state and local environmental laws and regulations, including, but not limited, to those laws concerning any petroleum products, toxic substances, medical wastes and hazardous materials, substances or wastes.

**10. Anti-Deficiency Act:** (31 USC §1341 as amended). Nothing in this Lease shall constitute an obligation of funds of the United States in advance of appropriation thereof.

**11. Government Liability:** The Government, in the manner and to the extent provided by the Federal Tort Claims Act (28 USC Sections 2671-2680 as amended) shall be liable for, and shall hold Lessor harmless from, claims for damage or loss of property, personal injury or death caused by the acts or omissions of the Government, its officers, employees and agents in the use of the leased premises.

**12. Insurance:** Government is a sovereign/self-insured entity and as such is not required to provide Lessor with any certificate of insurance or notice of renewal, termination, cancellation, expiration or alteration of insurance policy.

**13. Ownership of Improvements:** It is understood and agreed that any improvements added by the Government belong to the Government.

**14. Officials Not to Benefit:** No member or delegate to Congress or Resident Commissioner shall be admitted to any share or part of this Lease or any benefit to arise there from, but this provision shall not be construed to extend to this Lease if made with a corporation whose membership, includes a member or delegate to Congress or Resident Commissioner, who indirectly receives a general benefit from this Lease.

15. **Amendment or Modification:** No amendment or modification shall be valid unless evidenced by an agreement in writing signed by both parties.

16. **Governing Law:** The parties shall construe the Lease to be in accordance with and governed by the laws of the State of Oregon, insofar as those laws are consistent with applicable federal laws and regulations

17. **Severability:** If any term or provision of this Lease is held invalid or unenforceable, the remainder of this Lease shall not be affected thereby and each term and provision hereof shall be valid and enforced to the fullest extent permitted by law.

18. **Payments of Taxes and Assessments:** The Government is not responsible for or liable for the payment of any real property taxes, personal taxes or assessments levied or assessed upon or against the leased premises.

19. **Entire Agreement:** This Lease, with attachments, constitutes the only agreement between Lessor and Government. Any prior understanding or representation of any kind, which proceeded the date of this Lease, are not binding on either party, except to the extent the understandings are incorporated into this Lease.

20. **Mutual Authority:** Lessor and Government represent and warrant to each other that they have full right, power and authority to enter into this Lease without the consent or approval of any other entity or person and make these representations knowing that the other party will rely thereon. Furthermore, the signatories on behalf of Lessor and Government further represent and warrant that they have full right, power and authority to act for and on behalf of Lessor and Government in entering into this Lease.

21. **Attorney Fees:** To the extent not in conflict with federal law, should any dispute arise between the parties hereto or their legal representatives, successors and assigns concerning any provision of this Lease or the rights and duties of any person in relation thereto, the party prevailing in such dispute shall be entitled, in addition to such other relief that may be granted, to recover reasonable attorneys' fees and legal costs in connection with such dispute as determined by the judgment or award of any court or tribunal of competent jurisdiction.

22. **Payment Process:**

The following office will process lease payments:

Commanding Officer (c/o Leases)  
US Coast Guard Finance Center (Team 2A)  
1430A Kristina Way  
Chesapeake, VA 23326-1728  
Customer Service (800) 564-5504 or (757) 523-6940

23. **Lease Administration:**

The following office shall administer this Lease:

Commanding Officer  
USCG Civil Engineering Unit Oakland

Asset Management & Planning (rp)  
1301 Clay Street, Suite 700N  
Oakland, CA 94612-5203

24. **Tax Identification:** Government regulations require a SAM Unique Entity ID (UEI). Lessor hereby certifies that the following SAM UEI and telephone number are correct:

SAM Unique Entity ID: ZE7HXF6P4Y54

Telephone Number: (541) 469-2218

07/20/2022  
Date Signature

Gary Dehlinger, Port Manager  
Printed Name and Title

E-mail accounts@portofbrookingsharbor.com

25. Method of Payment:

1) PAYMENT BY ELECTRONIC FUNDS TRANSFER- SYSTEM FOR AWARD  
MANAGEMENT:

2) In the event the Government is unable to release one or more payments by EFT, the Lessor agrees to either accept payment by check or some other mutually agreeable method of payment or request the Government extend the payment due date until such time as the Government can make payment by EFT (see paragraph (d) of this clause).

b. Lessor EFT information. The Government shall make payment to the Lessor using the EFT information contained in the System for Award Management (SAM) database. In the event that the EFT information changes, the Lessor shall be responsible for providing the updated SAM database.

c. Mechanisms for EFT payment. The Government may make payments by EFT through either the Automated Clearing House (ACH) network, subject to the rules of the National Automated Clearing House Association, or Fed-wire Transfer System. The rules governing Federal payments through the ACH are contained in 31 CFR part 210.

d. Suspension of payment. If the Lessor EFT information in the SAM database is incorrect, then the Government need not make payment to the Lessor under this contract until correct EFT information is entered into the SAM database; and any invoice or contract financing request shall be deemed not to be proper invoice for the purpose of prompt payment under this contract. The prompt payment terms of the contract regarding notice of an improper invoice and delays in accrual of interest penalties apply.

e. Lessor EFT arrangements. If the Lessor has identified multiple payment receiving points (i.e., more than one remittance address and/or EFT information set) in the SAM database, and the Lessor has not notified the Government of the payment receiving point applicable to this contract, the Government shall make payment to the first payment receiving point (EFT information set or remittance address is applicable) listed in the SAM database.

f. Liability for incomplete or erroneous transfer:

1) If an incomplete or erroneous transfer occurs because of Government used the Lessor's EFT information incorrectly, the Government remains responsible for making a correct payment, paying any prompt payment penalty due and recovering any erroneously directed funds.

2) If any incomplete or erroneous transfer occurs because the Lessor's EFT information was incorrect, or was revised within 30 days of Government release of the EFT payment transaction instruction to the Federal Reserve System, and if funds are no longer under the control of the payment office, the Government is deemed to have made payment and the Lessor is responsible for recovery of an erroneously directed funds or if the funds remain under the control of payment office, the Government shall not make payment, and the provisions of paragraph (d) of this clause shall apply.

g. EFT and prompt payment. A payment shall be deemed to have been made in a timely manner in accordance with the prompt payment terms of this contract if, in the EFT payment transaction instruction released to the Federal Reserve System, the date specified for settlement of the payment is on or before the prompt payment due date, provided the specified payment date is a valid date under the rules of the Federal Reserve System.

h. EFT and assignment claims. If the Lessor assigns the proceeds of this contract as provided for in the assignment of claims terms of this contract, the Lessor shall require as a condition of any such assignment, that the assignee shall register in the SAM database and shall be paid by EFT in accordance with the terms of this clause. In all respects, the requirements of this clause shall apply to the assignee as if it were the Lessor. EFT information that shows the ultimate recipient of the transfer to be other than the Lessor, in the absence of proper assignment of claims acceptable to the Government, is incorrect EFT information within the meaning of paragraph (d) of this clause.

i. Liability for change to EFT information by financial agent. The Government is not liable for errors resulting from changes to EFT information made by the Lessor's financial agent.

j. Payment information. The payment or disbursing office shall forward to the Lessor available payment information that I suitable for transmission as of the date of release of EFT instruction to the Federal Reserve System. The Government may request the Lessor to designate a desired format and method(s) for delivery of payment information from a list of formats and methods the payment office is capable of executing. However, the Government does not guarantee that any particular format or method of delivery is available at any particular payment office and retains the latitude to use the format and delivery method most convenient to the Government. If the Government makes payment by check in accordance with paragraph (a) of this clause, the Government shall mail the payment information to the remittance address contained in the SAM database.

capable of executing. However, the Government does not guarantee that any particular format or method of delivery is available at any particular payment office and retains the latitude to use the format and delivery method most convenient to the Government. If the Government makes payment by check in accordance with paragraph (a) of this clause, the Government shall mail the payment information to the remittance address contained in the CCR database.

26. **CLAUSES INCORPORATED BY REFERENCE.** This lease incorporates one or more clauses by reference with the same force and effect as if they were given in full text. The full text may be found in GSA Form 3517B (Rev. 4/2015), and the following clauses are incorporated by reference:

**GSA Form 3517B**

**Clause Number 48 CFR Reference**


**Clause Title**

AND	3	552.270-23	SUBORDINATION, NON-DISTURBANCE
			ATTORNNMENT
	4	552.270-24	STATEMENT OF LEASE
	5	552.270-25	SUBSTITUTION OF TENANT AGENCY
	6	552.270-26	NO WAIVER
	8	552.270-28	MUTUALITY OF OBLIGATION
	9	--	DELIVERY AND CONDITION
	10	--	DEFAULT BY LESSOR
	14	--	COMPLIANCE WITH APPLICABLE LAW
	17	52.204-7	SYSTEM FOR AWARD MANAGEMENT
	18	52.204-13	SYSTEM FOR AWARD MANAGEMENT
MAINTENANCE			
	19	552.270-31	PROMPT PAYMENT
	20	552.232-23	ASSIGNMENT OF CLAIMS
	21		PAYMENT
	24	552.270-32	COVENANT AGAINST CONTINGENT
FEES			
IMPROPER	28	552.270-30	PRICE ADJUSTMENT FOR ILLEGAL OR
			ACTIVITY
	30	552.270-13	PROPOSALS FOR ADJUSTMENT
	31	--	CHANGES
	34	52.233-1	DISPUTES
	35	52.222-26	EQUAL OPPORTUNITY
	36	52.222-21	PROHIBITION OF SEGREGATED FACILITIES
	38	52.222-35	EQUAL OPPORTUNITY FOR VETERANS
	39	52.222-36	EQUAL OPPORTUNITY FOR WORKERS WITH DISABILITIES

27. **New Lease:** This is a succeeding Lease between Lessor and Lessee and succeeds HSCG89-16-1-0096, a prior lease.

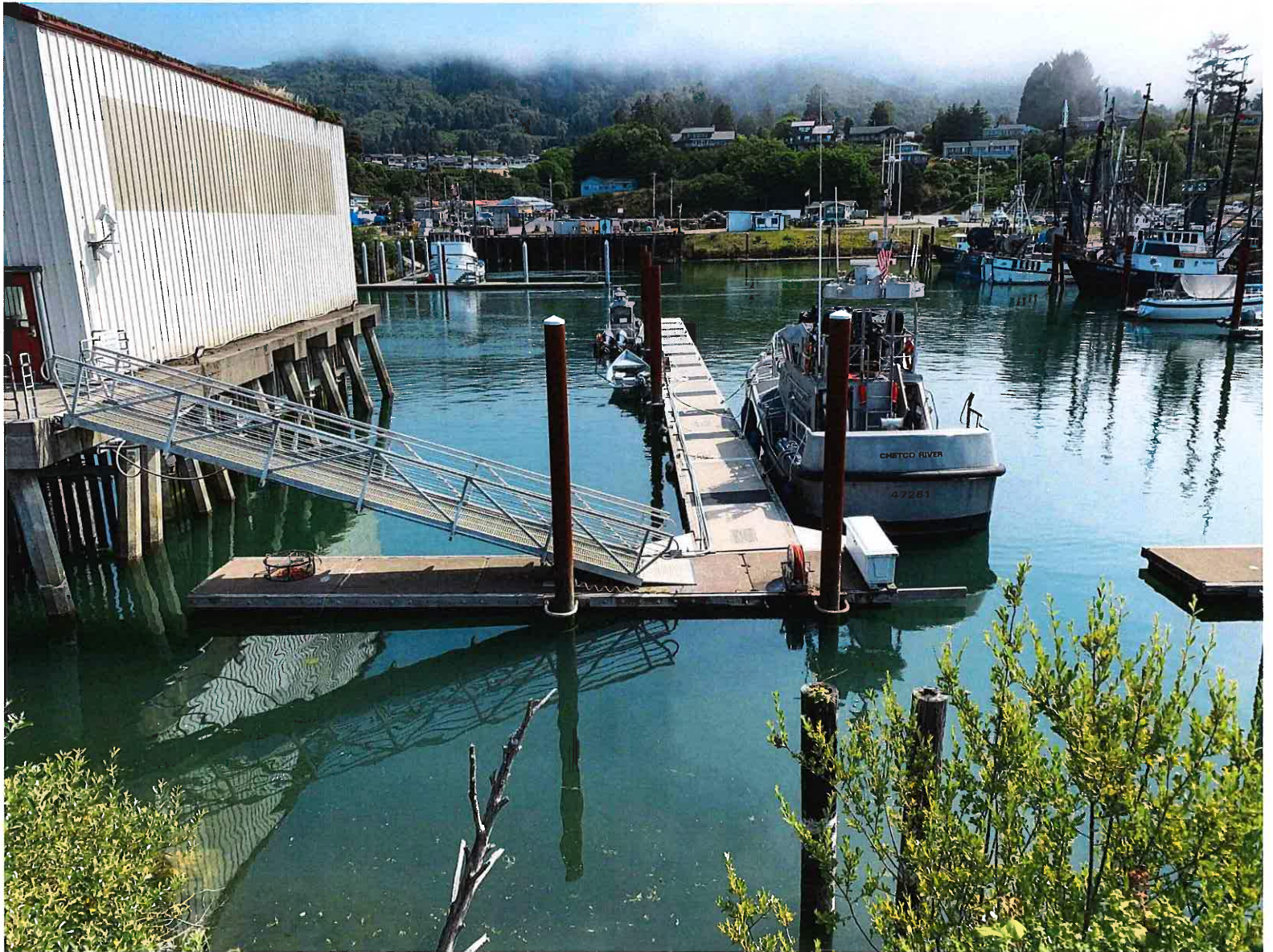
//////Last Item- Signatures Follow//////

IN WITNESS WHEREOF, the parties hereto have hereunto subscribed their names as of the date first above written.

<b>SIGNATURE</b>	<b>SIGNATURE</b>
<b>NAME OF SIGNER</b>	<b>NAME OF SIGNER</b>
<b>IN PRESENCE OF</b>	
<b>SIGNATURE</b>	<b>SIGNATURE</b>
<b>UNITED STATES OF AMERICA</b>	
<b>SIGNATURE</b>  Real Estate Contracting Officer U.S. Coast Guard	<b>NAME OF SIGNER</b> Loren David Mollner <hr/> <b>OFFICIAL TITLE OF SIGNER</b> Real Estate Contracting Officer United States Coast Guard
2022.07.20 09:33:09 -07'00'	

**Exhibit "A" Pier photos**

Image of USCG A





## ACTION ITEM – B

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**DATE:** August 17, 2022  
**RE:** Southern Oregon Credit Service Request for Small Claims Approval  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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### OVERVIEW

- Southern Oregon Credit Services / Collect Northwest is requesting permission to proceed with litigation on John Hartt to collect the remaining debt on the account.
- This account was sent to collections with Board approval on June 15, 2021. Principal balance on this account is \$3,601.00. John Hartt moorage with the port had expired and he failed renew and pay for moorage while the vessel remained in the slip. Then he failed to remove the vessel from the port.

### DOCUMENTS

- None

### COMMISSIONERS ACTION

- **Recommended Motion:**  
Motion to approve Southern Oregon Credit Services / Collect Northwest to proceed with litigation on John Hartt for the remaining debt owed.

# ACTION ITEM – C

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**DATE:** August 17, 2022  
**RE:** Boat Yard Warehouse Structural Analysis and Report Approval for Expense  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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## OVERVIEW

- May 18, 2022 meeting, the board approved seeking funding for a new building and provide proposals for the warehouse for board review.
- June 15, 2022 meeting, the board reviewed the initial replacement proposal with estimated cost to install the new building(s). During this review, suggestions were made to provide more information on estimated costs, review all port debt, and list of future projects that may require loans or other additional funding.
- July 20, 2022 meeting, the board approved seeking a second structural analysis and what it would take to bring the existing building up to code.
- The Business Oregon loan application expired. If the Port wants to apply for a loan again, the process will need to start over.
- Port contacted (9) nine structural engineering or architect firms seeking the analysis of the warehouse as the board requested.
- Does the board have any other firms they have in mind or select from the firms that did respond? What are the parameters of the contract?

## DOCUMENTS

- Emails from structural engineers and architects, 10 pages

## COMMISSIONERS ACTION

- **Recommended Motion:**

**From:** Dave Morris <davemorris@mcgee-engineering.com>  
**Sent:** Friday, August 5, 2022 10:17 AM  
**To:** portmanager@portofbrookingsharbor.com  
**Cc:** Alex Dunn; Thompson, Don; Chad Franklin  
**Subject:** RE: Port of Brookings-Harbor Shop Building Review  
**Attachments:** McGee Proposal Port of B-H Shop review.pdf

Gary:

Please find attached our proposal for the review of the existing Shop Building at the Port of Brookings-Harbor.

We have included two other firms on our team. West Coast Contractors out of Coos Bay ([www.westcoastcontractors.com](http://www.westcoastcontractors.com)) has extensive experience in the construction of all types of facilities up and down the coast specifically including structural work for port facilities. They will be consulting on the overall approach and doing the cost estimates other than electrical. We recently worked with West Coast on the emergency replacement of the dock mounted ice production facility for the Port of Charleston. Reese Electric out of North Bend ([www.resseelectric.com](http://www.resseelectric.com)) was the electrical contractor for that job. They also have decades of experience working on the coast on all kinds of projects. Reese Electric will be providing the electrical review and cost estimate.

We have included an option in our proposal for you to end the study early if the initial investigation shows that there is no practical way to renovate the structure. If you chose this option we would provide a letter explaining the issues that make the renovation difficult or impossible. This would allow you to avoid the cost of the full study if it becomes clear that the structure cannot be renovated to current code.

Thank you for the opportunity to provide this proposal. Please call or email if you have any questions. I am in today. I will be out of the office next week. Monday, I will be available by email. If you have questions the rest of the week please contact Alex Dunn at our office.

**Dave Morris, S.E.**  
*Structural Engineer*



**Office** [\(541\) 757-1270](tel:(541)757-1270)  
**Cell** [\(541\) 760-8270](tel:(541)760-8270)  
**Direct** [\(541\) 241-9574](tel:(541)241-9574)  
**Fax** [\(541\) 758-6585](tel:(541)758-6585)  
[www.mcgee-engineering.com](http://www.mcgee-engineering.com)

**Address:** 804 D NW Buchanan Ave. Corvallis, OR 97330  
**Mailing:** P.O. Box 1067 Corvallis, OR 97339



**Dave Morris, P.E., S.E. Senior Structural Engineer**

Office: (541) 757-1270  
Direct: (541) 241-9574  
davemorris@mcgee-engineering.com

804 D NW Buchanan Ave. Corvallis, OR 97330  
P.O. Box 1067 Corvallis, OR 97339  
www.mcgee-engineering.com

August 5, 2022

Port of Brookings-Harbor  
16330 Lower Harbor Road  
Brookings, Oregon 97415

**ATTN:** Gary Dehlinger  
Port Manager

**RE:** Proposal for Engineering Services  
Review of Existing Shop Building  
Port of Brookings-Harbor

Gary,

Thank you for the opportunity to provide this proposal for engineering services for the evaluation of the existing shop building at the Port. We have assembled a team to assist us with this project. West Coast Contractors will be providing assistance with overall construction strategy and cost estimating. Reese Electric will be handling the electrical review and cost estimate.

**DESCRIPTION OF PROJECT:**

The port owns an existing 11,500 square foot wood framed building in the harbor area. As we understand it the building has been its current location for 50 years when it was moved there from another site. Based on our previous correspondence we understand that the goal of this review is to determine what it would take to upgrade this building to meet current building code requirements and current electrical code requirements and how much these upgrades will cost.

**SCOPE OF SERVICES:**

- 1) Site Visit – McGee, WCC and Reese will make site visits to document the building construction and condition. We would also want to meet with someone on site who is familiar with the building and could talk about any known problems that should be considered in the review.
- 2) Building Analysis – McGee will do the structural review of the existing structure to identify what areas do not comply with current code requirements.
- 3) Electrical Review – Reese will do the review of the electrical systems and identify items that need to be improved or replaced.

- 4) Cost Estimate – McGee will prepare a site plan, a floor plan and two building sections. These drawings will show the proposed structural improvements. These will not be "construction ready" drawings. They are intended only to show the primary work elements. Based on these drawings and their site visit WCC will prepare an opinion of probable cost for the building construction. Reese will prepare a list of work items along with an opinion of probable cost for the remedial electrical work. We are not proposing to provide electrical drawings.
- 5) Report – McGee will provide a final report summarizing our observations, conclusions, recommendations, and an overall opinion of probable cost.

You had indicated that there may be some site work required. One item mentioned was a sump pump. We will plan to review that system or plan for an alternate. At this point we were not planning to include any work outside the building such as site grading, parking lot layout, site ADA improvements, surveying, signing, striping, or sidewalks.

We were also not planning to include a geotechnical investigation of the site. That would provide useful information but would be a substantial additional cost for this level or review. We can add that work and its associated cost if you like. Without the Geotech report we will be making our foundation recommendations based on information we can gather from other sources. We understand that the port does not have any Geotechnical Reports for other work done around the port facility or records of excavations done in this area. We further understand that the Port does not have any existing drawings of the building or of past work done on the buildings.

**ESTIMATED COST OF SERVICES:**

TOTAL DESIGN FEES:	\$ 29,000
<u>TOTAL REIMBURSABLE EXPENSES:</u>	<u>\$ 500</u>
TOTAL FEES:	\$ 29,500

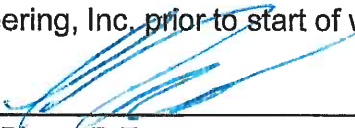
If, after the site visits, the team, in consultation with the Port, determines that there is clearly no feasible path to retrofit this building to meet current code requirements we would terminate the study at this point and provide a letter summarizing the reasons for our opinion. We would bill \$5000 for the site visits and hourly for other work after the site visits as required to prepare the summary letter, answer questions, on-line meetings and other coordination to provide the summary letter. Our hourly rates are attached. Time for West Coast Contractors and Reese Electric will be billed at \$115 per hour. If we stop here we would not provide a retrofit strategy or cost estimates. Our estimate is that fees at this point would not exceed \$10,000.

This proposal is an estimation of the work items included in this project. If in the event different items arise that are outside the scope of this contract, including meetings or revisions, they will be billed on a time and materials basis. Reimbursable expenses may include costs associated with (but not limited to) the following: printing, copies, postage, and mileage.

Thank you for the opportunity to provide a proposal for this work. Feel free to call or email with any questions.

**TERMS OF ACCEPTANCE OF PROPOSAL.**

Signing this proposal constitutes an acceptance of its terms including all prices and costs. It must be returned to McGee Engineering, Inc. prior to start of work.

Authorized Signature:  Date: 8/4/22  
Alex Dunn, P.E.

Authorized Signature: \_\_\_\_\_ Date: \_\_\_\_\_

ATTACHED: 2022 Rate Sheet



## SUMMARY OF BILLING RATES AND METHODS

Office: (541) 757-1270  
Fax: (541) 758-6585  
alexduinn@mcgee-engineering.com

804 D NW Buchanan Ave. Corvallis, OR 97330  
P.O. Box 1067 Corvallis, OR 97339  
www.mcgee-engineering.com

### *Effective January 1, 2022*

McGee Engineering, Inc, an Oregon corporation, registered in all counties in the State of Oregon.

#### **Hourly Rates:**

Engineer Grade VIII ..... \$177 per hour  
Engineer Grade VI ..... \$147 per hour  
Engineer Grade V ..... \$134 per hour  
Engineer Grade IV ..... \$122 per hour  
Engineer Grade III ..... \$90 per hour  
Engineer Grade II ..... \$82 per hour  
Technician Grade B ..... \$82 per hour  
Clerical ..... \$61 per hour  
No overtime fees added to professional fees

#### **Travel**

Automobile mileage is charged at the current federal rate. Mileage is charged from Corvallis. Plane fares and lodging are billed as project expenses.

**Project Direct Expenses** include, but are not limited to, the following invoiced costs:

Testing Fees, Freight, Plane Fares, Lodging Costs, and Parking Fees

**Overhead Expenses** are included in professional fees and are not charged separately. These overhead expenses include:

Professional and Commercial Insurance, Office Rent and Utilities, Telephone/fax expenses, Computer/software costs, Stationery supplies, Office related clerical, and copying costs

#### **Insurance Coverage:**

BROKER: Orion Insurance Group, Bothell, WA (425)771-5197, Christopher Day  
Professional Insurance - \$2,000,000 aggregate (Admiral)  
Commercial General Liability - \$2,000,000 (Travelers)  
Automobile Insurance - \$1,000,000 umbrella (Travelers)  
Workers' Compensation (SAIF)

**Professional Engineer Registrations** are held in the states of Oregon, Washington, California, Alaska, Idaho, and South Dakota.

**portmanager@portofbrookingsharbor.com**

---

**From:** J Burns <jburns@dyerpart.com>  
**Sent:** Thursday, July 21, 2022 9:19 AM  
**To:** portmanager@portofbrookingsharbor.com  
**Subject:** RE: Port of Brookings Harbor - Structure Analysis

Good Morning Gary,

Thank you for the request. Unfortunately, you will need an architect for this project. Maybe Gene Bolante with Studio 3 Architecture (503) 390-6500. Hope that helps.

Cordially,

*Mrs. Joey Burns*

The Dyer Partnership Engineers & Planners, Inc.  
1330 Teakwood Avenue  
Coos Bay, OR 97420  
(541) 269-0732 Fax: (541) 269-2044  
[jburns@dyerpart.com](mailto:jburns@dyerpart.com)

---

**From:** portmanager@portofbrookingsharbor.com <portmanager@portofbrookingsharbor.com>  
**Sent:** Thursday, July 21, 2022 6:37 AM  
**To:** Info <info@dyerpart.com>  
**Subject:** Port of Brookings Harbor - Structure Analysis

Hello,

The Port of Brookings Harbor, Board of Commissioners, have requested to get a structural analysis of our existing warehouse building and to bring it up to code, i.e., structural, electrical and possibly civil. Our existing building is well over 50 years old and has issues. The commissioners would like a total repair estimate of what it would cost to get it to today' standards. Could your company prepare this analysis and report and what would your estimated costs be for this analysis and report?

Thank you,

Gary Dehlinger  
Port of Brooking Harbor  
Cell 541-254-4162



**portmanager@portofbrookingsharbor.com**

---

**From:** Lance Martz <mt2@structure1.com>  
**Sent:** Wednesday, July 20, 2022 4:46 PM  
**To:** portmanager@portofbrookingsharbor.com  
**Subject:** RE: An Online Quote has been Submitted

Gary,

We are short-staffed and unable to do site inspections at this time.  
This inspection would take approximately 8 hours round trip plus site time.

It is too far for us to be able to be competitive with for cost, as well as not having the manpower to service your request.

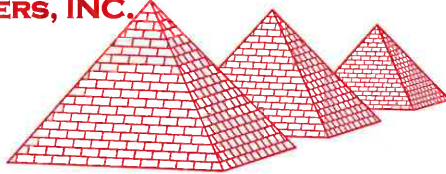
We are sorry, but we have to decline this opportunity.

**Thank you!**

**Lance Martz**  
**Business Development Manager**

**PSE CONSULTING ENGINEERS, INC.**

250 Main St., Ste. A  
Klamath Falls, OR 97601  
Ph: 541-850-6300  
Email: [mt2@structure1.com](mailto:mt2@structure1.com)  
Website: [www.structure1.com](http://www.structure1.com)



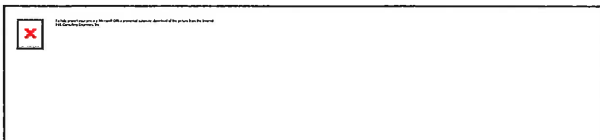
**Disclaimers:**

The information in this email and any attachments or links may contain proprietary and confidential information that is intended for the addressee(s) only. If you are not the intended recipient, you are hereby notified that any disclosure, copying, distribution, retention or use of the contents of this information is prohibited. When addressed to our clients or vendors, any information contained in this e-mail or any attachments is subject to the terms and conditions in any governing contract. If you have received this e-mail in error, please immediately contact the sender and delete the e-mail. Thank you.

The Drawings, Specifications, or other documents prepared or supplied by Precision Structural Engineering, Inc. & PSE Consulting Engineers, Inc. (PSE) for this project, whether in hard copy or electronic format, are instruments of PSE's service for one-time use solely with respect to this Project. As such, they shall be deemed the property of PSE, who shall retain all common law, statutory and other reserved rights, including copyright. No Drawings, Specifications, other documents or Data prepared or supplied by PSE may be used on this Project after PSE's involvement is completed or on any other Project without PSE's prior written consent. Any other use or reuse by the Recipient or others will be at the sole risk of the Recipient without liability or legal exposure to PSE.

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**From:** Gary Dehlinger <wordpress@www.structure1.com>  
**Sent:** Wednesday, July 20, 2022 4:41 PM  
**To:** Lance Martz <mt2@structure1.com>; MT1 <mt1@structure1.com>; webmaster226@hotmail.com; Natalie Hernandez <mt3@structure1.com>  
**Subject:** An Online Quote has been Submitted



**portmanager@portofbrookingsharbor.com**

---

**From:** brian@deiengineers.com  
**Sent:** Thursday, July 21, 2022 10:13 AM  
**To:** portmanager@portofbrookingsharbor.com  
**Subject:** RE: Port of Brookings Harbor

Gary,

You would be best to tear it down and build a new metal building on a new foundation.

I can tell you that just by looking at the pictures.

Best Regards,

Brian



Brian Dunagan, PE | *Principal Engineer*  
**Ashland:** 541.897.0021 | **Reno:** 775.329.2733 | **Cell:** 775.742.4200  
**DEIengineers.com**

**DEI Engineers southern Oregon office has moved to 106 South Market Street #2, Talent, OR 97540. Our mailing address is PO Box 796, Talent, OR 97540.**

---

**From:** portmanager@portofbrookingsharbor.com <portmanager@portofbrookingsharbor.com>  
**Sent:** Thursday, July 21, 2022 9:13 AM  
**To:** brian@deiengineers.com  
**Subject:** RE: Port of Brookings Harbor

Hi Brian,

My responses to your questions are below and attached some photos of the existing building. The building is approximately 11,500 sf.

Do you have any existing drawings so we could see what you have?

We do not have any drawings. This building was relocated to Port property over 50 years ago from a local farm, I was told. Actual age is unknown.

Is the report for you or a specific entity?

The report is for the Port of Brookings Harbor a public entity.

Do you have a format or specific type of report that has been requested?

No format requested, basically the board of commissioners would like to see a report on what it would take and cost to bring this building to code.

What is your timing?

Time is of the essence, but I need to get board approval for the expense, which may be approved in August. They will need to see your hourly rates and some sort of an estimated total cost or not to exceed amount. I am contacting other engineering firms for pricing and availability for the board to make a decision.



**Gary Dehlinger**  
*Port Manager*  
16330 Lower Harbor Road  
Brookings, Oregon 97415  
Office: (541) 541-2218, Ext 406  
Cell: (541) 254-4162

---

**From:** [brian@deiengineers.com](mailto:brian@deiengineers.com) <[brian@deiengineers.com](mailto:brian@deiengineers.com)>  
**Sent:** Thursday, July 21, 2022 8:32 AM  
**To:** [portmanager@portofbrookingsharbor.com](mailto:portmanager@portofbrookingsharbor.com)  
**Subject:** Port of Brookings Harbor

Mr. Dehlinger,

We could provide this?

Do you have any existing drawings so we could see what you have?

Is the report for you or a specific entity?

Do you have a format or specific type of report that has been requested?

What is your timing?

For something like this we would bill you hourly including travel time and mileage. We could provide an estimate but this will depend on the detail you want in the report.

Best Regards,

Brian



Brian Dunagan, PE | *Principal Engineer*  
**Ashland:** 541.897.0021 | **Reno:** 775.329.2733 | **Cell:** 775.742.4200  
**DEIengineers.com**

DEI Engineers southern Oregon office has moved to 106 South Market Street #2, Talent, OR 97540. Our mailing address is PO Box 796, Talent, OR 97540.

**portmanager@portofbrookingsharbor.com**

---

**From:** dduru@galligroup.com  
**Sent:** Thursday, July 21, 2022 10:20 AM  
**To:** portmanager@portofbrookingsharbor.com  
**Cc:** stephenc@zcsea.com; admin  
**Subject:** Re: New submission from Contact Form

Gary,

Thank you for inquiring with us on this project. As I mentioned in our phone conversation, we are a geotechnical firm and deal mostly with geological and geotechnical components of projects. However, we have worked with ZCS on numerous retrofit projects in southern Oregon and along the Oregon coast. These projects include retrofit of existing elementary and high schools, fire stations and other emergency services buildings, bringing them up to code. Below is the information for Stephen Chase (also copied on this email), one of ZCS lead designers we have worked with on most of these projects. We highly recommend them for your project.

ZCS Contact;  
Stephen Chase  
Lead Designer  
ZCS Engineering & Architecture  
Office | 541.479.3865  
127 NW D Street, Grants Pass, OR 97526

Please let me know if you need anything from the Galli Group as we enjoyed working with you in the past.

Best regards,

Dennis Duru, M.Sc., P.E., R.G., C.E.G.  
Engineering Manager

The Galli Group  
612 NW 3rd Street  
Grants Pass, OR 97526  
P: (541) 955-1611  
C: (541) 840-6046  
F: (541) 955-8150

Quoting admin@galligroup.com:

> From: Gary Dehlinger <portmanager@portofbrookingsharbor.com>  
> Sent: Thursday, July 21, 2022 6:43 AM  
> To: admin@galligroup.com  
> Subject: New submission from Contact Form  
>  
>  
>  
>  
>

## ACTION ITEM – D

---

**DATE:** August 17, 2022  
**RE:** New RV Park Fence Dividers  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

---

### OVERVIEW

- The new RV Park improvements to the front row does not include fence dividers. Fence dividers were planned to be done by the Port after construction was completed.
- There will be 28 sites that need new fence dividers. There are 10 more front row sites that will have upgrades to the power and have the old picket wood fence as dividers. Back row sites are not part of this remodel project.
- In June 2022, the Board reviewed various types of dividers with cost estimates. Port staff is recommending the concrete block wood style divider.
- If approved, concrete blocks will need to be ordered soon to have them onsite and available when the remodel construction is finished.
- Estimated cost per site fence divider is at \$850 each. Current budget has \$25,000 for this project.

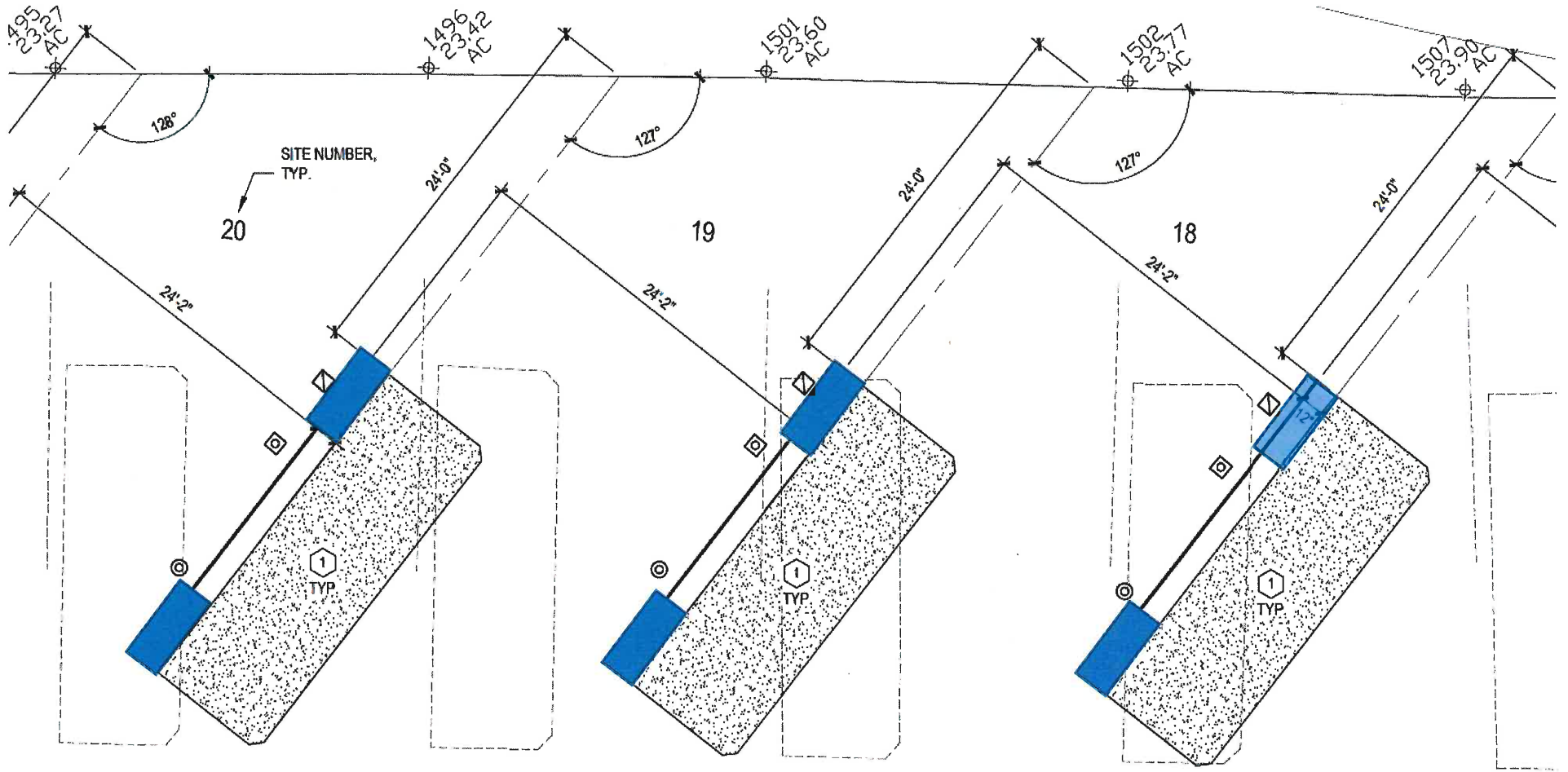
### DOCUMENTS

- Site Divider Drawings, 2 pages

### COMMISSIONERS ACTION

- **Recommended Motion:**  
Motion to approve the expenditure for new RV Park fence dividers at an estimated cost of \$23,800.

# Typical RV Park Remodeled Pull-Thru Site New Fence Divider



# New RV Park Fence Divider

Side View



Top View – Off Center



Top View - Center



Top View – Off Center



## **ACTION ITEM – E**

---

**DATE:** August 17, 2022  
**RE:** Permission to Display Bible Education Cart  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

---

### OVERVIEW

- Jeremy Watson contacted the Port requesting to re-establish the literature bible cart at the retail courtyard. Before COVID regulations, the literature bible cart was setup between Whale's Tail and Boardwalk Mail next to the main walkway on various days.
- Jeremy said they had an arrangement with management in the past, but the Port does not have anything in writing.

### DOCUMENTS

- Jeremy Watson Proposal, 5 pages

### COMMISSIONERS ACTION

- **Recommended Motion:**





## Jehovah's Witnesses Bible Educational Displays

- How is our work organized?
- What is included in our literature displays?
- Request approval to display on Port Boardwalk

## How is our work organized?



- 2 or 3 individuals will accompany the cart and endeavor to be warm, friendly and inviting.
- We stand or sit a few feet from our displays, we may offer a greeting to those that pass our display, but **we never approach people**. We always allow an interested individual to engage us in conversation or take a piece of our literature.
- At Matthew 28:19, 20, Jesus instructed his followers to “make disciples of people of all the nations.” Jehovah’s Witnesses follow that direction today, millions all over the world volunteer their time to follow the example of early Christians.

## Training we provide to our members



### Do:

- Be friendly, welcoming, and helpful to passersby.
- Stand at an appropriate distance from the display. Once a person approaches, try to engage them in conversation.



### Don't:

- Be aggressive, insistent, or pushy or stand next to the display like a sentinel.
- Block the display or become too engrossed in conversation with your partner.
- Get involved in debates with individuals, leave the area if an individual becomes abusive.

## What is included in our literature displays?

- Our literature points to the practical guidance that can be found in the Bible.
- It covers a wide range of topics, for people of all backgrounds and ages.
- The literature covers topics such as how to have a happy family life, how to find enjoyment in work and in relationships with others.
- It also discusses the purpose of life, why we suffer, and what the future holds.

JW  
.ORG

- All our literature is free
- Our displays highlight personal Bible study programs that individuals can request at a time and place convenient for them
- Our displays direct people to the website [jw.org](http://jw.org), where they can learn more about Jehovah's Witnesses and our preaching activity, as well as find answers to their Bible questions.

**We would like to request approval to display our Bible Educational Carts at the Port of Brookings Harbor Boardwalk**



We would like to request permission to have the display set up 4 days a week, Monday, Wednesday, Friday and Saturday between the hours of 10am and 4pm.

I'd like to take the opportunity to thank the Port of Brookings-Harbor Board of Commissioners and the Port Manager for considering our request.

If you have any follow-up questions or concerns, please contact Jeremy Watson at 541-373-0788.

## ACTION ITEM – F

---

**DATE:** August 17, 2022  
**RE:** Sale of Port Property  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

---

### OVERVIEW

- The Port received a purchase proposal for the Port property across Lower Harbor Road. Same property the Port had for sale few years ago and is currently not on the market.
- The last appraisal on the property was completed in 2017.
- Does the Port want to sell this property?

### DOCUMENTS

- None

### COMMISSIONERS ACTION

- **Recommended Motion:**

## ACTION ITEM – G

---

**DATE:** August 17, 2022  
**RE:** FEMA Construction Drawings  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

---

### OVERVIEW

- March 24, 2022, the Board approved preliminary drawings for FEMA budgetary approval.
- The Port was waiting months for FEMA to complete its review. Jack Akin and the Port is working with FEMA to finalizing dredging quantities and approve the budget.
- Once the board approves the construction drawings, the next step is to develop a bid package and release these projects in time for contractors to bid and schedule the work for next dry season (summer of 2023).
- Kite Field RV Park Expansion drawings are on hold until the other work is booked. Kite Field work would be scheduled for the following year (summer of 2024) and the Port has time to finalize the plans and budget.
- FEMA engineering and permitting expenses are reimbursable.

### DOCUMENTS

- Administrative Project Timeline, 1 page
- Basin Dredging Drawings, 4 pages
- Commercial Roads and Stockpile Drawings, 24 pages
- Boat Yard Paving Drawings, 15 pages
- Embankment Rock Reconstruction Drawings, 7 pages

### COMMISSIONERS ACTION

- **Recommended Motion:**  
Motion to approve FEMA construction drawings for dredging, commercial roads and stockpile, boat yard paving and embankment rock reconstruction and to be incorporated in the bid packages.

# Port of Brookings Harbor

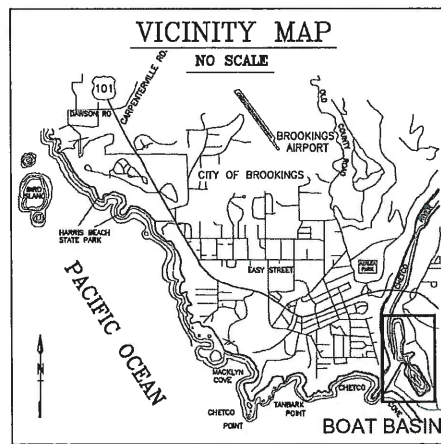
## FEMA Repairs and Mitigation Project Timeline

Commissioner Meetings Highlighted

Dates		Description
July 1, 2022	September 1, 2022	FEMA Funding Approval
August 17, 2022		Board Approval – Construction Drawings
October 1, 2022	November 1, 2022	OEM/FEMA Contract Review
November 16, 2022		Board Approval – OEM/FEMA Contract
November 16, 2022		Board Review – RV Park Expansion Construction Drawings
December 21, 2022		Board Approval – Bid Package for FEMA Repairs and Mitigation (Boat Yard and Commercial Gear Storage)
December 21, 2022		Board Approval – Bid Package for Basin 2 Slope Repair
December 21, 2022		Board Approval – Purchase Dredge Equipment & Materials
December 21, 2022		Board Approval – RV Park Expansion Construction Drawings
January 1, 2023	February 1, 2023	Invitation to Bid (ITB) FEMA Repairs and Mitigation (Boat Yard and Commercial Gear Storage)
January 1, 2023	February 1, 2023	Invitation to Bid (ITB) FEMA Repairs (Basin 2 Slope Repair)
January 1, 2023	April 1, 2023	County Planning and Building & Safety Permitting
February 1, 2023	March 1, 2023	Award Contractor (Boat Yard and Commercial Gear Storage)
February 1, 2023	March 1, 2023	Award Contractor (Basin 2 Slope Repairs)
February 15, 2023		Board Approval – Award Contractor (Boat Yard & Commercial Gear Storage)
February 15, 2023		Board Approval – Award Contractor (Basin 2 Slope Repair)
March 15, 2023		Board Review – Bid Package RV Park Expansion
April 19, 2023		Board Approval – Bid Package RV Park Expansion
May 1, 2023	June 1, 2023	Invitation to Bid (ITB) RV Park Expansion
June 1, 2023	July 1, 2023	Award Contractor RV Park Expansion
June 21, 2023		Board Approval – Award Contractor RV Park Expansion
July 2023	October 2023	Construction Begins (Boat Yard and Commercial Gear Storage)
October 2023	March 2024	Dredging Basins Begins (Port Staff)
October 2023	March 2024	Construction Begins Basin 2 Slope Repair
July 2024		Construction Begins RV Park Expansion

Dates are subject to change depending on FEMA funding, permitting, contractor schedules, and material & delivery challenges.





PORT OF BROOKINGS-HARBOR  
2021 CIVIL IMPROVEMENTS  
**BASIN DREDGING**

**NATURAL FEATURES**

EXISTING NATURAL RESOURCES OR NATURAL HAZARDS ON THE SUBJECT PROPERTY, INCLUDING WETLANDS, STREAMS, RIPARIAN AREAS, FLOOD PLAINS, OR FLOODWAYS TO BE DETERMINED BY ENGINEER

**EXISTING TREE CANOPY**

THERE ARE NO EXISTING TREES ON THE SUBJECT PROPERTY

**CULTURAL RESOURCES**

LOCALLY, OR FEDERALLY DESIGNATED HISTORIC AND/OR CULTURAL RESOURCES ON THE SITE OR ON ADJACENT PARCELS TO BE DETERMINED BY ENGINEER.

**PUBLIC SERVICES**

PUBLIC UTILITY SERVICES, INCLUDING WATER, SEWER, STORM DRAINAGE, POWER, TELEPHONE, CABLE INTERNET, AND GAS ARE AVAILABLE TO THE SUBJECT PROPERTY.

**UTILITY STATEMENT**

EXISTING UNDERGROUND UTILITIES ILLUSTRATED IN THESE PLANS ARE APPROXIMATED BASED ON MAPS OBTAINED FROM CURRY COUNTY GIS ELEVATIONS ESTIMATES, OR HAVE BEEN LOCATED BY A UTILITY LOCATE COMPANY. LAYOUT INDICATED IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. ALL LINES WITHIN PROJECTED WORK ZONE SHALL BE FIELD VERIFIED AS REQUIRED PRIOR TO CONSTRUCTION.

**PROJECT DESCRIPTION**

**TITLE:** BASIN DREDGING  
**REFERENCE:** PB115  
**LOCATION:** NORTH AND SOUTH BASIN  
**TAX LOT(S):** 298, 299, 498, 499, 500, 1100, 1200, 1700, 1799, 1798, 1800, 3400, 3100, 3300

**DRAWING REGISTER**

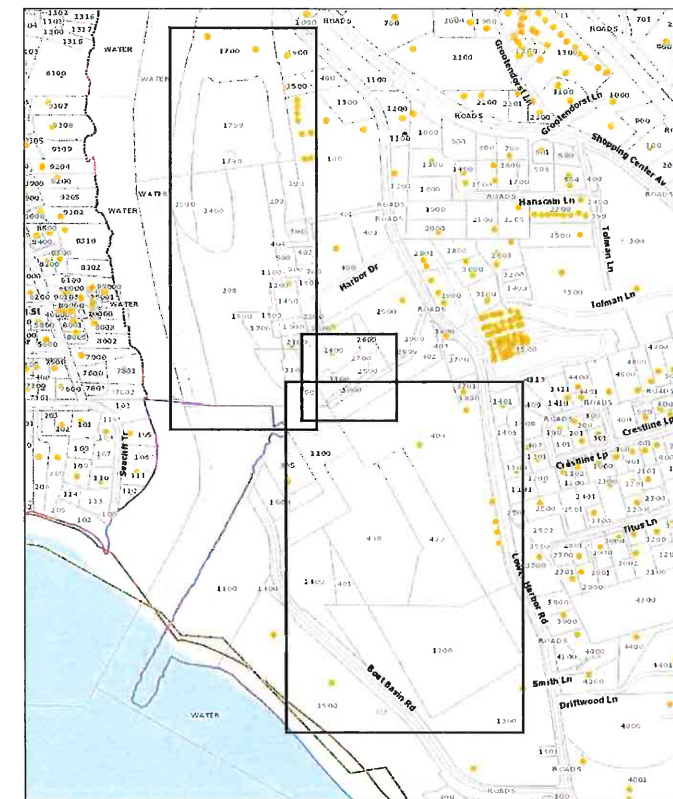
PB115-C100 Cover Sheet  
PB115-C101 Existing Condition  
PB115-C102 Planned Dredging  
PB115-C103 Isopach



PROJECT OVERVIEW  
SCALE 1" : 500'

Name	Type	2d Area (Sq. Ft.)	Cut (Cu. Yd.)	Fill (Cu. Yd.)	Net (Cu. Yd.)
DREDGE 4432	full	51826.48	9081.75	887.80	8193.95<Cut>
DREDGE 4452	full	349993.68	69380.03	0.00	69380.03<Cut>
DREDGE ICEHOUSE	full	35086.03	3862.12	2.58	3859.53<Cut>

VOLUME REPORT



PORT OF BROOKINGS HARBOR  
MAP OF TAX LOTS

**PRELIM GRADING NOTES**

1. DEQ 1200-C PERMIT IS REQUIRED.
2. UNLESS DIRECTED OTHERWISE, REMOVE CLEARED AND GRUBBED MATERIAL FROM THE SITE AND DISPOSE AT AN APPROVED LOCATION.
3. PRIOR TO THE START OF CONSTRUCTION, VERIFY GRADES AT SAWCUT LOCATIONS AND MATCHING OF EXISTING GRADE LOCATIONS.
4. MINIMIZE TRAFFIC ON SOIL AREAS DURING WET WEATHER. IF THE SITE SOILS ARE EXPOSED DURING WET WEATHER, THE USE OF CRUSHED ROCK PLACED AS ENGINEERED FILL IN THE BOTTOM OF THE EXCAVATIONS MAY BE NECESSARY TO PROTECT THE SUBGRADE. TAKE ALL PRECAUTIONS TO LIMIT SURFACE DISTURBANCE AND PROTECT THE SITE GRADING AREA FROM EROSION AND RUNOFF.
5. UNLESS OTHERWISE NOTED, THE SAMPLING AND TESTING OF MATERIALS FOR USE ON THE JOBSITE SHALL BE AT THE EXPENSE OF THE CONTRACTOR. ALL TESTING OF MATERIALS AND WORKMANSHIP SHALL BE PERFORMED BY A CERTIFIED TESTER. RESULTS OF THE TESTS SHALL BE SENT DIRECTLY TO THE PROJECT ENGINEER AS WELL AS THE CONTRACTOR, BY THE LABORATORY. LOCATION AND FREQUENCY OF TESTS SHALL BE DESIGNATED BY THE GENERAL CONTRACTOR.
6. ALL CUT AND FILL SLOPES SHALL BE MAXIMUM OF 2:1.

**LEGEND**

- 5 ELEVATION
- SUBGRADE MINOR CONTOUR
- SUBGRADE MAJOR CONTOUR
- PARCEL
- GEOTEXTILE
- CONCRETE PAD
- GRASS
- JETTY
- SLIP WAY
- PAVED ROAD



ENGINEER:



NO.	DATE	REVISION	BY

PREPARED FOR: (LOT 2900, MAP '360522DB')

**PORT OF BROOKINGS**  
16930 Lower Harbor Rd, Brookings, OR 97415



Date 01/06/2021

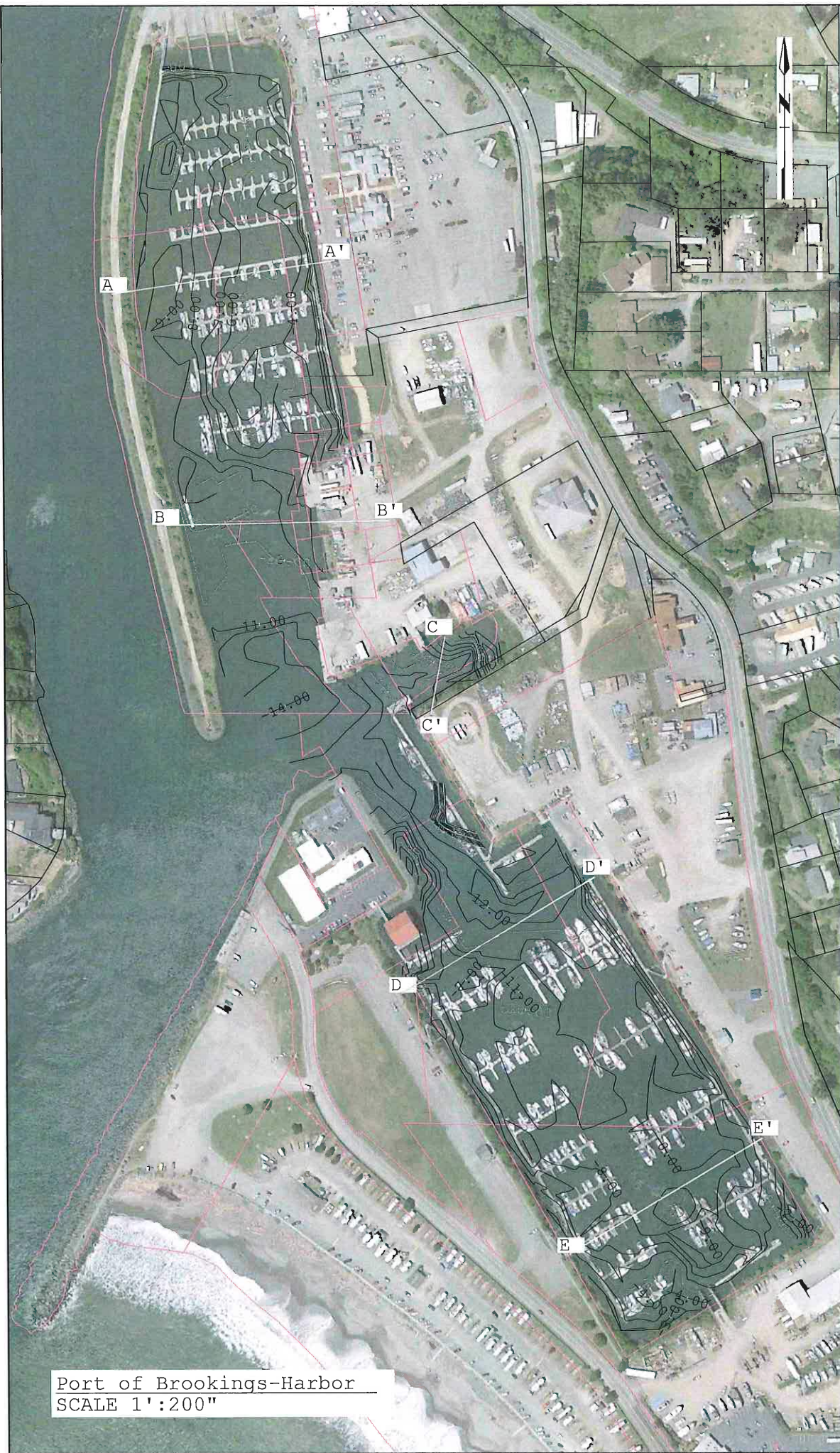
Drawn By INFRA DRAFT

Sheet No.

C-100

File No.

PB115



Port of Brookings-Harbor  
SCALE 1':200"



2019 BATHYMETRIC SURVEY  
SCALE 1':200"



Date  
12/26/2020  
Drawn By  
INFRADRAFT  
Sheet No.  
C-101  
File No.  
PB115

PREPARED FOR:  
**PORT OF BROOKINGS**  
16330 Lower Harbor Rd, Brookings, OR 97415  
(LOT 2900, MAP '360522DB')

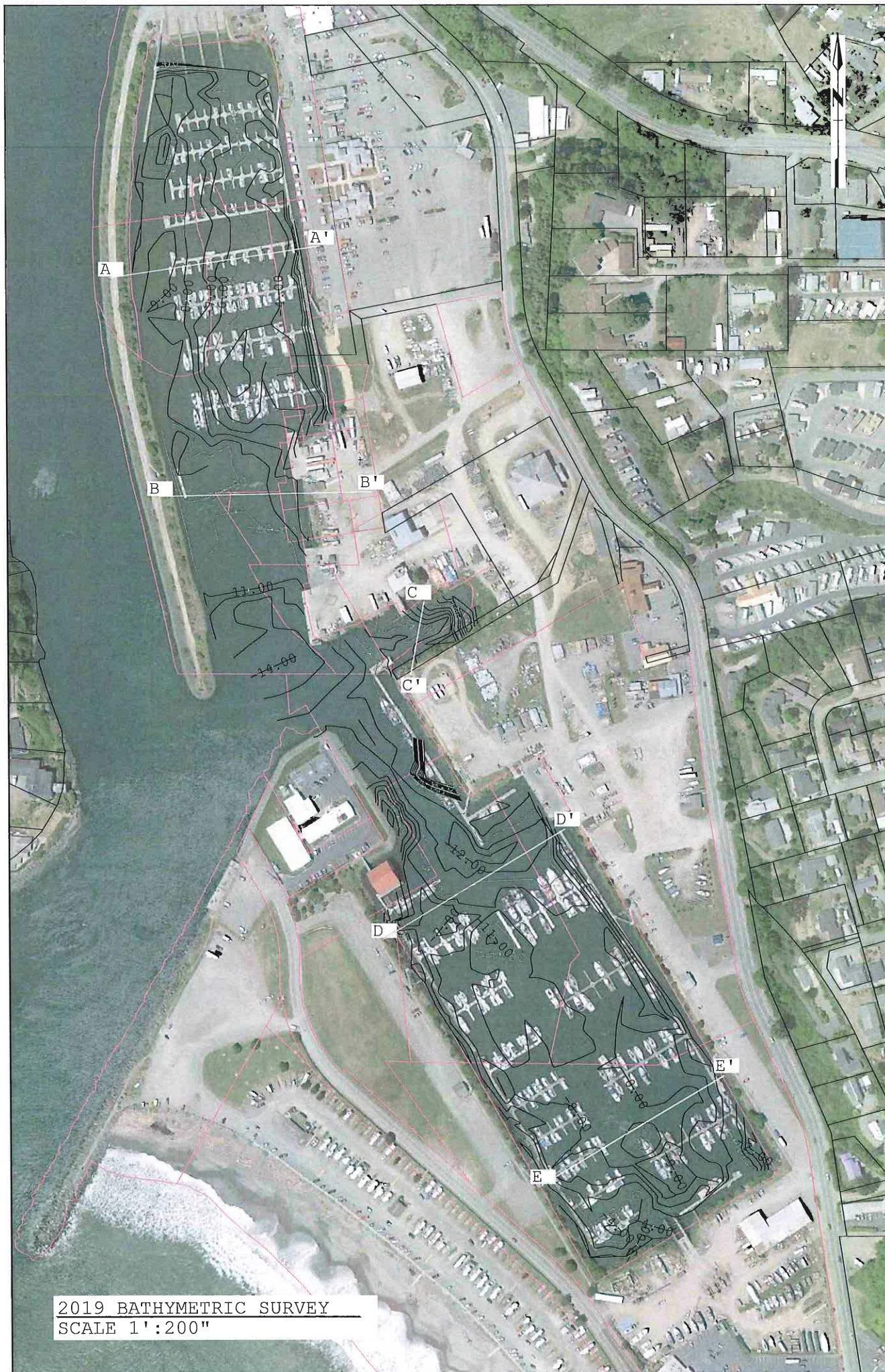


ENGINEER:

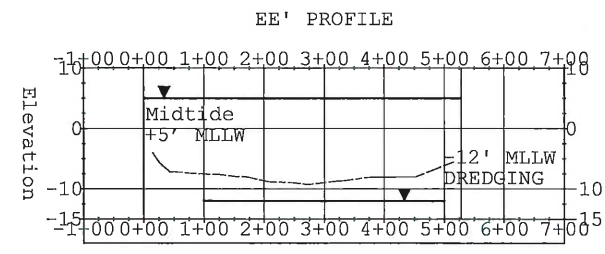
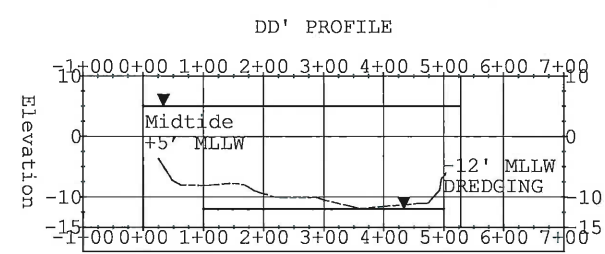
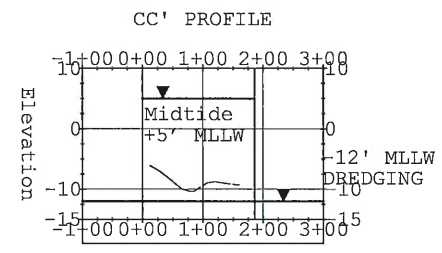
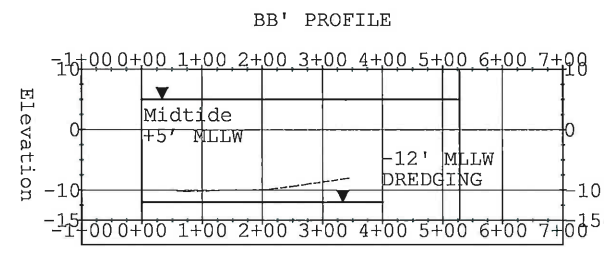
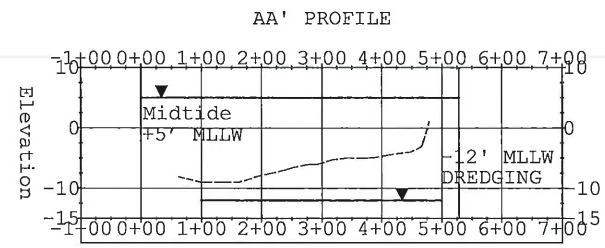


Grant P. A. Jackson, P.E., Mechanical, OR  
Professional Engineer License No. 19027  
EMC Engineers/Scientists, LLC  
2000 S. W. 10th St., Suite 100  
Beaverton, OR 97005  
503.638.8888  
www.emc-engineers.com

NO.	DATE	REVISION	BY



2019 BATHYMETRIC SURVEY  
SCALE 1':200"



PROFILES  
HZ SCALE 1':200"

ENGINEER:



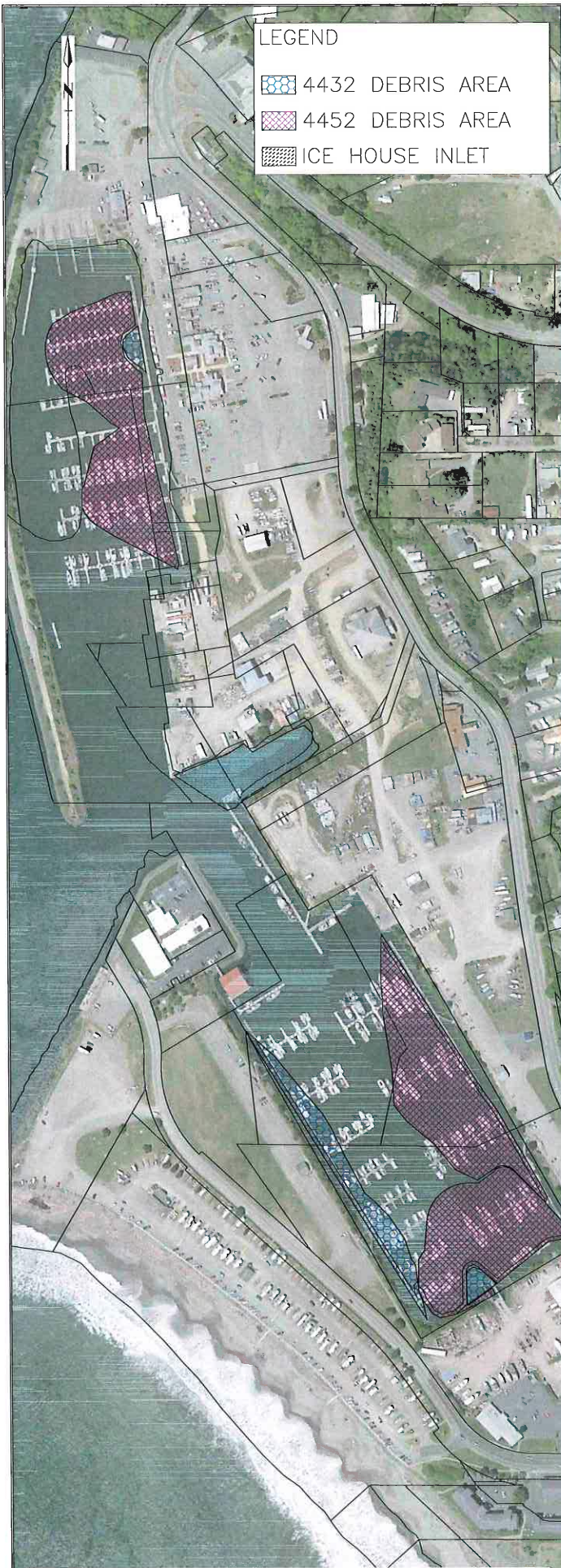
NO.	DATE	REVISION	BY



PREPARED FOR: (LOT 2900, MAP '360522DB')  
**PORT OF BROOKINGS**  
16330 Lower Harbor Rd, Brookings, OR 97415

Date	12/26/2020
Drawn By	INFRADRAFT
Sheet No.	C-102
File No.	PB115





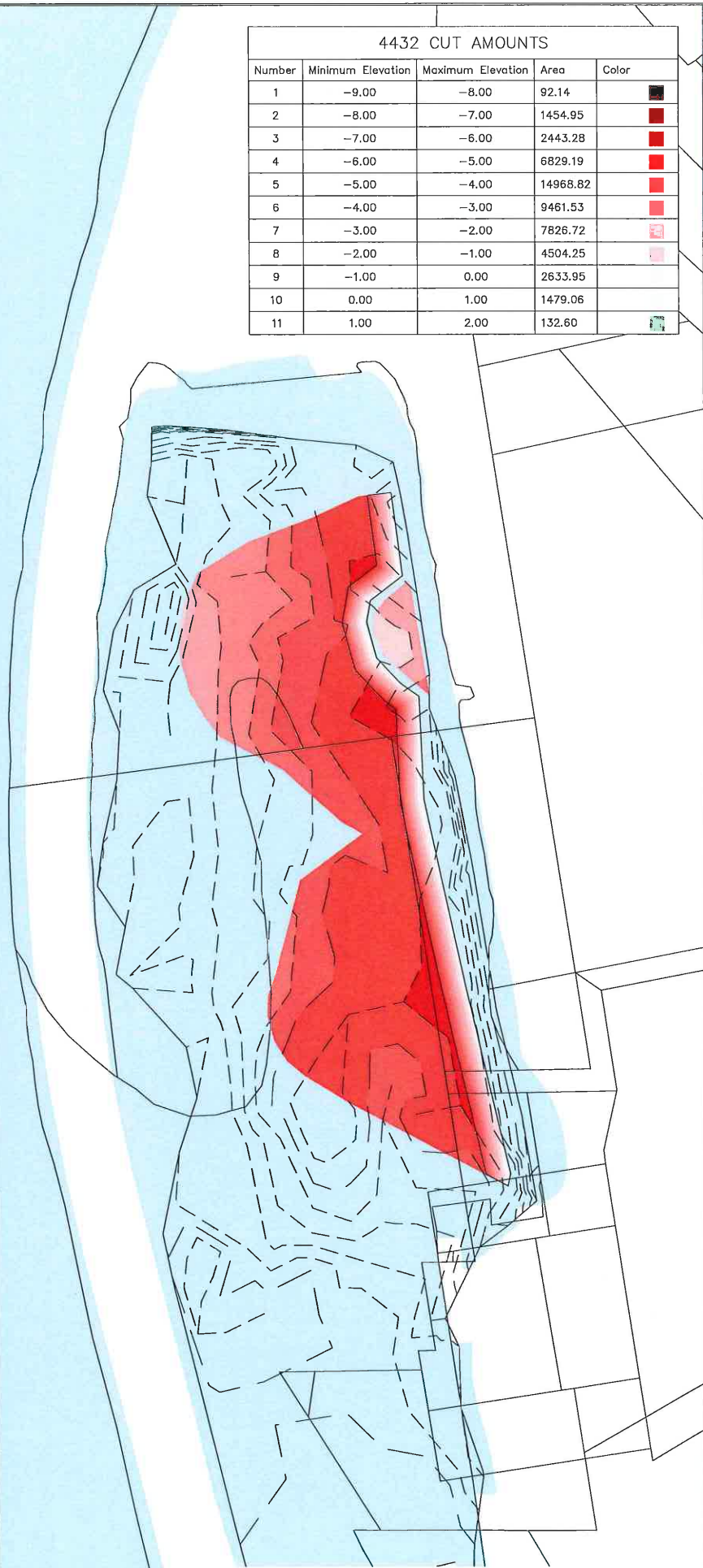
**LEGEND**

- 4432 DEBRIS AREA
- 4452 DEBRIS AREA
- ICE HOUSE INLET

PLANNED DREDGING  
SCALE 1" : 150'

**4432 CUT AMOUNTS**

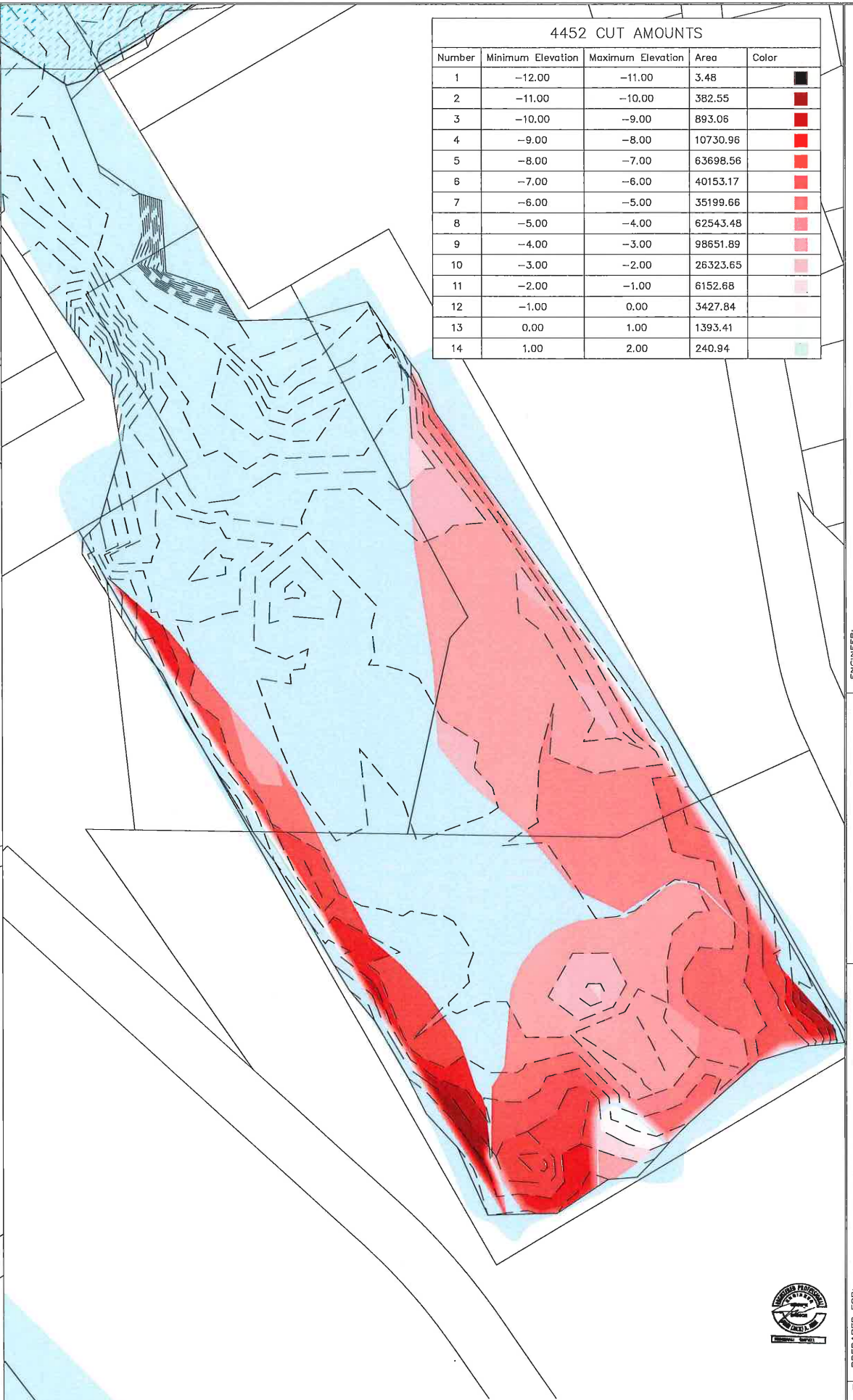
Number	Minimum Elevation	Maximum Elevation	Area	Color
1	-9.00	-8.00	92.14	Black
2	-8.00	-7.00	1454.95	Dark Red
3	-7.00	-6.00	2443.28	Red
4	-6.00	-5.00	6829.19	Light Red
5	-5.00	-4.00	14968.82	Very Light Red
6	-4.00	-3.00	9461.53	Lightest Red
7	-3.00	-2.00	7826.72	Very Lightest Red
8	-2.00	-1.00	4504.25	Lightestest Red
9	-1.00	0.00	2633.95	White
10	0.00	1.00	1479.06	Lightestestest Red
11	1.00	2.00	132.60	White



NORTH BASIN ISOPACH  
SCALE 1" : 80'

**4452 CUT AMOUNTS**

Number	Minimum Elevation	Maximum Elevation	Area	Color
1	-12.00	-11.00	3.48	Black
2	-11.00	-10.00	382.55	Dark Red
3	-10.00	-9.00	893.06	Red
4	-9.00	-8.00	10730.96	Light Red
5	-8.00	-7.00	63698.56	Very Light Red
6	-7.00	-6.00	40153.17	Lightest Red
7	-6.00	-5.00	35199.66	Very Lightest Red
8	-5.00	-4.00	62543.48	Lightestest Red
9	-4.00	-3.00	98651.89	White
10	-3.00	-2.00	26323.65	Lightestestest Red
11	-2.00	-1.00	6152.68	White
12	-1.00	0.00	3427.84	Lightestestestest Red
13	0.00	1.00	1393.41	White
14	1.00	2.00	240.94	White



SOUTH BASIN ISOPACH  
SCALE 1" : 80'

ENGINEER:  
**EMC**  
Engineering & Construction, LLC

NO.	DATE	REVISION	BY

PORT OF BROOKINGS  
TRITON

PREPARED FOR:  
**PORT OF BROOKINGS**  
16350 Lower Harbor Rd., Brookings, OR 97415

Date: 12/28/2020  
Drawn By: INFRADRAFT  
Sheet No.: C103  
File No.: 115

# GENERAL CONSTRUCTION NOTES

- WORK AND MATERIALS SHALL CONFORM TO THE PROVISIONS OF THE CURRENT "OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION", ODOT/AMERICAN PUBLIC WORKS ASSOCIATION (APWA), UNLESS OTHERWISE COVERED BY THE SPECIFICATIONS WRITTEN FOR THIS PROJECT (IF APPLICABLE), OR THE WATER / SEWER STANDARDS AND SPECIFICATIONS OR THE "STANDARD DRAWINGS" OF CURRY COUNTY.
- IN THE EVENT OF CONFLICT IN REGULATIONS AND SPECIFICATIONS GOVERNING THIS PROJECT, THE ORDER OF PRECEDENCE IS AS FOLLOWS:
  - CONTRACT SPECIAL PROVISIONS;
  - CONSTRUCTION PLANS;
  - PORT OF BROOKINGS HARBOR STANDARDS AND SPECIFICATIONS;
  - GENERAL NOTES;
  - ODOT/APWA SPECIFICATIONS FOR CONSTRUCTION.
- ALL CONSTRUCTION SHALL BE SUBJECT TO INSPECTION AND COMPLIANCE WITH THE ABOVE APPLICABLE REGULATIONS AND SPECIFICATIONS.
- ALL CONTRACTORS AND SUBCONTRACTORS SHALL POSSESS A VALID STATE CONTRACTOR'S LICENSE PRIOR TO COMMENCING WORK ON THIS PROJECT. ALL CONTRACTORS AND SUBCONTRACTORS MUST ALSO BE CURRENTLY PREQUALIFIED WITH CURRY COUNTY FOR THE CLASS(ES) OF WORK REQUIRED PRIOR TO ANY CONSTRUCTION.
- A MANDATORY PRE-CONSTRUCTION CONFERENCE OF ALL PARTIES SHALL BE HELD PRIOR TO ANY CONSTRUCTION.
- THE PORT OF BROOKINGS HARBOR SHALL BE NOTIFIED 24 HOURS IN ADVANCE OF ANY STAGE OF CONSTRUCTION.
- THE ENGINEER DOES NOT GUARANTEE THE COMPLETENESS OR ACCURACY OF THE EXISTING UNDERGROUND UTILITIES SHOWN ON THESE PLANS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR FIELD VERIFYING ANY POTENTIAL UTILITY CONFLICTS BETWEEN EXISTING UNDERGROUND UTILITIES AND THE WORK SHOWN ON THESE PLANS. THIS INCLUDES BOTH POTENTIAL UTILITY CONFLICTS SHOWN ON THESE PLANS AND POTENTIAL UTILITY CONFLICTS NOT SHOWN ON THESE PLANS BUT EITHER LOCATED, FOUND OR MARKED IN THE FIELD. WHERE POTENTIAL CONFLICTS ARE FOUND, THE CONTRACTOR SHALL TAKE WHATEVER MEASURES ARE NEEDED (INCLUDING PRELIMINARY POT-HOLING OF THE UTILITIES AT THE POTENTIAL CONFLICT LOCATION) PRIOR TO WORK IN THE AREA TO ALLOW TIME FOR THE RESPONSIBLE AGENCY TO CORRECT THE CONFLICT (UNLESS OTHERWISE SPECIFIED ON THE PLANS). NO ADDITIONAL PAYMENT SHALL BE MADE FOR ANY EXPENSE THE CONTRACTOR MAY INCUR AS A RESULT OF HIS FAILURE TO ADEQUATELY EXPLORE, IN THE OPINION OF THE ENGINEER, POTENTIAL UTILITY CONFLICTS.
- THERE SHALL BE NO DEVIATION FROM THE APPROVED PLANS UNLESS REQUESTED IN WRITING BY CONTRACTOR AND APPROVED IN WRITING BY THE PORT OF BROOKINGS HARBOR.
- ALL UNDERGROUND UTILITIES AND SERVICE LATERALS ARE TO BE INSTALLED PRIOR TO CONSTRUCTION OF CURB AND GUTTER.
- CRUSHED ROCK BASE MATERIAL SHALL BE OBTAINED FROM A SOURCE APPROVED BY THE PORT OF BROOKINGS HARBOR.
- CRUSHED ROCK BASE MATERIAL SHALL COMPLY WITH APWA/ODOT SEC. 00641 AND SEC. 02630 AND SHALL BE PLACED IN MAXIMUM LIFTS OF (6) INCHES AND SHALL BE COMPACTED TO 100% OF MAXIMUM RELATIVE DENSITY AT OPTIMUM MOISTURE IN ACCORDANCE WITH AASHTO T-99 METHOD A WITH COURSE PARTIAL CORRECTION ACCORDING TO ODOT TM-223 PROCEDURE FOR THE DETERMINATION OF 100% RELATIVE MAXIMUM DENSITY OF GRANULAR MATERIALS.
- CLASS "B" TRENCH BACKFILL MATERIAL SHALL COMPLY WITH APWA/ODOT SEC. 00405.14 AND SHALL BE 3/4" CRUSHED ROCK UNDER PAVEMENT OR IN RIGHT-OF-WAY. BACKFILL MATERIAL SHALL BE PLACED IN MAXIMUM LIFTS OF 6" AND SHALL BE COMPACTED BY MECHANICAL MEANS TO 95% OF MAXIMUM RELATIVE DENSITY AT OPTIMUM MOISTURE IN ACCORDANCE WITH AASHTO T-99 METHOD D PROCEDURE FOR THE DETERMINATION OF 95% RELATIVE MAXIMUM DENSITY OF GRANULAR MATERIALS.
- UNLESS NOTED OTHERWISE, THE SAMPLING AND TESTING OF MATERIALS FOR USE ON THE JOBSITE SHALL BE AT THE EXPENSE OF THE CONTRACTOR. ALL TESTING OF MATERIALS AND WORKMANSHIP SHALL BE PERFORMED BY A CERTIFIED TESTER. RESULTS OF THE TESTS SHALL BE SENT DIRECTLY TO THE PORT OF BROOKINGS HARBOR AS WELL AS THE CONTRACTOR, BY THE LABORATORY. LOCATION AND FREQUENCY OF TESTS SHALL BE DESIGNATED BY THE PORT OF BROOKINGS HARBOR AND COORDINATED BY THE CONTRACTOR.
- CLASS "A" TRENCH BACKFILL MATERIAL SHALL BE APPROVED NATIVE MATERIAL PER ODOT/APWA SPECS. SEC. 00405.14 FOR ALL AREAS OUTSIDE OF TRAFFIC AREAS AND THE RIGHT-OF-WAY. BACKFILL MATERIAL SHALL BE PLACED IN MAXIMUM LIFTS OF SIX (6) INCHES AND SHALL BE COMPACTED BY MECHANICAL MEANS TO 75% OF MAXIMUM RELATIVE DENSITY AND OPTIMUM MOISTURE IN ACCORDANCE WITH AASHTO T-99 METHOD D PROCEDURE FOR THE DETERMINATION OF 95% RELATIVE MAXIMUM DENSITY OF GRANULAR MATERIALS.
- ASPHALTIC CONCRETE PAVEMENT SHALL BE LEVEL 2, 1/2" DENSE GRADE MIX. MATERIALS AND WORKMANSHIP SHALL BE AS SPECIFIED IN SECTION 00744 OF THE ODOT/APWA SPECS. INSTALLATION SHALL BE IN ACCORDANCE WITH PORT OF BROOKINGS HARBOR STANDARD SPECIFICATIONS, AND TO THE CROSS-SECTION(S), GRADE AND LOCATIONS SHOWN ON THE APPROVED PLANS.
- CONSTRUCTION STAKING SHALL BE PROVIDED BY THE CONTRACTOR'S SURVEYOR, FOR EACH PHASE OF CONSTRUCTION. STAKES SHALL BE IN PLACE PRIOR TO COMMENCING CONSTRUCTION AND SHALL BE CONTINUOUSLY MAINTAINED BY THE CONTRACTOR UNTIL EACH PHASE OF CONSTRUCTION HAS BEEN COMPLETED AND INSPECTED. THE CONTRACTOR'S SURVEYOR SHALL PERFORM THE CONTRACTOR RESPONSIBILITIES DESCRIBED IN THE CONSTRUCTION SURVEYING MANUAL FOR CONTRACTORS, CHAPTER 1.6 (SEC. 00305).
- A COPY OF THE APPROVED PLANS, SPECIFICATIONS AND STANDARD DRAWINGS SHALL BE ON THE JOBSITE AT ALL TIMES WHILE THE WORK IS IN PROGRESS.
- MONOLITHIC CURB & GUTTER SHALL BE CONSTRUCTED IN ACCORDANCE WITH PORT OF BROOKINGS HARBOR STANDARD SPECIFICATIONS AND TO THE CROSS-SECTION, GRADE AND LOCATIONS AS SHOWN ON THE APPROVED PLANS.
- STREET NAMES, SIGNS, STOP BARS AND STOP SIGNS SHALL BE INSTALLED BY PORT OF BROOKINGS HARBOR. SIGN SLEEVES TO BE SUPPLIED AND INSTALLED BY THE CONTRACTOR.
- ALL MATERIAL REMAINING AFTER BACKFILLING OPERATIONS HAVE BEEN COMPLETED SHALL BE DISPOSED OF BY THE CONTRACTOR IN A MANNER APPROVED OF BY THE PORT OF BROOKINGS HARBOR AND THE ENGINEER.
- PRIOR TO FINAL ACCEPTANCE, THE PORT OF BROOKINGS HARBOR SHALL CERTIFY THAT ALL IMPROVEMENTS HAVE BEEN CONSTRUCTED IN ACCORDANCE WITH THE APPROVED PLANS AND SPECIFICATIONS.
- IF THE CONTRACTOR WISHES TO USE WATER FROM THE PORT OF BROOKINGS HARBOR WATER DISTRIBUTION SYSTEM, CONTRACTOR SHALL MAKE USE OF A BULK WATER STATION IF AVAILABLE. AS AN OPTION, THE CONTRACTOR MAY APPLY FOR A HYDRANT METER PERMIT THROUGH HARBOR WATER DISTRICT.
- CONTRACTOR SHALL NOT USE THE PUBLIC RIGHT-OF-WAY FOR LONG TERM STAGING OR MATERIAL STORAGE WITHOUT PRIOR APPROVAL. WORK AREA FOR STORAGE OF PROJECT MATERIALS AND EQUIPMENT THAT WILL BE USED DURING THAT DAY; HOWEVER, AT THE END OF THE DAY, THE WORK SITE SHALL BE CLEANED UP TO THE SATISFACTION OF THE ENGINEER. DISPOSAL OF MATERIALS IS NOT PERMITTED WITHIN THE PUBLIC RIGHT-OF-WAY.
- THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DISPOSAL SITES OUTSIDE OF THE RIGHT-OF-WAY, SHALL PAY ANY AND ALL COSTS INVOLVED, AND SHALL FURNISH THE PORT OF BROOKINGS HARBOR WITH ALL REQUIRED PERMITS AND DISPOSAL SITE AGREEMENTS.
- THE CONTRACTOR SHALL DESIGNATE AN EMERGENCY CONTACT PERSON THAT WILL HANDLE AFTER-HOURS ISSUES RELATED TO THE PROJECT, AND SHALL PROVIDE EMERGENCY CONTACT TELEPHONE NUMBERS TO THE PORT OF BROOKINGS HARBOR.
- ALL MATERIALS SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE SHOWN ON THE PLANS OR LISTED IN THE CONTRACT SPECIFICATIONS.
- ALL GRADES SHOWN ON THE PLANS SHALL BE SUBJECT TO ADJUSTMENT IN THE FIELD BY THE ENGINEER (IN ACCORDANCE WITH THE PROJECT SURVEYOR).
- THE CONTRACTOR SHALL CONTACT THE PORT OF BROOKINGS HARBOR 24 HOURS IN ADVANCE OF ANY EXCAVATION NEAR THE COUNTY OR PORT FACILITIES. CONTACT THE PORT OF BROOKINGS HARBOR IF ANY UNALLOCATED FACILITIES ARE DISCOVERED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL NOTIFY CURRY COUNTY 24 HOURS IN ADVANCE OF ANY EXCAVATION OR CONSTRUCTION ACTIVITIES.

# PORT OF BROOKINGS - HARBOR COMMERCIAL ROADS AND STOCKPILE

## PROJECT DESCRIPTION

TITLE: COMMERCIAL ROADS AND STOCKPILE  
 REFERENCE: 140 & 114  
 LOCATION: 16330 LOWER HARBOR ROAD, BROOKINGS, OR  
 TAX LOT(S): 2500,2600,2700,2800, 2900,2999,402, 400

## PROJECT NOTES

### NATURAL FEATURES

EXISTING NATURAL RESOURCES OR NATURAL HAZARDS ON THE SUBJECT PROPERTY, INCLUDING WETLANDS, STREAMS, RIPARIAN AREAS, FLOOD PLAINS, OR FLOODWAYS TO BE DETERMINED BY ENGINEER

### EXISTING TREE CANOPY

THERE ARE NO EXISTING TREES ON THE SUBJECT PROPERTY

### CULTURAL RESOURCES

LOCALLY, OR FEDERALLY DESIGNATED HISTORIC AND/OR CULTURAL RESOURCES ON THE SITE OR ON ADJACENT PARCELS TO BE DETERMINED BY ENGINEER.

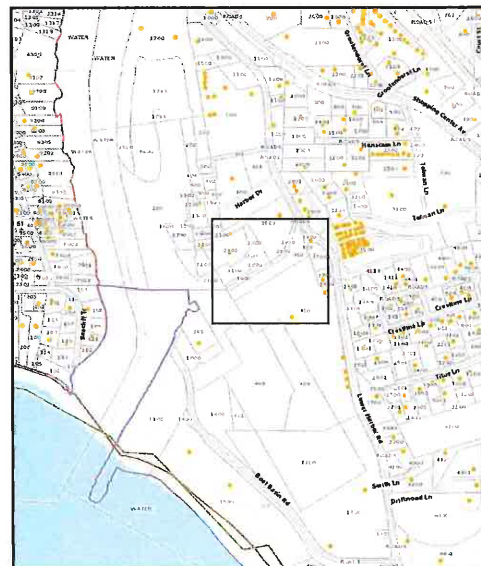
### PUBLIC SERVICES

PUBLIC UTILITY SERVICES, INCLUDING WATER, SEWER, STORM DRAINAGE, POWER, TELEPHONE, CABLE INTERNET, AND GAS ARE AVAILABLE TO THE SUBJECT PROPERTY.

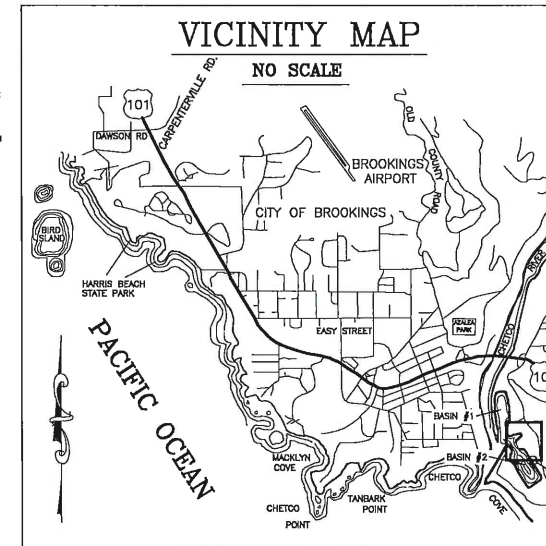
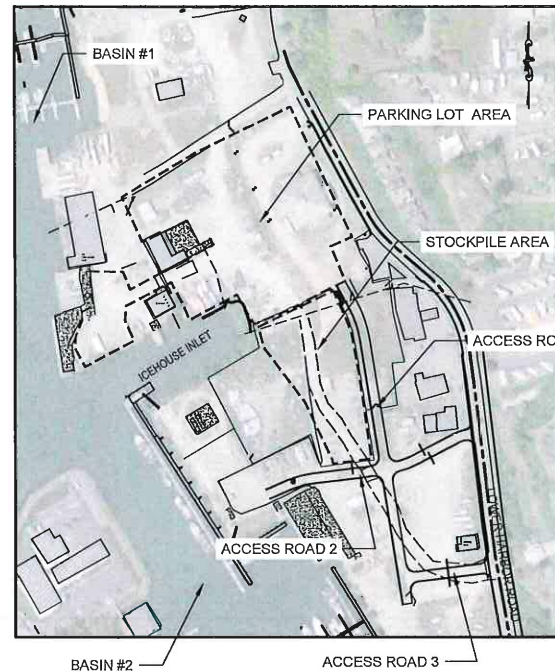
### UTILITY STATEMENT

EXISTING UNDERGROUND UTILITIES ILLUSTRATED IN THESE PLANS ARE APPROXIMATED BASED ON MAPS OBTAINED FROM CURRY COUNTY GIS ELEVATIONS ESTIMATES, OR HAVE BEEN LOCATED BY A UTILITY LOCATE COMPANY. LAYOUT INDICATED IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. ALL LINES WITHIN PROJECTED WORK ZONE SHALL BE FIELD VERIFIED AS REQUIRED PRIOR TO CONSTRUCTION.

## ASSESSOR MAP (NO SCALE)



## PROJECT OVERVIEW



### SURVEYOR

ROBERTS & ASSOCIATES LAND SURVEYING, INC.  
 611 SPRUCE STREET  
 BROOKINGS, OR 97415  
 (541) 489-0182  
 CONTACT: RICH ROBERTS

### PROJECT ENGINEER

EMC ENGINEERS / SCIENTISTS  
 450 CONESTOGA DRIVE  
 JACKSONVILLE, OR 97530  
 (541) 281-9929  
 CONTACT: JACK AKIN, P.E.

### HORIZONTAL DATUM

OREGON COORDINATE REFERENCE SYSTEM (OREGON COAST ZONE) AS DEFINED IN OREGON ADMINISTRATIVE RULES 734-005-0005 THRU 734-005-0015. COORDINATES WERE CONSTRAINED TO THE OREGON REAL-TIME (GRS) REFERENCE NETWORK (ORGN) REFERENCED TO NAD 83(2011) EPOCH 2010, INTERNATIONAL FEET, WITH A RELATIVE ACCURACY OF <2cm.

### VERTICAL DATUM

MEAN LOWER LOW WATER EPOCH 1983-2001. BENCH MARK UTILIZED FOR THIS SURVEY US ARMY CORPS OF ENGINEERS BENCH MARK - "FUEL 2" ELEVATION - 21.65 FEET

### GEOTECHNICAL NOTE

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE PROJECT ENGINEER FOR REQUIRED REMEDIATION. THE CONTRACTOR SHALL COORDINATE WITH THE PROJECT ENGINEER FOR REQUIRED SITE OBSERVATIONS AND TESTING OF ALL FILLS.

DATE	SET	DESCRIPTION	REVIEW & APPROVED BY ENGINEER
04/12/2022		PRELIMINARY - CD REVIEW 90% SUBMITTAL	X
05/18/2022		PRELIMINARY - REV. CD REVIEW 90% SUBMITTAL	X
06/01/2022		CONSTRUCTION DOCUMENTS SUBMITTAL SET	X
07/15/2022	X	CONSTRUCTION DOCUMENTS SUBMITTAL SET	X
		RECORD DRAWINGS	

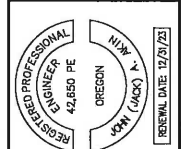
### SHEET INDEX

C.01 COVER SHEET - PROJECT NOTES	C2.5 PARKING LOT DRAINAGE PLAN
C1.0 GENERAL NOTES	C2.6 STOCKPILE GRADING PLAN
C1.01 GENERAL NOTES	C2.8 STOCKPILE SECTION
C1.1 EXISTING CONDITIONS	C2.9 PARKING LOT SECTIONS
C1.1.1 DCP DATA	C3.0 ROAD 1 SECTIONS
C1.1.2 DCP DATA	C3.1 ROAD 2 & 3 SECTIONS
C2.0 DEMOLITION & CLEARING PLAN	C5.0 DETAILS
C2.01 SITE PLAN	C5.1 DETAILS
C2.02 ROAD 1 PLAN & PROFILE	C5.2 SWPPP DETAILS
C2.03 ROAD 2 & 3 PLAN & PROFILE	C5.3 SWPPP DETAILS
C2.1 PARKING SUBGRADE PLAN	
C2.2 ROAD PAVING PLAN	
C2.3 PARKING LOT FINISH PAVING	
C2.4 ROAD STORM DRAINAGE PLAN	

	AGENCY	APPROVED BY	DATE
POWER	COOS-CURRY ELECTRIC		
DOMESTIC WATER	HARBOR WATER DISTRICT		
SANITARY SEWER	HARBOR SANITARY		
STORM DRAINAGE	CURRY COUNTY		
STREETS	CURRY COUNTY		
ENGINEERING	PORT OF BROOKINGS HARBOR		
CONSTRUCTION AUTHORIZED TO PROCEED IN ACCORDANCE WITH APPROVED PLANS WHEN ALL PERMITS HAVE BEEN ISSUED AND PRE-CONSTRUCTION MEETING HAS CONCLUDED.			
BY	TITLE	DATE	

REVISIONS	BY:	DATE
	JA	

Grants Pass • Jacksonville • Medford, OR  
 CP Office: 1807 William Hwy, Suite 216, Grants Pass, OR, 97527  
 JVH Office: 480 Coalinga Dr., Jacksonville, OR, 97530  
 Ph: 541-274-9424 • Fax: 541-281-9929 • Email: info@emcsc.com  
 emcsc.com • emcsc.com



PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202

SHEET No:  
**C0.1**  
 COVER SHEET



**GENERAL NOTES**

THESE NOTES ARE INTENDED FOR USE IN INTERPRETING AND IMPLEMENTING THE TASKS SHOWN ON THE FOLLOWING CONSTRUCTION PLANS AND SPECIFICATIONS SHEETS. THESE ARE IN ADDITION TO THE OVERALL PROJECT SPECIFICATION AND BID DOCUMENTS AND CONTRACTUAL ITEMS.

THESE ARE INTENDED FOR THE USE OF THE GENERAL CONTRACTOR AND HIS/HER SUBCONTRACTORS IN THE DEMOLITION AND RECONSTRUCTION OF THE SUBJECT AREA OF THE PORT OF BROOKINGS HARBOR. USE FOR OTHER PURPOSES OR AT OTHER SITES IS NOT RECOMMENDED AND IS ACCOMPLISHED AT THE SOLE RISK OF THE USER.

THESE ITEMS ARE TO BE USED AS A SUPPLEMENT TO THE DETAILS PROVIDED ON THE PLAN SHEETS AND SPECIFICATION PAGES. ANY DISCREPANCIES FOUND AMONG THE DRAWINGS, THE SPECIFICATIONS, REFERENCED REPORTS, THESE GENERAL NOTES AND OTHER ITEMS LISTED ON THIS SHEET AND THE SITE CONDITIONS SHALL BE REPORTED TO THE ENGINEER, WHO SHALL CORRECT SUCH DISCREPANCY IN WRITING. ANY WORK DONE BY THE GENERAL CONTRACTOR AFTER DISCOVERY OF SUCH DISCREPANCY SHALL BE DONE AT THE GENERAL CONTRACTOR'S RISK. THE GENERAL CONTRACTOR SHALL VERIFY AND COORDINATE DIMENSIONS AMONG ALL DRAWINGS PRIOR TO PROCEEDING WITH ANY WORK.

STABILITY OF THE CUT/FILL AND SLOPE AREAS PRIOR TO COMPLETION IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR. THIS RESPONSIBILITY INCLUDES, BUT IS NOT LIMITED TO JOBSITE SAFETY, CONSTRUCTION MEANS, METHODS, AND SEQUENCES, TEMPORARY SHORING, SLOPE STABILITY, FORMWORK AND BRACING, USE OF EQUIPMENT AND CONSTRUCTION PROCEDURES.

CONSTRUCTION OBSERVATION BY THE ENGINEER IS FOR CHECKING FOR CONFORMANCE WITH DESIGN ASPECTS ONLY AND IS NOT INTENDED IN ANY WAY TO REVIEW AND/OR APPROVE THE GENERAL CONTRACTOR'S CONSTRUCTION PROCEDURES OR RELIEVE THE CONTRACTOR FROM PROVIDING A COMPLETED PROJECT, CONSISTENT WITH THE PLANS AND SPECIFICATIONS AND GOOD CONSTRUCTION PRACTICES. SPECIAL INSPECTION BY THE ENGINEER DOES NOT PROVIDE A CERTIFICATION OF THE PROJECT OR RELIEVE THE CONTRACTOR OF ALL RESPONSIBILITY FOR A PROPERLY CONSTRUCTED PROJECT.

**STANDARDS USED FOR DESIGN**

ALL METHODS, MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE PLANS AND SPECIFICATIONS AND ODOT STANDARD SPECIFICATIONS, UNLESS ELSEWHERE HEREIN SPECIFIED OTHERWISE. INTERNATIONAL BUILDING CODE (IBC) 2018; 2019 OREGON STRUCTURE SPECIALTY CODE (OSSC); AMERICAN SOCIETY OF CIVIL ENGINEERS (ASCE)

**OTHER NOTES**

1. JOBSITE SAFETY IS THE RESPONSIBILITY OF THE CONTRACTOR.
2. ALL PRODUCTS AND WORKMANSHIP SHALL BE NEW MATERIALS OF GOOD QUALITY, ACCEPTABLE FOR THIS TYPE OF CONSTRUCTION. WORK TO BE ACCOMPLISHED IN A GOOD AND WORKMANLIKE MANNER.
3. ALL MATERIALS TO BE SHIPPED, HANDLED AND STOCKPILED IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS AND GOOD CONSTRUCTION PRACTICES.
4. LOCATIONS MUST BE VERIFIED AT THE SITE WITH THE ENGINEER AND THE PORT OF BROOKINGS HARBOR REPRESENTATIVE PRIOR TO PLACEMENT.
5. ABIDE BY LOCAL, STATE AND FEDERAL BUILDING ORDINANCES, INCLUDING ALL SAFETY REQUIREMENTS, IN ALL PHASES OF THE PROJECT.
6. ALL PHASES OF THE PROJECT ARE TO CONFORM TO THE PLANS AND SPECIFICATIONS ATTACHED HERETO AND SPECIFICATIONS PROVIDED BY THE OWNER AND THE ENGINEER-OF-RECORD.
7. PROPOSED CHANGES TO PROJECT PLANS AND SPECIFICATIONS MUST BE APPROVED BY THE ENGINEER PRIOR TO ACCEPTANCE AND IMPLEMENTATION AT THE SITE.
8. PROPOSED CHANGES MUST BE SUBMITTED IN WRITING FOR REVIEW AND APPROVAL/DISAPPROVAL BY THE ENGINEER AND THE OWNER.
9. IN NO CASE SHALL CHANGES, SUBSTITUTIONS OR OMISSIONS BE MADE TO THE DESIGN OR MATERIALS WITHOUT THE WRITTEN AUTHORIZATION OF THE ENGINEER AND THE OWNER.
10. AUTHORIZATION OF A DESIGN CHANGE BY THE ENGINEER DOES NOT CONSTITUTE ACCEPTANCE BY THE PORT OF BROOKINGS HARBOR, NOR DOES IT AUTHORIZE ADDITIONAL FUNDS FOR THE CHANGES. THE PORT'S REPRESENTATIVE MUST AUTHORIZE IN WRITING THE DESIGN CHANGE AND APPLICABLE CHANGES IN THE CONTRACT AMOUNT AND/OR CONSTRUCTION TIME PERIOD PRIOR TO IMPLEMENTATION OF SUCH CHANGES.
11. PROJECT SCHEDULE AND GENERAL SEQUENCING OF ALL WORK MUST BE REVIEWED AND APPROVED BY THE ENGINEER AND THE OWNER. SUCH APPROVAL DOES NOT RELIEVE THE CONTRACTOR OR HIS/HER SUBCONTRACTORS OF ALL RESPONSIBILITIES FOR PROPER EXECUTION OF THE SUBJECT PROJECT CONSTRUCTION.
12. SEQUENCING OF TASKS THAT REQUIRES VARYING THE INSTALLED SIZES OF PROJECT MATERIALS MUST BE REVIEWED AND APPROVED BY THE ENGINEER AND OWNER.
13. TRAFFIC CONTROL AND SIGNAGE MUST BE PROVIDED BY THE CONTRACTOR UNLESS OTHERWISE SO STATED IN THE CONTRACT. ACCESS TO THE US COAST GUARD FACILITIES MUST BE MAINTAINED DURING CONSTRUCTION.
14. CONTRACTOR MUST UNDERSTAND THAT THE PROJECT SITE IS IN A HARBOR AREA SUBJECT TO TIDAL FLUCTUATIONS. THEREFORE, SEQUENCING AND PROJECT WORK MUST TAKE INTO ACCOUNT EFFECTS OF HIGH AND LOW TIDES(IF APPLICABLE).
15. REPLACEMENT OF SPECIFIED PRODUCTS BY AN "EQUIVALENT" PRODUCT MUST BE APPROVED BY THE ENGINEER AND THE OWNER. REDESIGN REQUIRED FOR USE OF ALTERNATE "EQUIVALENT" MATERIALS IS TO BE BORNE BY THE CONTRACTOR.

**GRADING NOTES**

1. PRIOR TO THE CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE UNSUITABLE FOUNDATION MATERIAL, VOIDS, TRENCHES AND HOLES ENCOUNTERED WITHIN CONSTRUCTION LIMITS SHALL BE FILLED WITH APPROVED MATERIAL. PRIOR TO BACKFILLING THE CONTRACTOR SHALL BREAK CONCRETE(IF REQUIRED) AS DIRECTED. THE CONTRACTOR SHALL BREAK UP AND ROUGHEN THE GROUND SURFACE BEFORE MATERIAL IS PLACED. THE NATURAL GROUND UNDERLYING CONSTRUCTION AREAS SHALL BE COMPACTED TO THE DENSITY SPECIFIED FOR THE SPECIFIED MATERIALS TO BE PLACED.
2. CONSTRUCTION SHALL INCLUDE PREPARATION OF THE AREAS UPON WHICH MATERIALS ARE PLACED, THE PLACEMENT AND COMPACTION OF APPROVED MATERIALS AND FILLING OF HOLES, PITS AND OTHER DEPRESSIONS WITHIN THE PROJECT AREA.
3. THE CONTRACTOR SHALL PLACE FILLS IN THE HORIZONTAL LAYERS AS SPECIFIED AND COMPACT EACH LAYER TO THE DENSITY SPECIFIED.
4. IMMEDIATELY PRIOR TO COMPLETION OF THE EARTHWORK, THE CONTRACTOR SHALL CLEAN THE ENTIRE WORK AREA OF DEBRIS AND FOREIGN MATTER.
5. THE MAXIMUM DENSITY OF COMPACTED MATERIAL WILL BE DETERMINED BY AASHTO T-99
6. THE CONTRACTOR SHALL COMPACT ALL FILLS AND BACKFILLS TO A MINIMUM IN PLACE DENSITY OF 95 PERCENT.
7. THE CONTRACTOR SHALL WATER THE MATERIALS TO PROVIDE OPTIMUM MOISTURE FOR COMPACTION BACKFILLS. BACKFILL MATERIALS SHALL NOT BE PLACED IN FINAL POSITION UNTIL MOISTURE IN EXCESS OF OPTIMUM MOISTURE HAS BEEN REMOVED.
8. IF THE SPECIFIED COMPACTION IS NOT OBTAINED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER. THE CONTRACTOR MAY BE REQUIRED TO USE A MODIFIED COMPACTION PROCEDURE OR APPLY ADDITIONAL COMPACTION EFFORT. IF APPROVED MATERIALS MEETING THE SPECIFICATIONS CANNOT BE COMPACTED TO THE REQUIRED DENSITY REGARDLESS OF COMPACTION EFFORT OR METHOD, THE ENGINEER MAY REDUCE THE REQUIRED DENSITY OR DIRECT THE USE OF ALTERNATE MATERIALS. IN NO CASE SHALL EARTHWORK OPERATIONS PROCEED UNTIL THE CONTRACTOR IS ABLE TO COMPACTION THE MATERIAL TO THE SATISFACTION OF THE ENGINEER.
9. DEQ 1200-C PERMIT IS REQUIRED FOR THIS PROJECT.
10. UNLESS DIRECTED OTHERWISE, REMOVE CLEARED AND GRUBBED MATERIAL FROM THE SITE AND DISPOSE AT AN APPROVED LOCATION.
11. UNLESS OTHERWISE NOTED, THE SAMPLING AND TESTING OF MATERIALS FOR USE ON THE JOBSITE SHALL BE AT THE EXPENSE OF THE CONTRACTOR. ALL TESTING OF MATERIALS AND WORKMANSHIP SHALL BE PERFORMED BY A CERTIFIED TESTER. RESULTS OF THE TESTS SHALL BE SENT DIRECTLY TO THE PROJECT ENGINEER AS WELL AS THE CONTRACTOR, BY THE LABORATORY. LOCATION AND FREQUENCY OF TESTS SHALL BE DESIGNATED BY THE GENERAL CONTRACTOR.
12. ALL CUT AND FILL SLOPES SHALL BE MAXIMUM OF 2:1.

**EROSION, SEDIMENTATION & POLLUTION CONTROL PLAN NOTES**

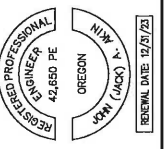
1. PROJECT/PURPOSE - WITH THE PURPOSE OF DEMOLITION, GRADING AND INSTALLATION OF ACCESS ROADS AND PARKING AREA FOR THE COMMERCIAL AREA OF THE PORT OF BROOKINGS.
2. CONTRACTOR ACTIVITIES - CONTRACTOR ACTIVITIES ARE DESCRIBED AS EXCAVATION AND CLEARING USING A 25T EXCAVATOR. EXCAVATION AND SPREADING AT THE BASE OF ROADS AND PARKING AREA. PLACEMENT OF GEOGRID. PLACEMENT OF CRUSHED AGGREGATE .
3. SOIL DISTURBING ACTIVITIES - EXCAVATION WILL BE LIMITED TO EXISTING AREAS SHOWN ON THE PLANS.
4. NON-STORMWATER DISCHARGES - NO DEWATERING, WATER-LINE FLUSHING, PAVEMENT WASH WATERS OR IRRIGATION WATER DISCHARGES ARE PLANNED FOR THIS PROJECT.
5. ESTIMATED START DATE FOR CONSTRUCTION - 06/22
6. NEAREST SURFACE WATER BODIES - PORT OF BROOKINGS ICE HOUSE INLET IN BASIN 2 (SOUTH BASIN) AND BASIN 1, NEAR DOCK A (NORTH BASIN).
7. RECEIVING WATERS - PACIFIC OCEAN
8. SPECIAL ENVIRONMENTAL CONSIDERATIONS - SEE NOTE 10
9. DESIGNATED EPCM - THE DESIGNATED EROSION AND POLLUTION CONTROL MANAGER (EPCM) WHO WILL ASSURE COMPLIANCE WITH ALL ITEMS IN THIS PLAN IS THE CONTRACTOR PERFORMING INSTALLATION OF REQUIRED BMP'S.
10. EROSION, SEDIMENTATION AND POLLUTION CONTROL BMP'S - BEST MANAGEMENT PRACTICES (BMP) TO BE USED, WHEN APPLICABLE, TO PREVENT POLLUTION RELATED TO CONTRACTOR ACTIVITIES LISTED IN THIS SECTION ARE AS FOLLOWS: A) OFFSITE VEHICLE TRACKING AND DUST PREVENTION - MEASURES WILL BE TAKEN TO PREVENT OFFSITE TRACKING OF MATERIALS, INCLUDING SWEEPING PAVEMENTS, COVERING LOADS AND WETTING SOIL TO PREVENT DUST. THERE WILL BE NO AGGREGATE CONSTRUCTION. B) MATERIAL MANAGEMENT AND SPILL PREVENTION - ALL ON SITE FUELS WILL BE DELIVERED, HANDLED, STORED, USED, AND APPLIED SO AS NOT TO BE RELEASED INTO THE WATERS OF THE STATE/US. FUELING WILL BE ACCOMPLISHED AWAY FROM THE WORK AREA A SPILL CLEANUP KIT WILL BE AVAILABLE IF DEEMED BY THE EPCM TO BE REQUIRED. C) WASTE MANAGEMENT - HANDLING, STORAGE AND DISPOSAL OF SOLID WASTE AND/OR HAZARDOUS WASTE WILL BE DISPOSED INTO SUITABLE LANDFILL OFFSITE D) INSPECTION AND MAINTENANCE - DAILY INSPECTION AND MAINTENANCE FOR ALL CONTROLS INCLUDED IN THE POLLUTION CONTROL PLAN AND THE ESCP WILL BE PERFORMED BY THE EPCM OR HIS DESIGNEE. E) EMPLOYEE AND SUBCONTRACTOR TRAINING - EMPLOYEE AND SUBCONTRACTOR EDUCATION AT A MINIMUM WILL INCLUDES INFORMING PERSONNEL OF THE POSTED LOCATIONS OF THE POLLUTION CONTROL PLAN/EROSION AND SEDIMENT CONTROL PLAN/MSDS'S AND IMPORTANT EMERGENCY PHONE NUMBERS. EDUCATION WILL ALSO INCLUDE INFORMING PERSONNEL OF REVISED MATERIAL MANAGEMENT PROCEDURES FOLLOWING A SPILL F) (CRITERIA 15) PRECONSTRUCTION ACTIVITY - BEFORE ALTERATION OF THE ACTION AREA, FLAG THE BOUNDARIES OF CLEARING LIMITS ASSOCIATED WITH SITE ACCESS AND CONSTRUCTION TO MINIMIZE SOIL AND VEGETATION DISTURBANCE, AND ENSURE THAT ALL TEMPORARY EROSION CONTROLS ARE IN PLACE AND FUNCTIONAL. G) (CRITERIA 16) SITE PREPARATION - DURING SITE PREPARATION, CONSERVE NATIVE MATERIALS FOR RESTORATION, INCLUDING LARGE WOOD, VEGETATION, TOPSOIL AND CHANNEL MATERIALS (GRAVEL, COBBLE AND BOULDERS) DISPLACED BY CONSTRUCTION. WHENEVER PRACTICAL, LEAVE NATIVE MATERIALS WHERE THEY ARE FOUND AND IN AREAS TO BE CLEARED, CLIP VEGETATION AT GROUND LEVEL TO RETAIN ROOT MASS AND ENCOURAGE REESTABLISHMENT OF NATIVE VEGETATION. BUILDING AND RELATED STRUCTURES MAY NOT BE CONSTRUCTED INSIDE THE RIPARIAN MANAGEMENT AREA H) (CRITERIA 17) HEAVY EQUIPMENT - HEAVY EQUIPMENT WILL BE SELECTED AND OPERATED AS NECESSARY TO MINIMIZE ADVERSE EFFECTS ON THE ENVIRONMENT; AND ALL VEHICLES AND OTHER HEAVY EQUIPMENT WILL BE USED AS FOLLOWS:
  - (A.) STORED, FUELED AND MAINTAINED IN A VEHICLE STAGING AREA PLACED 150 FEET OR MORE FROM ANY WATER BODY, OR IN AN ISOLATED HARD ZONE SUCH AS A PAVED PARKING LOT.
  - (B.) INSPECTED DAILY FOR FLUID LEAKS BEFORE LEAVING THE VEHICLE STAGING AREA FOR OPERATION WITHIN 50 FEET OF ANY WATER BODY.
  - (C.) STEAM-CLEANED BEFORE OPERATION BELOW ORDINARY HIGH WATER, AND AS OFTEN AS NECESSARY DURING OPERATION TO REMAIN FREE OF ALL EXTERNAL OIL, GREASE, MUD, SEEDS, ORGANISMS AND OTHER VISIBLE CONTAMINANTS.
  - (D.) GENERATORS, CRANES AND ANY OTHER STATIONARY EQUIPMENT OPERATED WITHIN 150 FEET OF ANY WATER BODY WILL BE MAINTAINED AND PROTECTED AS NECESSARY TO PREVENT LEAKS AND SPILLS FROM ENTERING THE WATER I) (CRITERIA 18)

**GEOTECHNICAL NOTE**

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE PROJECT ENGINEER FOR REQUIRED REMEDIATION. THE CONTRACTOR SHALL COORDINATE WITH THE PROJECT ENGINEER FOR REQUIRED SITE OBSERVATIONS AND TESTING OF ALL FILLS.

REVISIONS	BY:	DATE
	JA	

Grants Pass • Jacksonville • Medford, OR  
 CP Office 1807 William Hwy. Suite 214 Grants Pass, OR 97527  
 JV Office 480 Cottage Dr., Jacksonville, OR 97530  
 Ph: 541-772-9141 • Cell: 541-262-9919 • Fax: 541-772-5488  
 info@emcsciences.com • <http://www.emcsciences.com>  
**Engineers/Scientists, LLC**



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C1.0**  
 GENERAL NOTES

## STORM DRAIN NOTES

1. ALL STORM SEWER PIPE SHALL MEET THE OREGON STATE PLUMBING SPECIALTY CODE.
2. ALL PIPE SHALL BE PLACED ON STABLE EARTH, OR IF IN THE OPINION OF THE PROJECT ENGINEER, THE EXISTING FOUNDATION IS UNSATISFACTORY, THEN IT SHALL BE EXCAVATED BELOW GRADE AND BACKFILLED WITH A GRAVEL MATERIAL TO SUPPORT THE PIPE.
3. THE BACKFILL SHALL BE PLACED EQUALLY ON BOTH SIDES OF THE PIPE IN LAYERS WITH A LOOSE AVERAGE DEPTH OF 6", MAXIMUM DEPTH 8"-9" THOROUGHLY TAMPING EACH LAYER. THESE COMPACTED LAYERS MUST EXTEND FOR ONE DIAMETER ON EACH SIDE OF THE PIPE OR TO THE OTHER SIDE OF THE TRENCH. MATERIALS TO COMPLETE THE FILL OVER THE PIPE SHALL BE THE SAME AS DESCRIBED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL (AS APPLICABLE) MANHOLE, INLET AND CATCH BASIN FRAMES AND GRATES TO GRADE PRIOR TO PAVING.
3. UNLESS OTHERWISE NOTED, ALL STORM SEWER PIPE SHALL BE CORRUGATED POLYETHYLENE PIPE. THE MATERIAL SUPPLIED UNDER THIS SPECIFICATION SHALL BE HIGH DENSITY CORRUGATED POLYETHYLENE SMOOTH INTERIOR PIPE AND SHALL BE MANUFACTURED IN CONFORMITY WITH THE LATEST AASHTO SPECIFICATIONS. COUPLERS SHALL COVER NOT LESS THAN ONE FULL CORRUGATION ON EACH ANNULAR SECTION OF PIPE.
6. PIPE ENDS AT OUTFALLS SHALL BE BEVELED TO MATCH SIDE SLOPES. FIELD CUT OF PIPE ENDS IS PERMITTED WHEN APPROVED BY THE COUNTY/PORT ENGINEER OR HIS DESIGNATED REPRESENTATIVE. PIPE OUTFALLS SHALL BE RIPRAPPED WITH A PAD MINIMUM OF 12" THICK, EXTENDING MINIMUM OF 6' FROM DISCHARGE POINT.
7. ALL STEEL PARTS OF ANY STORM DRAINAGE SYSTEM SHALL BE GALVANIZED OR HAVE A TREATMENT 1 ASPHALT COATING OR BETTER AS SPECIFIED IN THE ODOT STANDARD SPECIFICATIONS. ALUMINUM AND CONCRETE PIPES AND STRUCTURES DO NOT REQUIRE A TREATMENT 1 COATING.
8. STORM DRAINAGE PIPE AND CATCH BASINS SHALL BE FLUSHED AND CLEANED PRIOR TO ENGINEER/JURISDICTIONAL ACCEPTANCE.
9. ALL PIPES SHALL MINIMUM OF 12" COVER AT THE TOP OF THE BELL, OR PIPES SHALL HAVE MINIMUM COVER PER THE MANUFACTURER'S SPECIFICATIONS, WHICHEVER IS GREATER.
10. CATCH BASIN STATIONS AND OFFSETS ARE MEASURED TO CENTER OF GRATE. FIELD ADJUSTMENTS MAY BE REQUIRED.
11. 100-FT MAX LINEAR RUN BETWEEN CLEANOUTS. 135' MAX AGGREGATE HORIZONTAL CHANGE IN DIRECTION WITHOUT CLEANOUT.
12. ENGINEER TO VERIFY ALL EXISTING AND PROPOSED CB FINISH TOP OF GRATE ELEVATIONS. ADJUSTMENTS MAY BE REQUIRED IN THE FIELD DURING CONSTRUCTION BASED UPON EXISTING / FINISH GRADES.

INSPECTION TESTING & FREQUENCY TABLE			
SEE NOTE 1			
STREETS, PARKING LOTS, FILLS, TRENCHES, ETC.			
MATERIAL	FREQUENCY	MIN. NUMBER OF TESTS	NOTES
SUB-GRADE	1 TEST PER 4,000 SF PER LIFT	4	2, 3, & 5
ENGINEERED FILL	1 TEST PER 4,000 SF PER LIFT	4	2 & 4
BASEROCK	1 TEST PER 4,000 SF PER LIFT	4	2, 3, & 5
ASPHALT	1 TEST PER 6,000 SF PER LIFT	4	2 & 5
TRENCH BACKFILL	1 TEST PER 200 LIN. FT. PER LIFT	4	2
TRENCH ASPHALT PATCHING	1 TEST PER 300 LIN. FT. PER LIFT	4	2
CONCRETE			
SLUMP, AIR AND CYLINDERS FOR ALL SITE CONCRETE AND PCC PAVEMENT, UNLESS OTHERWISE SPECIFIED, ONE SET OF CYLINDERS PER 100 CUBIC YARDS OR PORTION THEREOF OF CONCRETE POURED PER DAY. SLUMP AND AIR TESTS ARE REQUIRED ON SAME LOAD AS CYLINDERS.			2 & 5
BUILDING PERMIT INSPECTION AND SPECIAL INSPECTIONS FOR STRUCTURAL CONCRETE, MASONRY, EPOXY ANCHORS, ETC., AS REQUIRED BY PROJECT STRUCTURAL ENGINEER AND CURRENT BUILDING CODES.			2 & 5
ENGINEER TO INSPECT FORMS PRIOR TO PLACEMENT OF CONCRETE.			5
UNDERGROUND VAULTS, MANHOLES & STORMWATER DETENTION SYSTEMS			
PROVIDE ENGINEER WITH AS-BUILT SURVEY PRIOR TO BACKFILL. INSPECTIONS BY ENGINEER REQUIRED PRIOR TO BACKFILL.			5

## INSPECTION AND TESTING NOTES

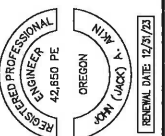
1. CONTRACTOR IS RESPONSIBLE FOR SCHEDULING ALL TESTING, INSPECTIONS, AND SPECIAL INSPECTIONS, AND SPECIAL INSPECTIONS AS REQUIRED BY PROJECT ENGINEER, CURRENT BUILDING CODES, OR JURISDICTIONS HAVING AUTHORITY. ALL TESTING MUST BE COMPLETED AND APPROVED PRIOR TO SUBSEQUENT WORK. ADDITIONAL OR FREQUENT TESTS MAY BE REQUIRED BY AGENCY, BUILDING OFFICIAL OR ENGINEER.
2. TESTING MUST BE PERFORMED BY AN APPROVED INDEPENDENT TESTING LABORATORY RETAINED BY THE CONTRACTOR.
3. IN ADDITION TO IN-PLACE DENSITY TESTING, THE SUB-GRADE AND BASE ROCK SHALL BE PROOF-ROLLED WITH A LOADED DUMP TRUCK OR HEAVY NON-VIBRATORY ROLLER. SOILS SHALL BE REMOVED AND RE-COMPACTED OR REPLACED WITH APPROVED IMPORTED STRUCTURAL FILL IF THEY DO NOT DEMONSTRATE A FIRM, UNYIELDING CONDITION. BASE ROCK PROOF-ROLL SHALL TAKE PLACE LESS THAN 24 HOURS PRIOR TO PAVING AND SHALL BE WITNESSED BY THE ENGINEER OR GOVERNING AGENCY.
4. THE APPROVED INDEPENDENT LABORATORY SHALL PROVIDE CLARIFICATION STAMPED BY AN ENGINEER LICENSED IN THE STATE OF OREGON THAT THE SUB-GRADE IS PREPARED AND ALL ENGINEERED FILLS ARE PLACED IN ACCORDANCE WITH THE CONTRACT DRAWINGS AND DOCUMENTS.
5. PROVIDE ENGINEER WITH SPOT ELEVATION VERIFICATION FOR SUB-GRADE AND TOP OF AGGREGATE PRIOR TO PLACING CONCRETE, ASPHALT, AND/OR OTHER STRUCTURES (WHEN INCLUDED IN THE PROJECT).

## CONCRETE STANDARDS

1. EXCEPT AS OTHERWISE NOTED OR DEFINED BY COUNTY AND/OR PROJECT ENGINEER APPROVAL, ALL CONCRETE SHALL CONFORM TO SECTIONS 00440 AND 00759 OF THE CURRENT OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION MANUAL.
2. CONCRETE SHALL NOT BE PLACED UNTIL FORMS HAVE BEEN INSPECTED.
3. INSPECTION REQUESTS MUST BE MADE 24 HOURS PRIOR TO DATE OF INSPECTION. TO SCHEDULE AN INSPECTION, CALL PORT OF BROOKINGS HARBOR, PHONE: 541-469-2218.
4. CONCRETE SHALL BE COMMERCIAL GRADE RETAINING THE FOLLOWING CHARACTERISTICS:  
ENTRAINED AIR - 4.0% TO 7.0%  
SLUMP - 5 INCHES OR LESS  
COMPRESSIVE STRENGTH - MINIMUM 3,000 PSI AT 28 DAYS  
TEMPERATURE - MINIMUM 50° F TO MAXIMUM 90° F.
5. ALL CONCRETE SHALL BE FORMED ON A MINIMUM 95% RD COMPACTED BASE OF 1/2"-0" CRUSHED AGGREGATE. DEPTH OF BASE VARIES WITH STRUCTURE. MINIMUM 4" COMPACTED BASE FOR SIDEWALKS, RAMPS, AND APPROACHES. MINIMUM 6" BASE FOR CURB, GUTTER, VALLEY GUTTERS AND INLETS.
6. SAFETY YELLOW TRUNCATED DOME DETECTABLE WARNING SURFACES ARE REQUIRED ON ALL SIDEWALK RAMPS AND ACCESSIBLE ROUTE ISLANDS.
7. CONCRETE EXTRUDING MACHINES SHALL OPERATE UNDER SUFFICIENT RESTRAINT TO FORWARD MOTION TO PRODUCE A WELL-CONSOLIDATED MASS OF CONCRETE.
8. ALL CONCRETE STRUCTURES REINFORCED WITH REINFORCING BARS SHALL BE VIBRATED TO REMOVE VOIDS AS APPLICABLE.
9. SURFACE SHALL HAVE A FINISHED TEXTURE THAT WILL NOT BE SLICK WHEN WET (MEDIUM BROOM FINISH). CURING COMPOUND MAY BE APPLIED IMMEDIATELY AFTER CONCRETE IS FINISHED. WHITE PIGMENT RECOMMENDED, CLEAR ACCEPTABLE.
10. AN EDGING TOOL SHALL BE USED ON ALL EDGES AND JOINTS.
11. PROVIDE CONTRACTION JOINTS AT 15' INTERVALS AND "FALSE" TOOLED JOINTS AT 5' INTERVALS ON CURBS, SIDEWALKS AND APPROACHES. CONTRACTION JOINT GROOVES SHALL BE AT MINIMUM, 1-1/2" DEEP OR ONE-THIRD THE THICKNESS OF CONCRETE.
12. PROVIDE EXPANSION JOINTS OPPOSITE ABUTTING EXPANSION JOINTS IN ABUTTING CONCRETE, AT EACH POINT OF TANGENCY IN THE STRUCTURE ALIGNMENT, BETWEEN DRIVEWAYS AND CONCRETE PAVEMENT, AROUND POLES, POSTS, BOXES, AND OTHER FIXTURES WHICH PROTRUDE THROUGH OR AGAINST THE STRUCTURES, AT ALL BCR'S AND ECR'S, AT MAXIMUM OF 100' INTERVALS. EXPANSION JOINT MATERIAL SHALL BE OF THE BITUMINOUS, PREFORMED FILLER TYPE NOT LESS THAN 1/2" WIDE, PLACED FLUSH OR NO MORE THAN 1/2" BELOW THE CONCRETE SURFACE.
13. STRAIGHT LINE EDGES SHALL NOT VARY MORE THAN 1/4" UNDER A 12 FT. STRAIGHT EDGE.
14. CURE AND PROTECT CONCRETE AFTER PLACING AND FINISHING. KEEP STRUCTURES FREE FROM CONTACT, STRAIN AND PUBLIC TRAFFIC FOR AT LEAST SEVEN DAYS OR LONGER AS DIRECTED. MIXES TO EXPEDITE CURING MAY BE USED WITH APPROVAL OF COUNTY ENGINEER.

REVISIONS	BY:	DATE
	JA	

Counts Pass • Jacksonville • Medford, OR  
 1400 NE 15th Street, Suite 214, Grants Pass, OR 97526  
 Phone: 541-772-5488 • Fax: 541-772-5489  
 Email: info@emciscientists.com • Website: www.emciscientists.com



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C1.01**  
 GENERAL NOTES



**1** EXISTING CONDITIONS  
SCALE 1" = 50'



**LEGEND**

(17)	CONTOUR LABEL EX.
15'	CONTOUR LABEL PROP. FG
---	CONTOUR EX.
---	CONTOUR PROP. SUBGRADE
---	FENCE LINE
---	BURIED PIPELINE
---	EXPOSED PIPELINE
---	ROW
---	ROAD CENTERLINE
---	EXTENTS SURVEY
---	SURFACE WATER FLOW
---	SURFACE BREAKLINE
---	CULTURAL VILLAGE HERITAGE SITE
+	SPOT ELEVATION
DI	AREA DRAIN
PP	TRANSFORMER
PP	POWER POLE
PP	TELEPHONE POLE
LP	STREET LIGHT
GUY	GUY
WV	WATER VALVE
WM	WATER METER
FH	FIRE HYDRANT
SSM	SANITARY SEWER MANHOLE
SSC	SANITARY SEWER CLEAN OUT
UE	UNDERGROUND ELECTRIC

REVISIONS	BY:	DATE
	JA	

Grants Pass • Jacksonville • Medford, OR  
 272 Office 482 Williams Hwy. Suite 214 Grants Pass, OR 97527  
 272 Office 450 Connelley Dr., Jacksonville, OR 97530  
 Ph: 541-772-9434 • Cell: 541-238-9959 • Fax: 541-772-5488  
 emc@emcengineers.com • www.emcengineers.com



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

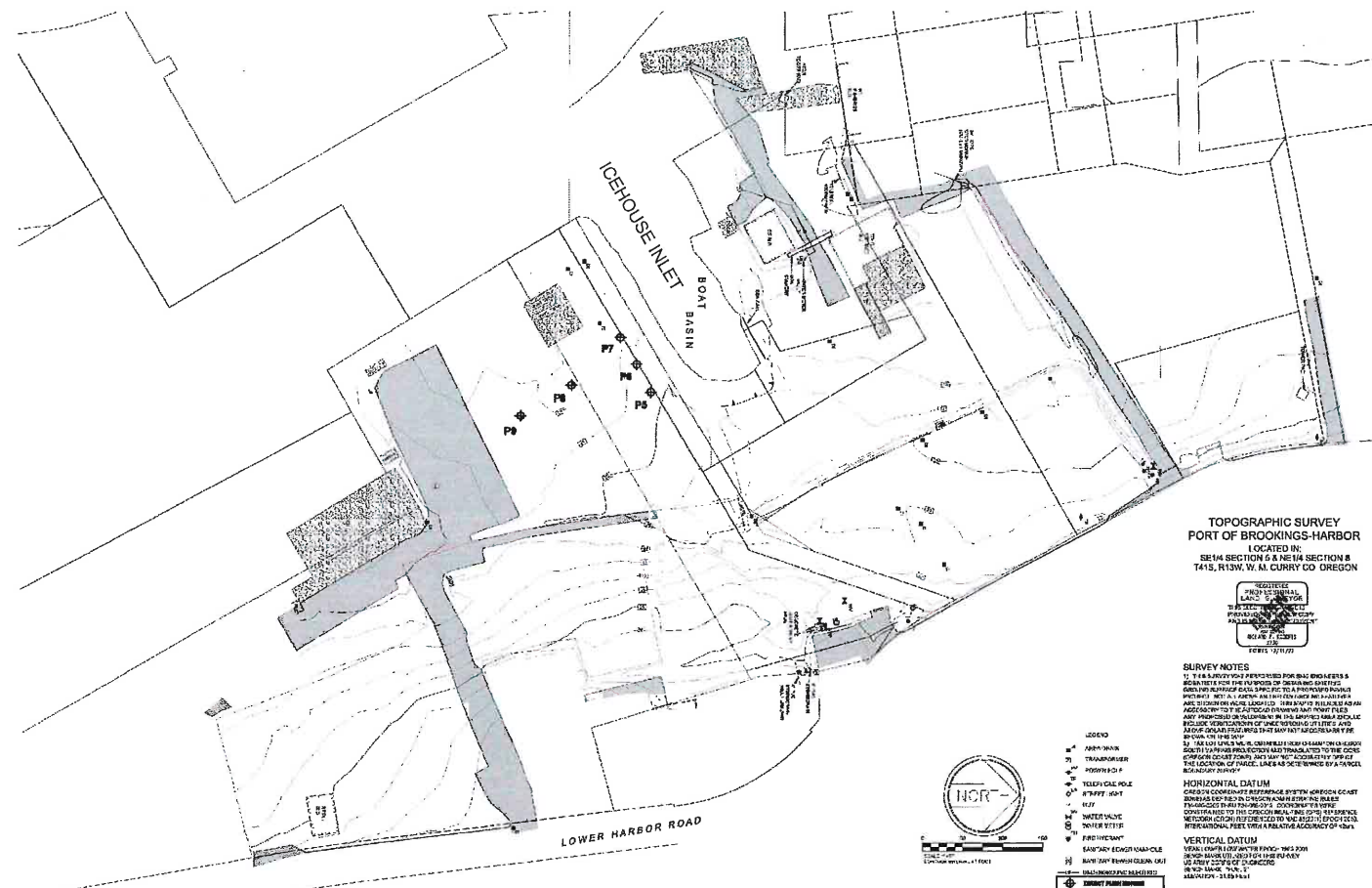
DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:

**C1.1**  
 EXISTING  
 CONDITIONS



# COMMERCIAL AREA DCP LOCATIONS

NOTE:  
DCP AREAS REPRESENTED BY POINTS LESS  
THAN 2000 LBS/FT WILL NEED  
SUBBASE/SUBGRADE REINFORCEMENT



TOPOGRAPHIC SURVEY  
PORT OF BROOKINGS HARBOR  
LOCATED IN:  
SE 1/4 SECTION 5 & NE 1/4 SECTION 8  
T41S, R13W, W. A. CURRY CO. OREGON

EMCI  
Engineers/Scientists, LLC  
910 S. Commercial Blvd. Suite 214, Brookings, OR 97530  
Phone: 541-775-5488  
Fax: 541-775-5489  
www.emciscientists.com

REVISIONS  
BY: JA  
DATE:

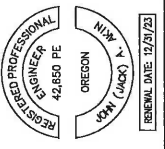
SEE SHEET C1.1.2 FOR DCP TEST RESULTS



### DCP DATA SHEET

Project POB: Sediment Basin Road					Project				
Material Class GW-CL					Material Class				
Pavement None					Pavement				
Moisture 9.70%					Moisture				
Date/Time 2/16/2022 11:55 AM					Date/Time				
Personnel RG					Personnel				
Hammer Wt. 10 lbs					Hammer Wt.				
Weather Clear, Dry					Weather				
Site ID	Station	Depth	Blows	PSF	Site ID	Station	Depth	Blows	PSF
Area 2	1	6	8	1580					
Area 2	2	4	13	3070					
Area 2	3	2	>15	>5700					
Area 2	4	2	>15	>5700					
Area 2	5	2	10	4420					
Area 2	6	6	6	1280					
Area 2	7	6	12	2140					
Area 2	8	6	5	1110					
Area 2	9	2	>15	>5700					
Area 2	10	2	>15	>5700					
Area 2	11	2	10	4420					
Area 2	12	6	6	1280					

PSF: Pounds per square Foot  
PFV Values from Table 3, Table 4, and Table 5 of KSE DCP Owners Manual



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COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
DATE: 15-JULY-2022  
JOB No: 022-2202  
SHEET No:  
**C1.1.1**  
DCP DATA

**GEOTECHNICAL HOLE COMPLETION LOG**

Project Name: Soil Assessment  
 Location: Port of Brookings Harbor, Oregon  
 Date Completed: March 24, 2022  
 Sample Method: Push Probe; Macrocore Sampler

Boring Identification: P5  
 Surface Elevation: Approx 20 feet AMSL  
 Total Depth: 5 Feet Below Ground Surface  
 Logged by: Rick Gates, G1912

DEPTH (feet)	SAMPLED INTERVAL	STATIC WATER LEVEL	COLLECTION INTERVAL AND RECOVERY	GRAPHIC	LITHOLOGIC DESCRIPTION
0					(0.0 to 0.8) - Gravel (GP); Poorly-graded, fine, angular gravel; 10% medium to coarse sand; 10% low plasticity fines, gray color; loosely compacted; class-supported; moist (0.8 to 2.0) - Elastic SIL (MH); Red brown elastic silt; 10% fine, angular gravel; 20% medium to coarse sand; low to medium plasticity; medium strength; loose; moist (2.0 to 4.5) - Elastic SIL (MH); Brown elastic silt; 20% fine to medium sand; 10% coarse gravel; 20% fine to medium sand; low to medium plasticity; medium to high strength; cemented; damp (4.5 to 5.0) - Gravel (GP); Poorly-graded, fine to medium, subrounded gravel; 30% fine to coarse sand; 30% low plasticity fines, gray color; tightly compacted; matrix-supported; moist
5					NOTES: Hole was backfilled with bentonite chips.
10					

**EMC** Engineers/Scientists, LLC  
 450 Coonanga Drive  
 Jacksonville, OR 97530

NOTES: Classification of soils based on ASTM D 2488 - 05. Group symbols represent the Unified Soils Classification System.  
 — Abrupt contact  
 — Gradual contact  
 Saturated Conditions  
 Static Water Level

**GEOTECHNICAL HOLE COMPLETION LOG**

Project Name: Soil Assessment  
 Location: Port of Brookings Harbor, Oregon  
 Date Completed: March 24, 2022  
 Sample Method: Push Probe; Macrocore Sampler

Boring Identification: P6  
 Surface Elevation: Approx 20 feet AMSL  
 Total Depth: 5 Feet Below Ground Surface  
 Logged by: Rick Gates, G1912

DEPTH (feet)	SAMPLED INTERVAL	STATIC WATER LEVEL	COLLECTION INTERVAL AND RECOVERY	GRAPHIC	LITHOLOGIC DESCRIPTION
0					(0.0 to 0.5) - Elastic SIL (MH); Brown elastic silt; 30% fine to medium, angular gravel; 10% fine to medium sand; medium plasticity; medium strength; loose; wet (0.5 to 1.5) - Sand (SW); Well-graded, medium to coarse sand; 20% fine, angular gravel; 10% low plasticity fines, gray color; loose; wet (1.5 to 3.2) - Lean Clay (CL); Gray brown lean clay; 20% fine, subrounded gravel; 20% fine sand; medium to high plasticity; medium strength; tight; saturated (3.2 to 5.0) - Clay (CV); Well-graded, fine to medium, subrounded gravel; 30% fine to coarse sand; 30% low plasticity fines, gray color; tightly compacted; matrix-supported; damp
5					NOTES: Hole was backfilled with bentonite chips.
10					

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 450 Coonanga Drive  
 Jacksonville, OR 97530

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 — Abrupt contact  
 — Gradual contact  
 Saturated Conditions  
 Static Water Level

**GEOTECHNICAL HOLE COMPLETION LOG**

Project Name: Soil Assessment  
 Location: Port of Brookings Harbor, Oregon  
 Date Completed: March 24, 2022  
 Sample Method: Push Probe; Macrocore Sampler

Boring Identification: P7  
 Surface Elevation: Approx 20 feet AMSL  
 Total Depth: 5 Feet Below Ground Surface  
 Logged by: Rick Gates, G1912

DEPTH (feet)	SAMPLED INTERVAL	STATIC WATER LEVEL	COLLECTION INTERVAL AND RECOVERY	GRAPHIC	LITHOLOGIC DESCRIPTION
0					(0.0 to 1.7) - Gravel (GW); Well-graded, fine, angular gravel; 25% medium to coarse sand; 15% medium plasticity fines, brown color; loosely compacted; class-supported; moist to wet (1.7 to 2.5) - Lean Clay (CL); Gray brown lean clay; 10% fine, subrounded gravel; 20% fine to medium sand; medium to high plasticity; medium strength; tight; saturated (2.5 to 5.0) - Gravel (GW); Well-graded, medium to coarse, subrounded gravel; 30% medium to coarse sand; 30% low plasticity fines, gray color; cemented; matrix-supported; damp
5					NOTES: Hole was backfilled with bentonite chips.
10					

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 450 Coonanga Drive  
 Jacksonville, OR 97530

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 — Gradual contact  
 Saturated Conditions  
 Static Water Level

**GEOTECHNICAL HOLE COMPLETION LOG**

Project Name: Soil Assessment  
 Location: Port of Brookings Harbor, Oregon  
 Date Completed: March 24, 2022  
 Sample Method: Push Probe; Macrocore Sampler

Boring Identification: P8  
 Surface Elevation: Approx 20 feet AMSL  
 Total Depth: 5 Feet Below Ground Surface  
 Logged by: Rick Gates, G1912

DEPTH (feet)	SAMPLED INTERVAL	STATIC WATER LEVEL	COLLECTION INTERVAL AND RECOVERY	GRAPHIC	LITHOLOGIC DESCRIPTION
0					(0.0 to 1.0) - Elastic SIL (MH); Red brown elastic silt; 10% fine to medium, angular gravel; 20% fine to medium sand; medium plasticity; low to medium strength; loose; wet (1.0 to 2.6) - Fat Clay (CH); Gray brown fat clay; 20% medium to coarse sand; high plasticity; medium to high strength; tight; saturated (2.6 to 5.0) - Gravel (GP); Poorly-graded, medium to coarse, subrounded gravel; 20% fine to medium sand; 40% medium plasticity fines, gray color; tight; matrix-supported; moist
5					NOTES: Hole was backfilled with bentonite chips.
10					

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 450 Coonanga Drive  
 Jacksonville, OR 97530

NOTES: Classification of soils based on ASTM D 2488 - 05. Group symbols represent the Unified Soils Classification System.  
 — Abrupt contact  
 — Gradual contact  
 Saturated Conditions  
 Static Water Level

**GEOTECHNICAL HOLE COMPLETION LOG**

Project Name: Soil Assessment  
 Location: Port of Brookings Harbor, Oregon  
 Date Completed: March 24, 2022  
 Sample Method: Push Probe; Macrocore Sampler

Boring Identification: P9  
 Surface Elevation: Approx 20 feet AMSL  
 Total Depth: 5 Feet Below Ground Surface  
 Logged by: Rick Gates, G1912

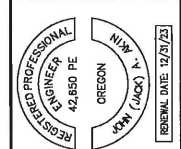
DEPTH (feet)	SAMPLED INTERVAL	STATIC WATER LEVEL	COLLECTION INTERVAL AND RECOVERY	GRAPHIC	LITHOLOGIC DESCRIPTION
0					(0.0 to 1.5) - Elastic SIL (MH); Red brown elastic silt; 30% fine, angular gravel; 20% medium to coarse sand; medium plasticity; medium strength; loose; wet (1.5 to 4.1) - Clay (CV); Well-graded, fine to medium, subrounded gravel; 30% medium to coarse sand; 20% medium plasticity fines, gray brown color; tight; matrix-supported; moist (4.1 to 5.0) - Gravel (GW); Well-graded, medium to coarse, subrounded gravel; 30% fine to medium sand; 30% low to medium plasticity fines, gray color; cemented; matrix-supported; moist
5					NOTES: Hole was backfilled with bentonite chips.
10					

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NOTES: Classification of soils based on ASTM D 2488 - 05. Group symbols represent the Unified Soils Classification System.  
 — Abrupt contact  
 — Gradual contact  
 Saturated Conditions  
 Static Water Level

REVISIONS	BY:	DATE
	JA	

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**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C1.1.2**  
 DCP DATA



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 Engineers/Scientists, LLC

REGISTERED PROFESSIONAL ENGINEER  
 #42850 PE  
 OREGON  
 NEW YORK (NYC)  
 RENEWAL DATE: 12/31/23

**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:

**C2.0**  
 DEMOLITION &

**LEGEND**

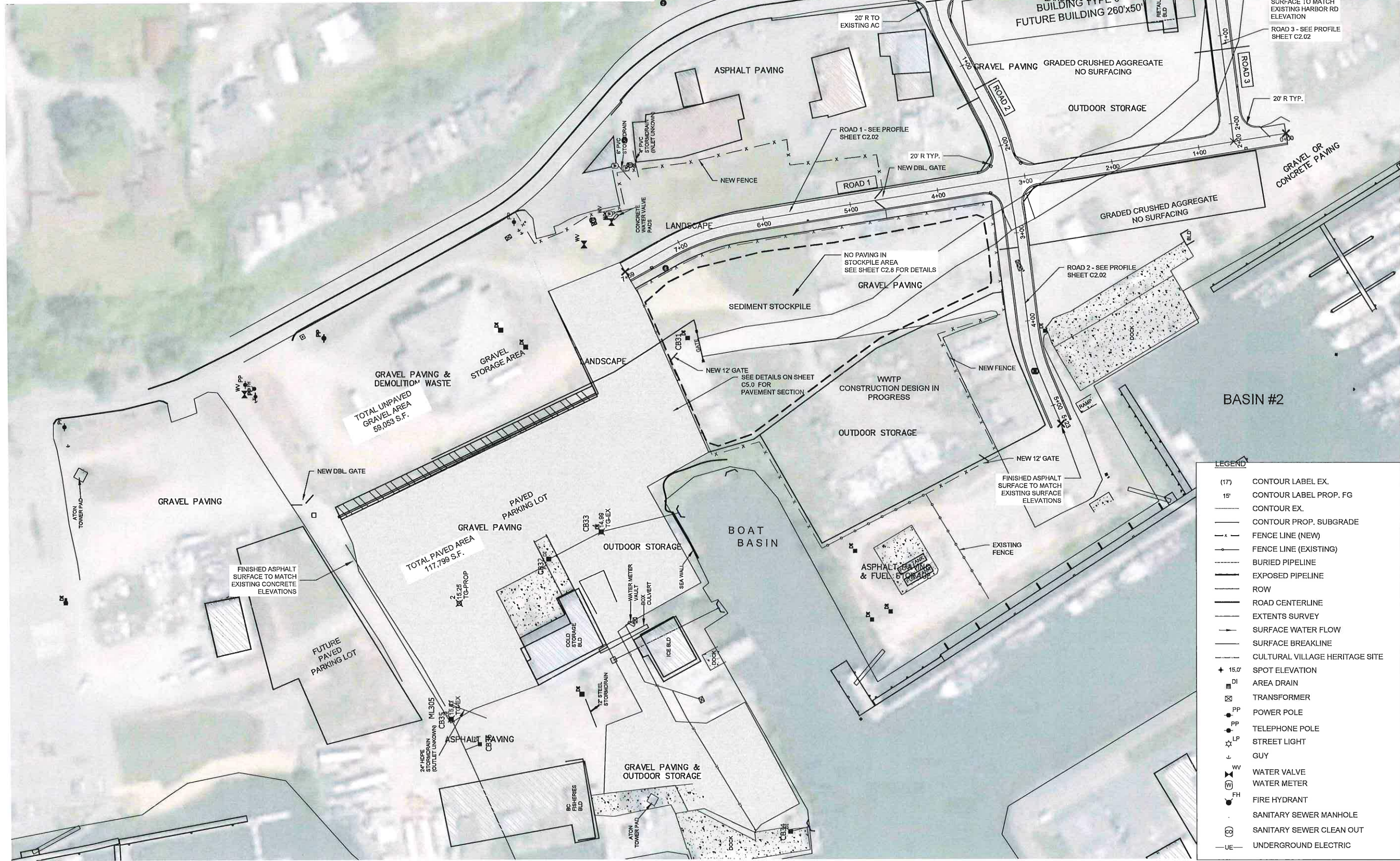
17.0	CONTOUR LABEL EX.
---	CONTOUR EX.
---	CONTOUR PROP. SUBGRADE
---	FENCE LINE
---	BURIED PIPELINE
---	EXPOSED PIPELINE
---	ROW
---	ROAD CENTERLINE
---	EXTENTS SURVEY
---	SURFACE WATER FLOW
---	SURFACE BREAKLINE
---	CULTURAL VILLAGE HERITAGE SITE
+ 15.0'	SPOT ELEVATION
DI	AREA DRAIN
⊠	TRANSFORMER
PP	POWER POLE
PP	TELEPHONE POLE
⊙ LP	STREET LIGHT
---	GUY
WV	WATER VALVE
WM	WATER METER
FH	FIRE HYDRANT
SM	SANITARY SEWER MANHOLE
SC	SANITARY SEWER CLEAN OUT
UE	UNDERGROUND ELECTRIC



**1 DEMOLITION & CLEARING PLAN**  
 SCALE 1" = 50'

**Cut/Fill Summary**

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
ROAD 1 VOLUME	1.000	1.000	25771.39 Sq. Ft.	636.52 Cu. Yd.	70.28 Cu. Yd.	566.24 Cu. Yd.<Cut>
ROAD 2 VOLUME	1.000	1.000	18485.16 Sq. Ft.	370.98 Cu. Yd.	227.61 Cu. Yd.	143.37 Cu. Yd.<Cut>
ROAD 3 VOLUME	1.000	1.000	11763.83 Sq. Ft.	229.25 Cu. Yd.	50.52 Cu. Yd.	178.74 Cu. Yd.<Cut>
STOCKPILE GRADING VOLUME	1.000	1.000	54412.63 Sq. Ft.	2031.75 Cu. Yd.	2471.36 Cu. Yd.	439.61 Cu. Yd.<Fill>
PARKING LOT VOLUME UPDATED	1.000	1.000	176679.82 Sq. Ft.	1054.57 Cu. Yd.	2945.78 Cu. Yd.	1891.22 Cu. Yd.<Fill>
<b>Totals</b>			<b>287112.83 Sq. Ft.</b>	<b>4323.08 Cu. Yd.</b>	<b>5765.55 Cu. Yd.</b>	<b>1442.47 Cu. Yd.&lt;Fill&gt;</b>



**LEGEND**

- (17) CONTOUR LABEL EX.
- 15' CONTOUR LABEL PROP. FG
- CONTOUR EX.
- CONTOUR PROP. SUBGRADE
- FENCE LINE (NEW)
- FENCE LINE (EXISTING)
- BURIED PIPELINE
- EXPOSED PIPELINE
- ROW
- ROAD CENTERLINE
- EXTENTS SURVEY
- SURFACE WATER FLOW
- SURFACE BREAKLINE
- CULTURAL VILLAGE HERITAGE SITE
- 15.0' SPOT ELEVATION
- DI AREA DRAIN
- TRANSFORMER
- PP POWER POLE
- PP TELEPHONE POLE
- LP STREET LIGHT
- GUY
- WV WATER VALVE
- WM WATER METER
- FH FIRE HYDRANT
- SSM SANITARY SEWER MANHOLE
- SSCO SANITARY SEWER CLEAN OUT
- UE UNDERGROUND ELECTRIC

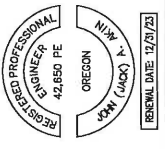
**1 SITE PLAN**  
SCALE 1" = 50'



REVISIONS

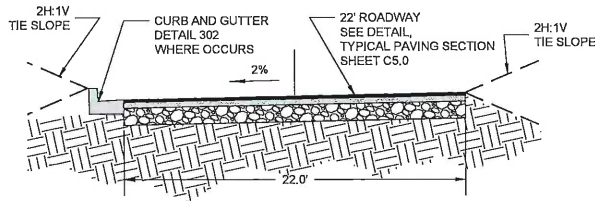
NO.	DATE	DESCRIPTION
1	JA	

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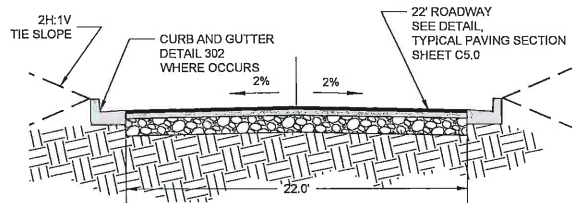


**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

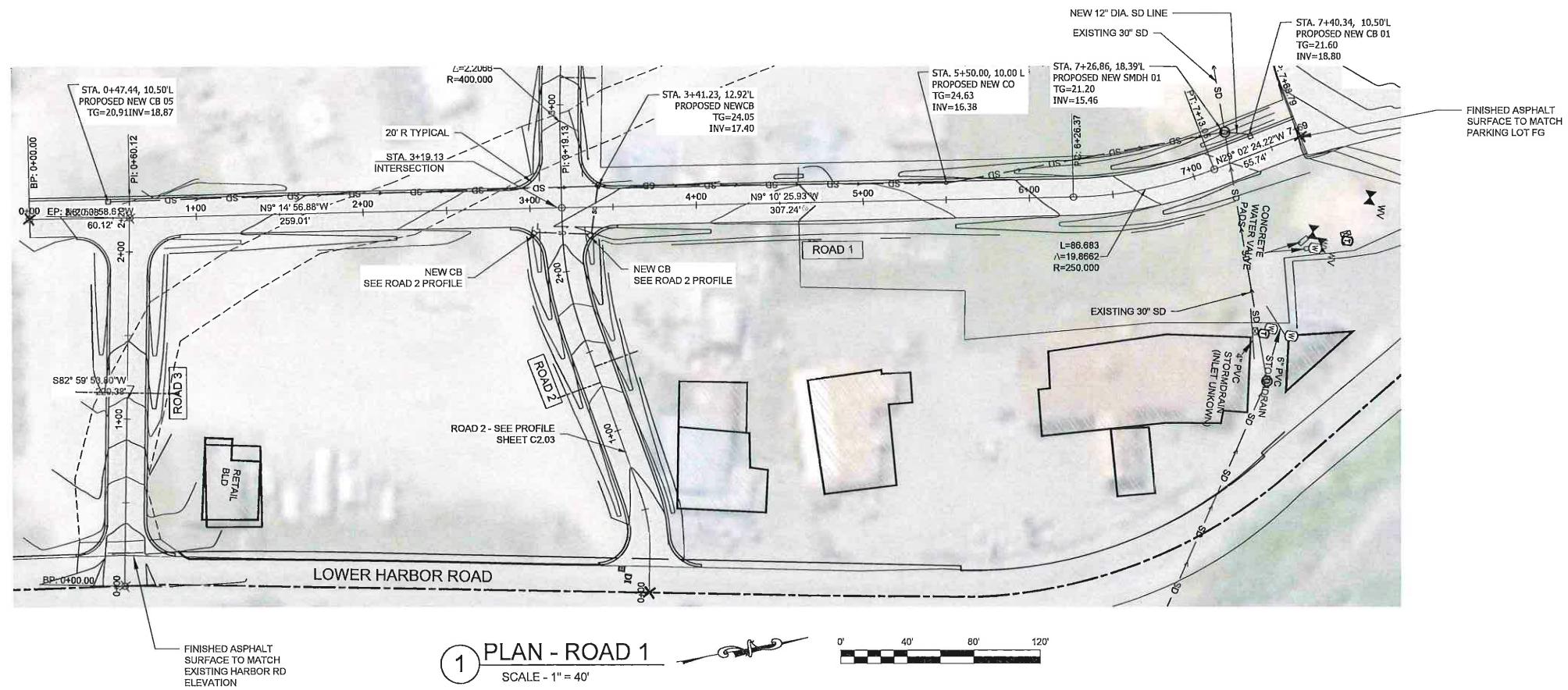
DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C2.01**  
 SITE PLAN



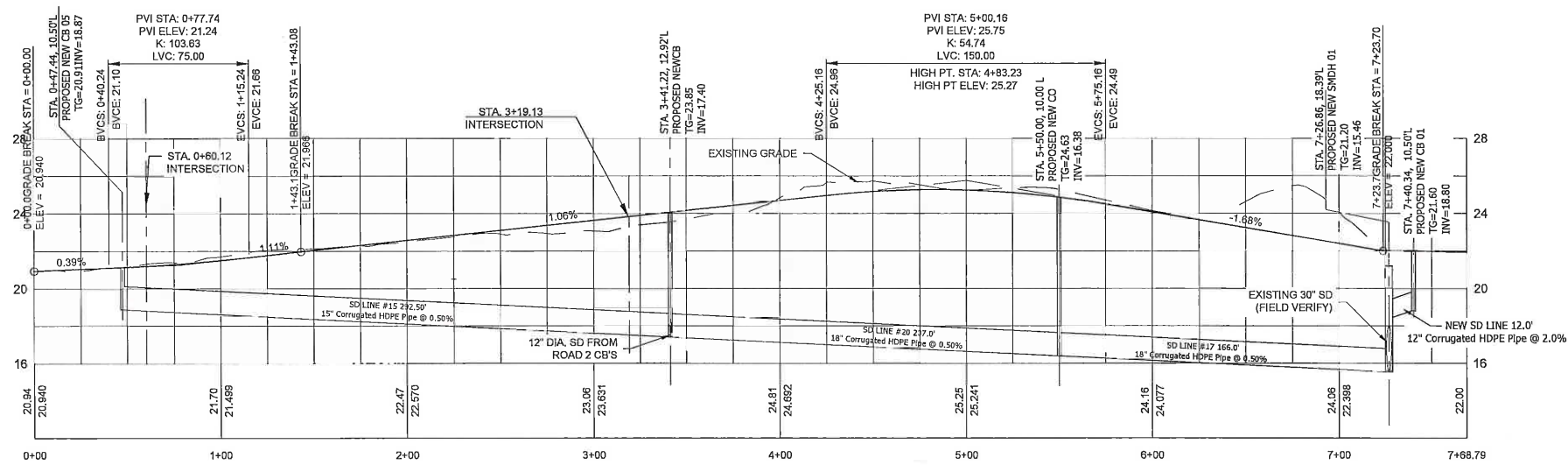
**A** ROAD SECTION - CURB ONE SIDE  
SCALE 1" = 5'



**B** ROAD SECTION - CURB BOTH SIDES  
SCALE 1" = 5'



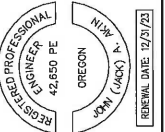
**1** PLAN - ROAD 1  
SCALE - 1" = 40'



**1** PROFILE - ROAD 1  
SCALE - 1" = 40' H 1" = 4' V

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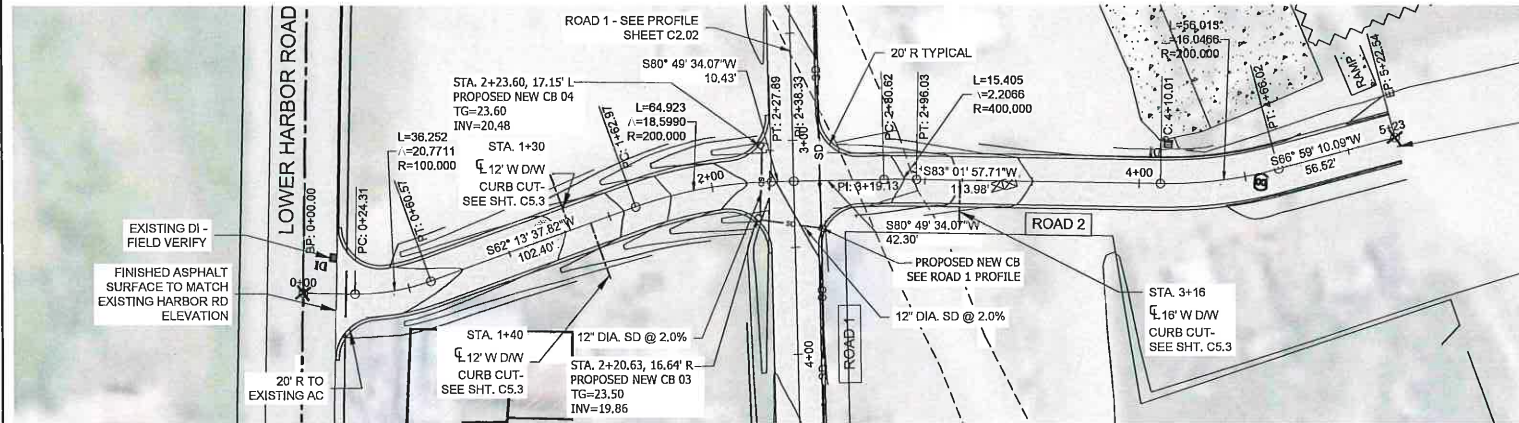


**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

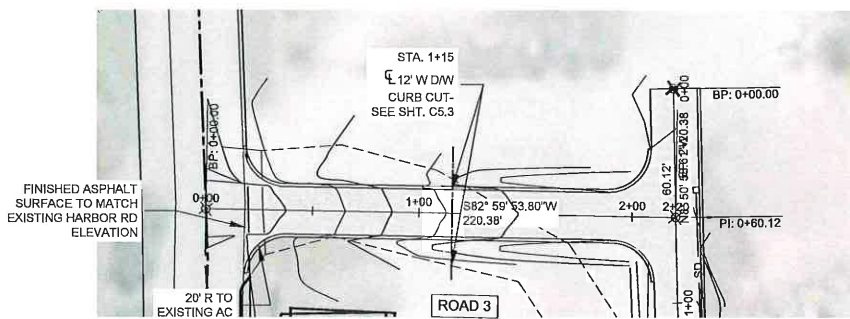
DRAWN BY: CD/JW  
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 JOB No: 022-2202  
 SHEET No:

**C2.02**  
 ROAD 1 PLAN  
 & PROFILES

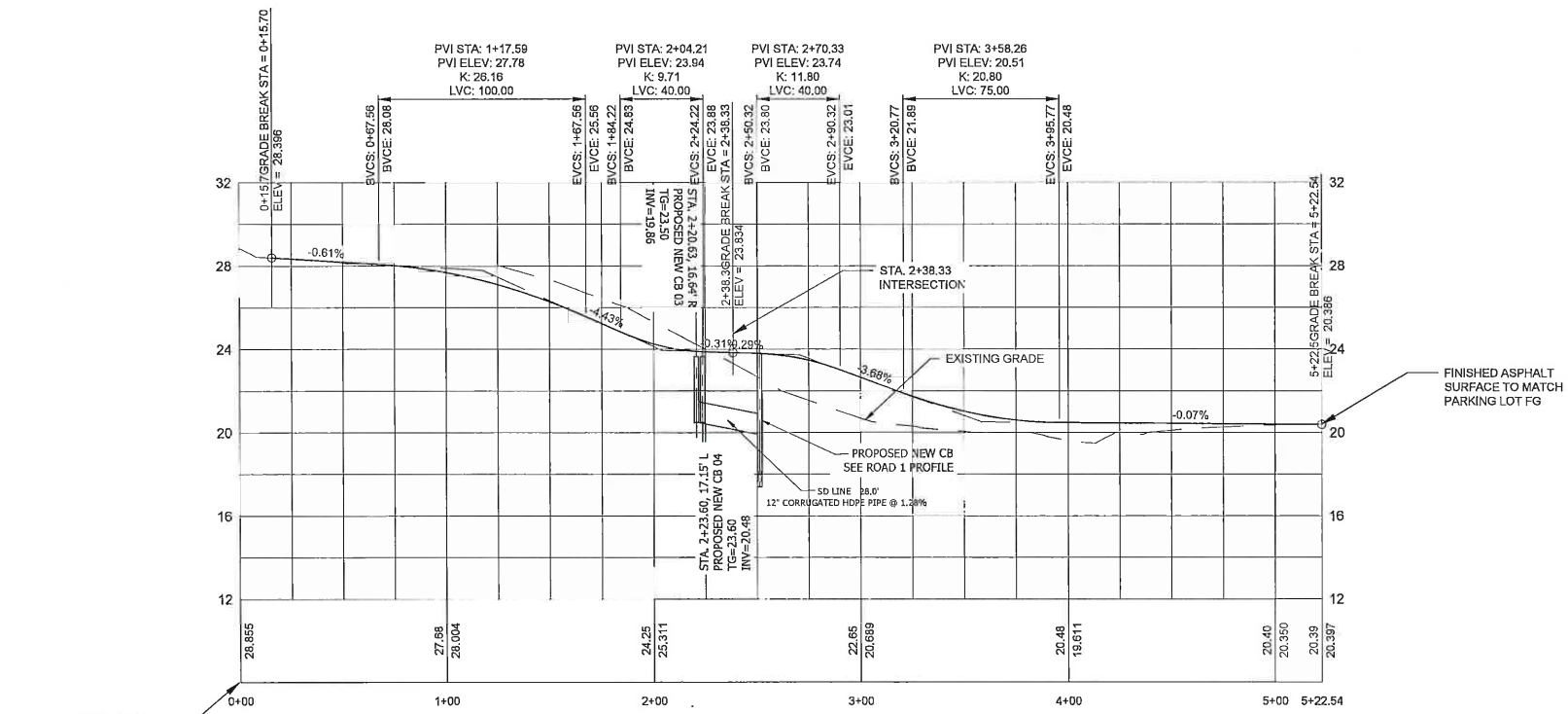
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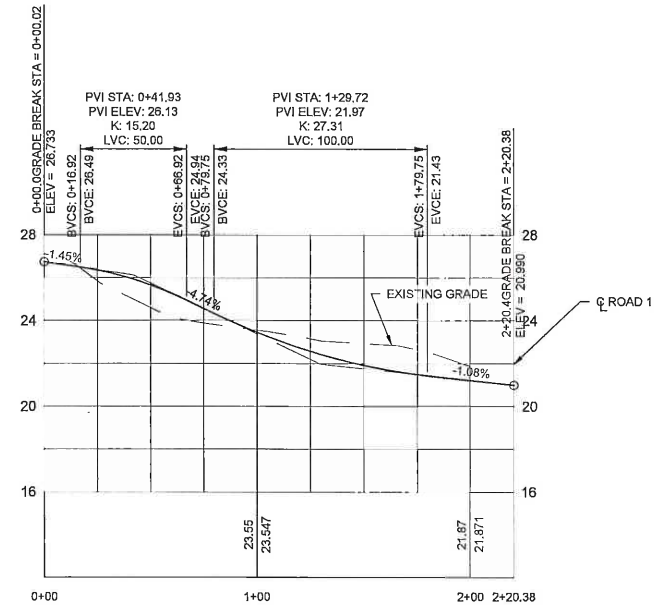
1 PLAN - ROAD 2  
SCALE - 1" = 40'



3 PLAN - ROAD 3  
SCALE - 1" = 40'



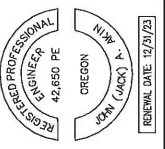
2 PROFILE - ROAD 2  
SCALE - 1" = 40' H 1" = 4' V



4 PROFILE - ROAD 3  
SCALE - 1" = 40' H 1" = 4' V

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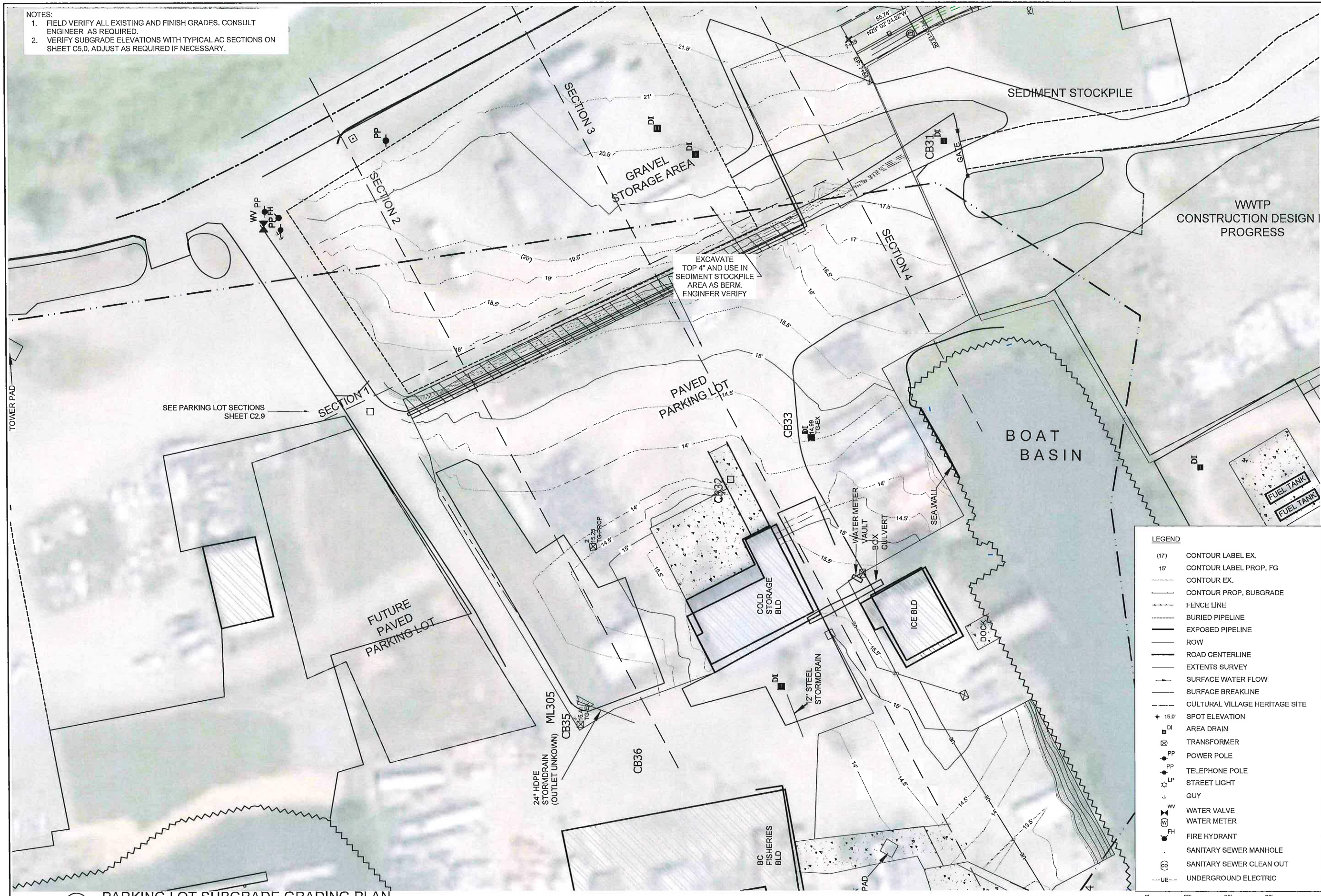


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 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:

**C2.03**  
 ROAD 2 & 3 PLAN  
 & PROFILES

NOTES:  
 1. FIELD VERIFY ALL EXISTING AND FINISH GRADES. CONSULT ENGINEER AS REQUIRED.  
 2. VERIFY SUBGRADE ELEVATIONS WITH TYPICAL AC SECTIONS ON SHEET C5.0. ADJUST AS REQUIRED IF NECESSARY.



**LEGEND**

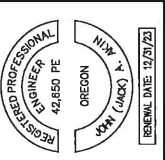
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15'	CONTOUR LABEL PROP. FG
---	CONTOUR EX.
---	CONTOUR PROP. SUBGRADE
---	FENCE LINE
---	BURIED PIPELINE
---	EXPOSED PIPELINE
---	ROW
---	ROAD CENTERLINE
---	EXTENTS SURVEY
---	SURFACE WATER FLOW
---	SURFACE BREAKLINE
---	CULTURAL VILLAGE HERITAGE SITE
+ 15.0'	SPOT ELEVATION
DI	AREA DRAIN
⊠	TRANSFORMER
PP	POWER POLE
PPH	TELEPHONE POLE
⊗	STREET LIGHT
⊥	GUY
WV	WATER VALVE
WM	WATER METER
FH	FIRE HYDRANT
SSM	SANITARY SEWER MANHOLE
SSCO	SANITARY SEWER CLEAN OUT
UE	UNDERGROUND ELECTRIC



**1** PARKING LOT SUBGRADE GRADING PLAN  
 SCALE 1" = 30'

BY:	JA
REVISIONS	

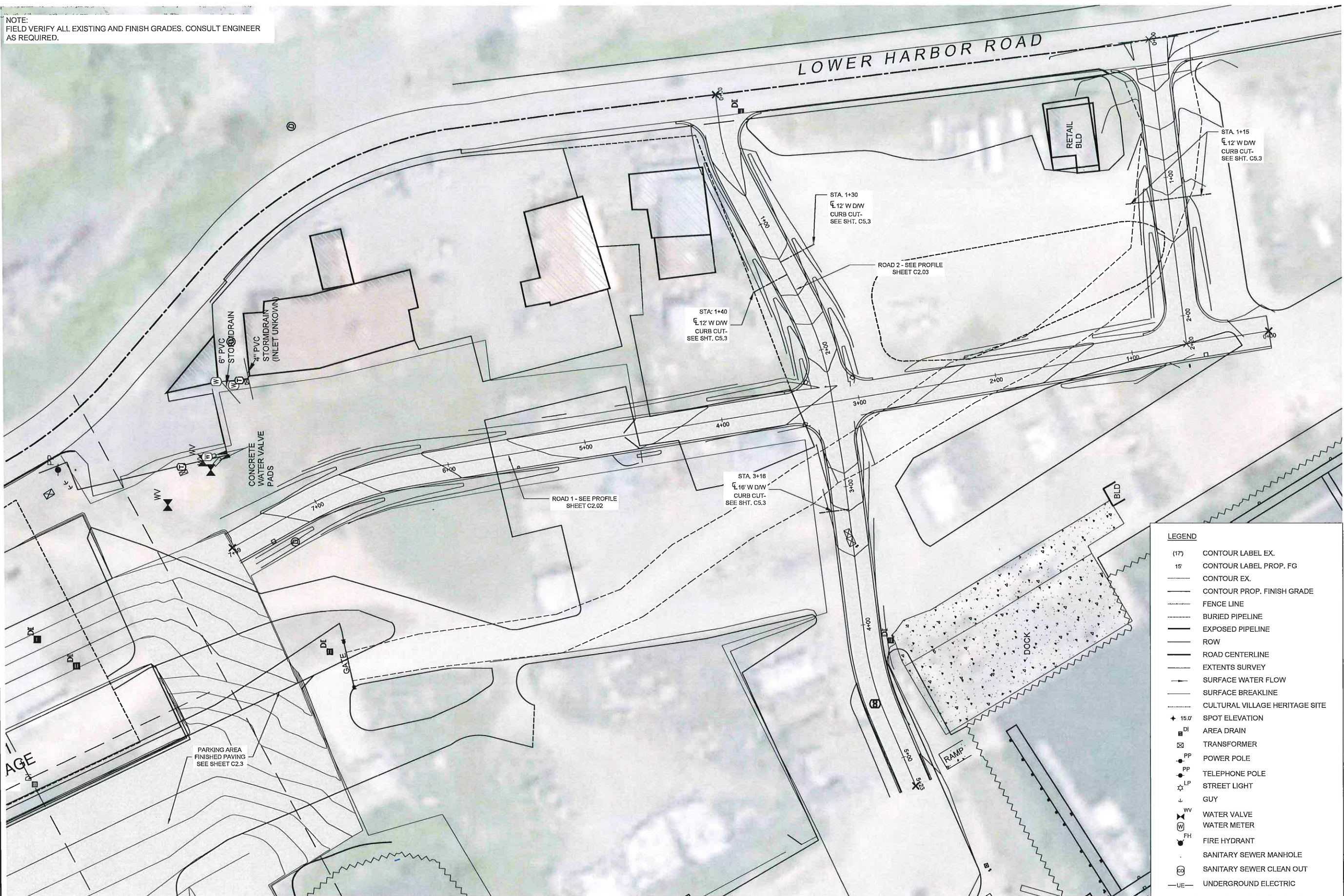
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**PORT OF BROOKINGS HARBOR**  
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 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C2.1**  
 PARKING  
 SUBGRADE PLAN

NOTE:  
FIELD VERIFY ALL EXISTING AND FINISH GRADES. CONSULT ENGINEER  
AS REQUIRED.



**LEGEND**

(17)	CONTOUR LABEL EX.
15'	CONTOUR LABEL PROP. FG
---	CONTOUR EX.
---	CONTOUR PROP. FINISH GRADE
---	FENCE LINE
---	BURIED PIPELINE
---	EXPOSED PIPELINE
---	ROW
---	ROAD CENTERLINE
---	EXTENTS SURVEY
---	SURFACE WATER FLOW
---	SURFACE BREAKLINE
---	CULTURAL VILLAGE HERITAGE SITE
+ 15.0'	SPOT ELEVATION
DI	AREA DRAIN
⊠	TRANSFORMER
PP	POWER POLE
PP	TELEPHONE POLE
⊙	STREET LIGHT
GUY	GUY
WV	WATER VALVE
WM	WATER METER
FH	FIRE HYDRANT
SM	SANITARY SEWER MANHOLE
SC	SANITARY SEWER CLEAN OUT
UE	UNDERGROUND ELECTRIC

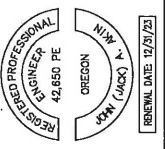
1 ROADS FINISHED PAVING PLAN  
SCALE 1" = 30'



REVISIONS	BY:
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 CP Office: 1807 Williams Hwy, Suite 216, Grants Pass, OR 97527  
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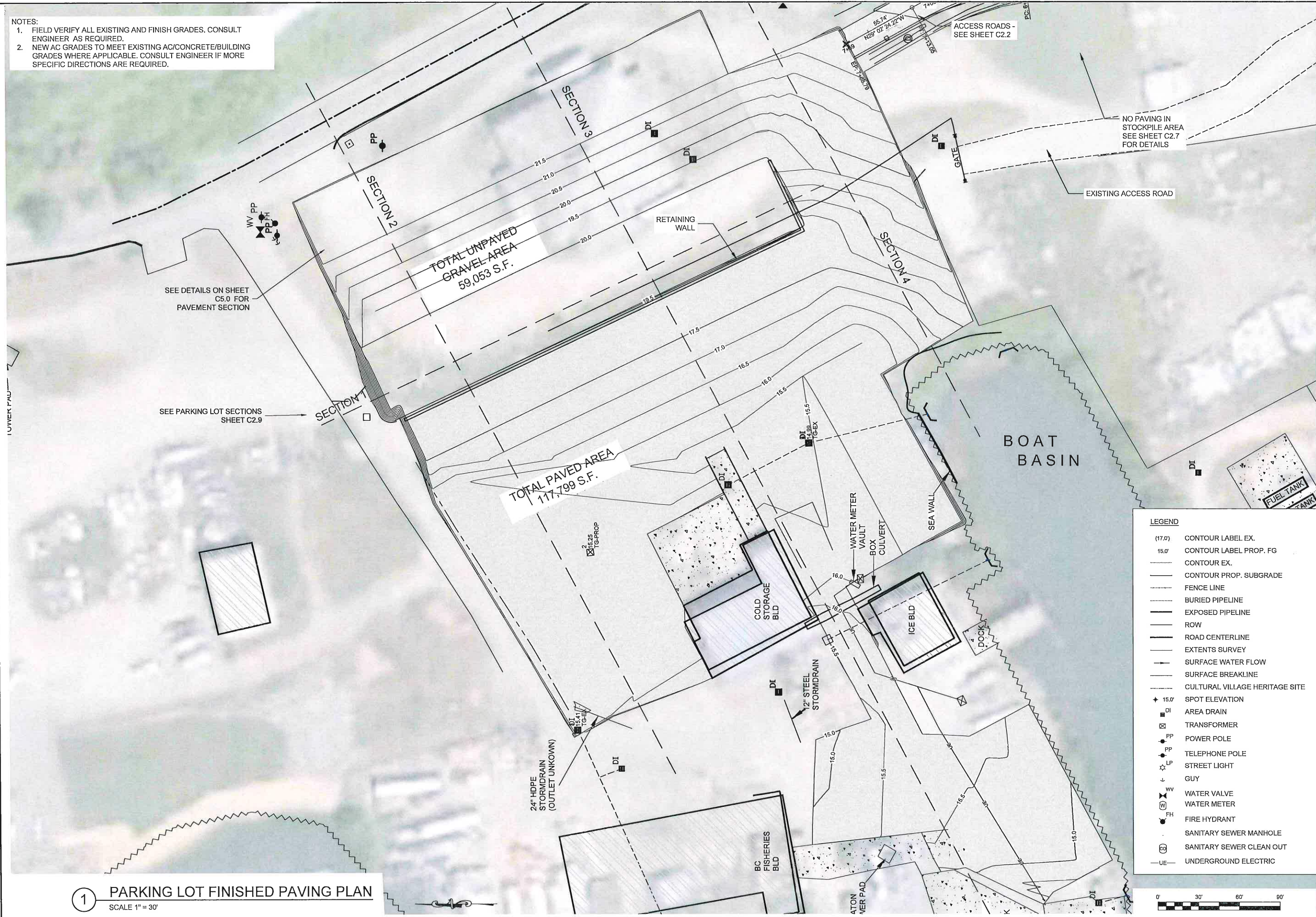
**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:

**C2.2**  
 ROADS  
 PAVING PLAN



NOTES:  
 1. FIELD VERIFY ALL EXISTING AND FINISH GRADES. CONSULT ENGINEER AS REQUIRED.  
 2. NEW AC GRADES TO MEET EXISTING AC/CONCRETE/BUILDING GRADES WHERE APPLICABLE. CONSULT ENGINEER IF MORE SPECIFIC DIRECTIONS ARE REQUIRED.



SEE DETAILS ON SHEET C5.0 FOR PAVEMENT SECTION

SEE PARKING LOT SECTIONS SHEET C2.9

TOTAL UNPAVED GRAVEL AREA  
59,053 S.F.

TOTAL PAVED AREA  
117,799 S.F.

NO PAVING IN STOCKPILE AREA  
SEE SHEET C2.7 FOR DETAILS

EXISTING ACCESS ROAD

ACCESS ROADS -  
SEE SHEET C2.2

**LEGEND**

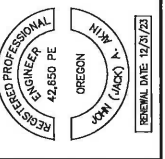
(17.0)	CONTOUR LABEL EX.
15.0'	CONTOUR LABEL PROP. FG
---	CONTOUR EX.
---	CONTOUR PROP. SUBGRADE
---	FENCE LINE
---	BURIED PIPELINE
---	EXPOSED PIPELINE
---	ROW
---	ROAD CENTERLINE
---	EXTENTS SURVEY
---	SURFACE WATER FLOW
---	SURFACE BREAKLINE
---	CULTURAL VILLAGE HERITAGE SITE
+ 15.0'	SPOT ELEVATION
DI	AREA DRAIN
⊠	TRANSFORMER
PP	POWER POLE
PP	TELEPHONE POLE
LP	STREET LIGHT
+	GUY
WV	WATER VALVE
⊕	WATER METER
FH	FIRE HYDRANT
⊕	SANITARY SEWER MANHOLE
⊕	SANITARY SEWER CLEAN OUT
UE	UNDERGROUND ELECTRIC

1 **PARKING LOT FINISHED PAVING PLAN**  
 SCALE 1" = 30'



BY:	JA
REVISIONS	

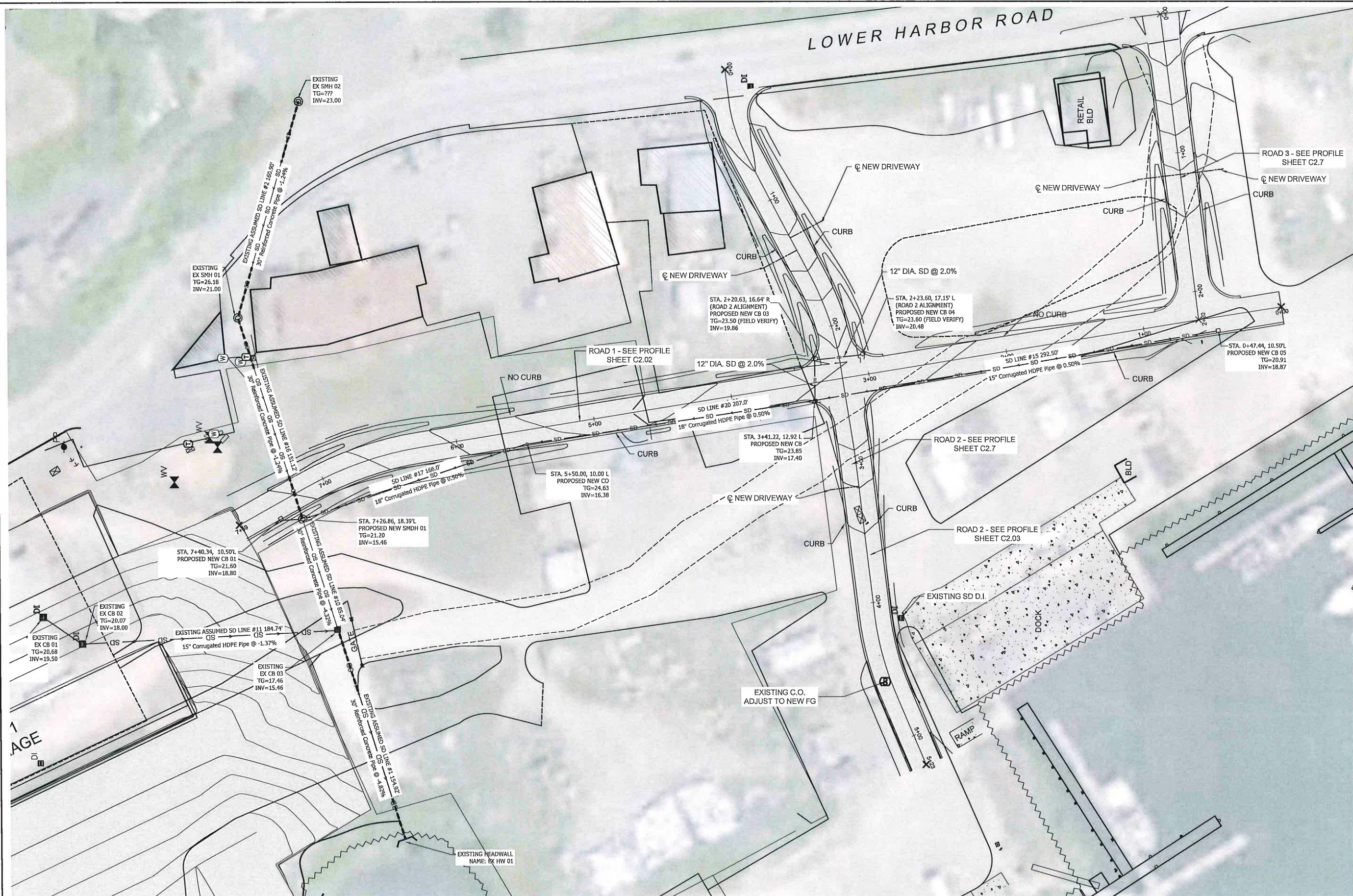
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**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

DRAWN BY: CD/JW  
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 SHEET No:

**C2.3**  
 PARKING LOT PAVING PLAN



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 Ph: 541-474-9424 • Cell: 541-310-9939 • Fax: 541-775-5488  
 email: grantspass@emcinc.com • info@jacksonvilleengineers.com • info@medfordengineers.com



REGISTERED PROFESSIONAL ENGINEER  
 42480 PE  
 OREGON  
 0344 (JACK)  
 EXPIRES: 12/31/23

**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

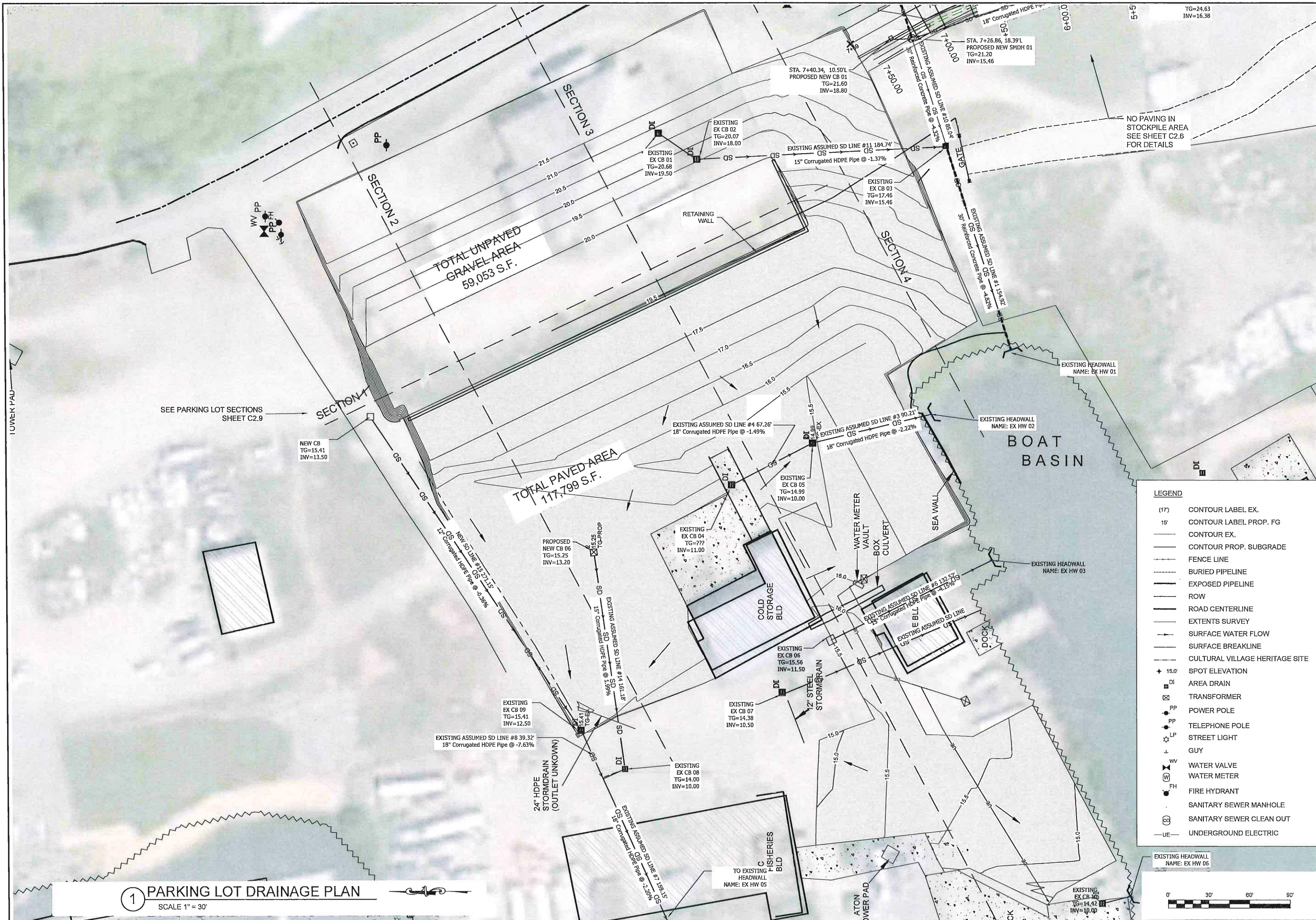
DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:

**C2.4**  
 ROAD  
 STORM DRAIN

**1 ROAD STORM DRAIN PLAN**  
 SCALE 1" = 30'

NOTE:  
 SEE SHEET C2.5 FOR STORM DRAIN NOTES





**1** PARKING LOT DRAINAGE PLAN  
SCALE 1" = 30'

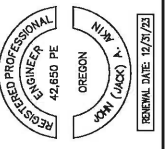
**LEGEND**

(17)	CONTOUR LABEL EX.
15'	CONTOUR LABEL PROP. FG
---	CONTOUR EX.
---	CONTOUR PROP. SUBGRADE
---	FENCE LINE
---	BURIED PIPELINE
---	EXPOSED PIPELINE
---	ROW
---	ROAD CENTERLINE
---	EXTENTS SURVEY
---	SURFACE WATER FLOW
---	SURFACE BREAKLINE
---	CULTURAL VILLAGE HERITAGE SITE
+ 15.0'	SPOT ELEVATION
DI	AREA DRAIN
⊠	TRANSFORMER
PP	POWER POLE
PPH	TELEPHONE POLE
☆	STREET LIGHT
---	GUY
WV	WATER VALVE
WM	WATER METER
FH	FIRE HYDRANT
⊙	SANITARY SEWER MANHOLE
⊙	SANITARY SEWER CLEAN OUT
UE	UNDERGROUND ELECTRIC



BY:	JA
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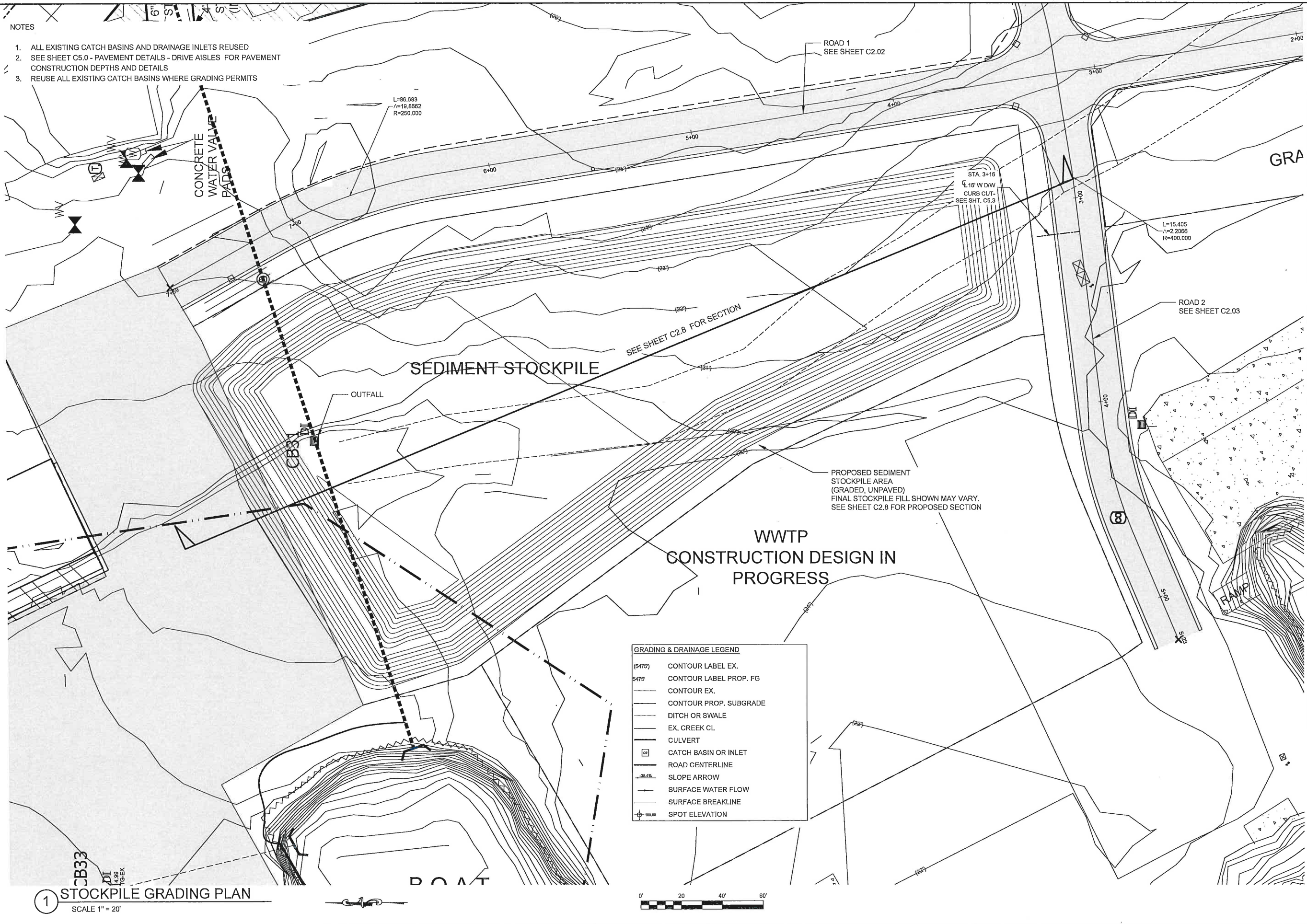
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 DATE: 15-JULY-2022  
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 SHEET No:

**C2.5**  
 PARKING  
 LOT DRAINAGE

NOTES

1. ALL EXISTING CATCH BASINS AND DRAINAGE INLETS REUSED
2. SEE SHEET C5.0 - PAVEMENT DETAILS - DRIVE AISLES FOR PAVEMENT CONSTRUCTION DEPTHS AND DETAILS
3. REUSE ALL EXISTING CATCH BASINS WHERE GRADING PERMITS



**GRADING & DRAINAGE LEGEND**

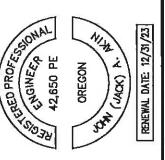
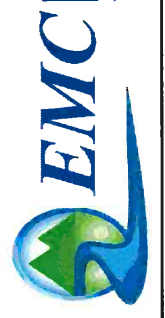
(5475)	CONTOUR LABEL EX.
5475	CONTOUR LABEL PROP. FG
---	CONTOUR EX.
---	CONTOUR PROP. SUBGRADE
---	DITCH OR SWALE
---	EX. CREEK CL
---	CULVERT
□	CATCH BASIN OR INLET
---	ROAD CENTERLINE
→	SLOPE ARROW
→	SURFACE WATER FLOW
---	SURFACE BREAKLINE
⊕	SPOT ELEVATION



**1 STOCKPILE GRADING PLAN**  
SCALE 1" = 20'

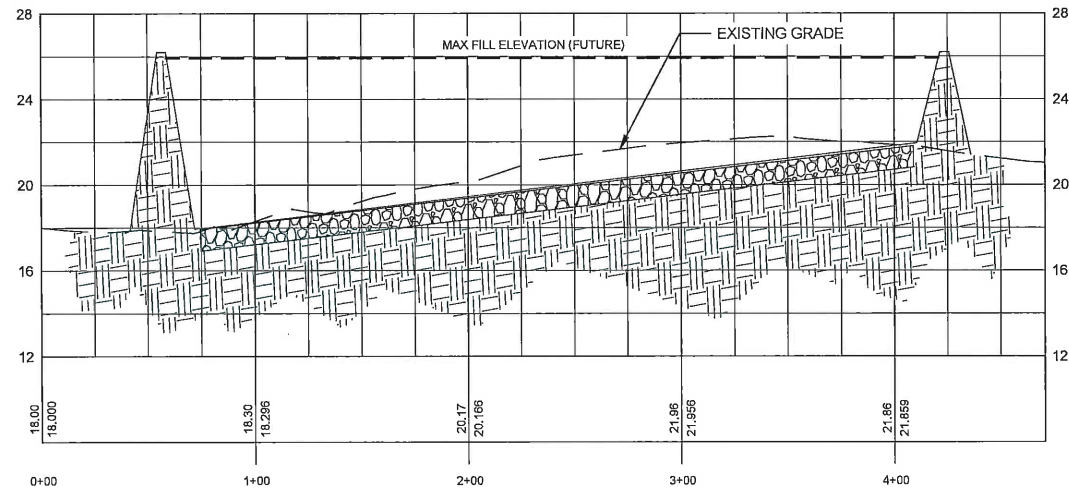
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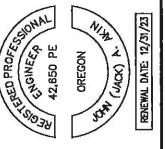


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 JOB No: 022-2202  
 SHEET No:  
**C2.6**  
 STOCKPILE  
 GRADING PLAN



1 STOCKPILE SECTION  
SCALE - 1" = 40' H 1" = 4' V



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 emc@emcengineers.com  
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REVISIONS	BY:
	JA

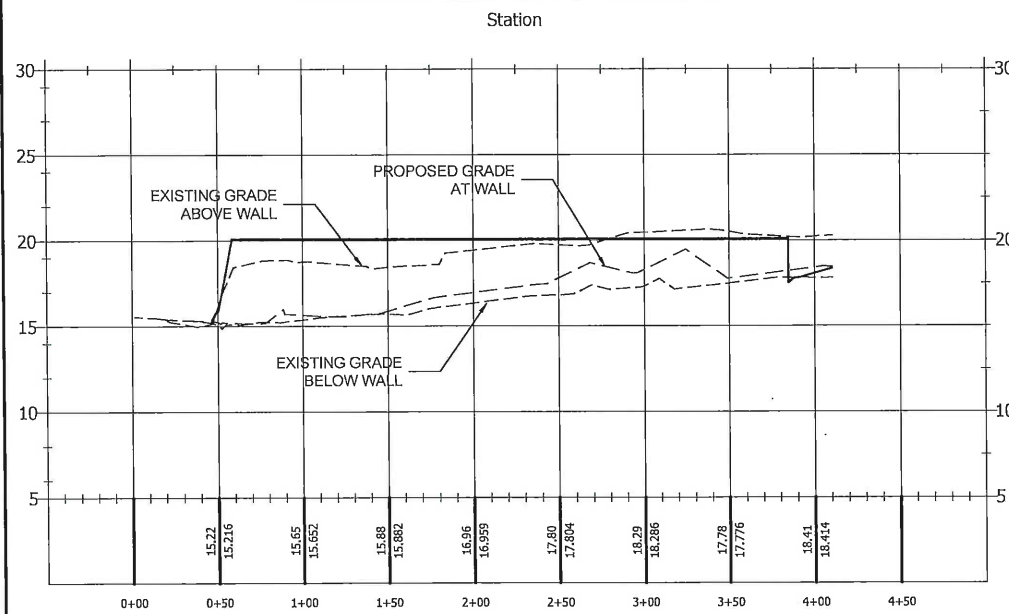
PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:

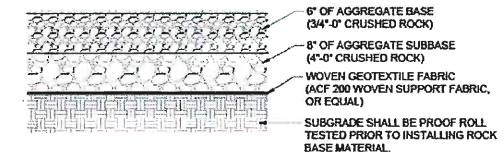
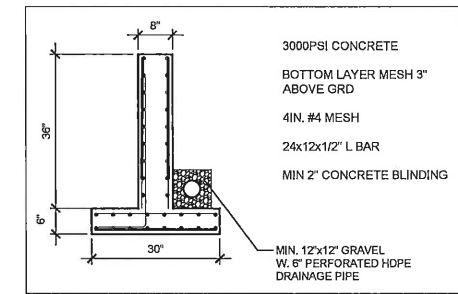
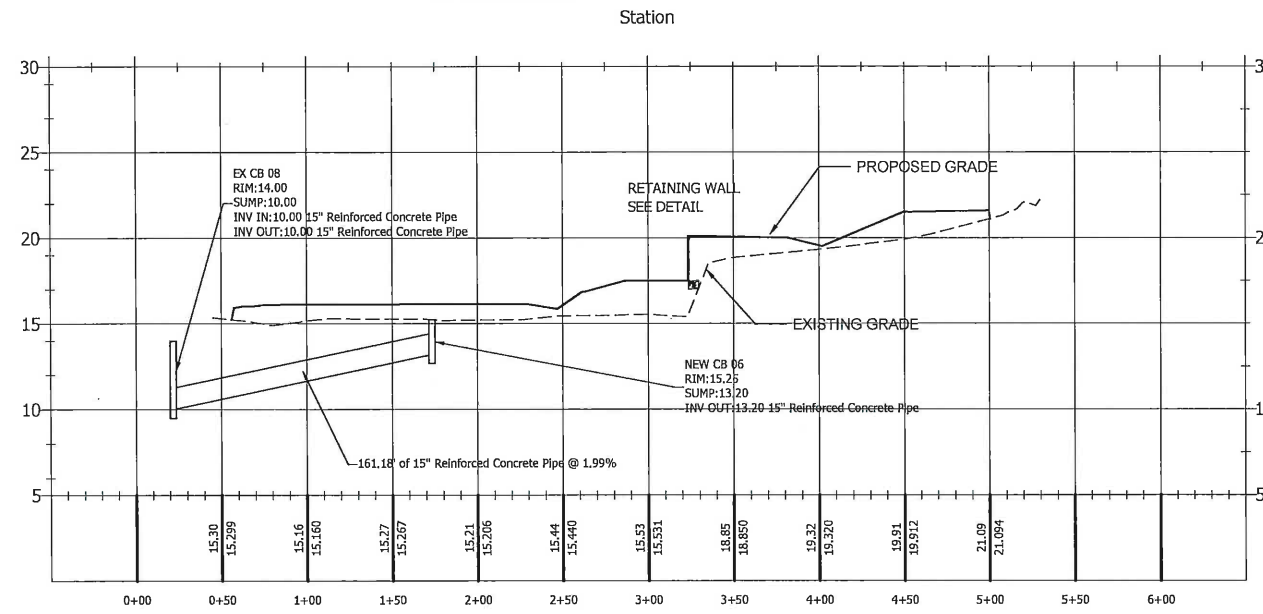
**C2.8**  
 STOCKPILE SECTION

85

Parking Lot Section 1 PROFILE

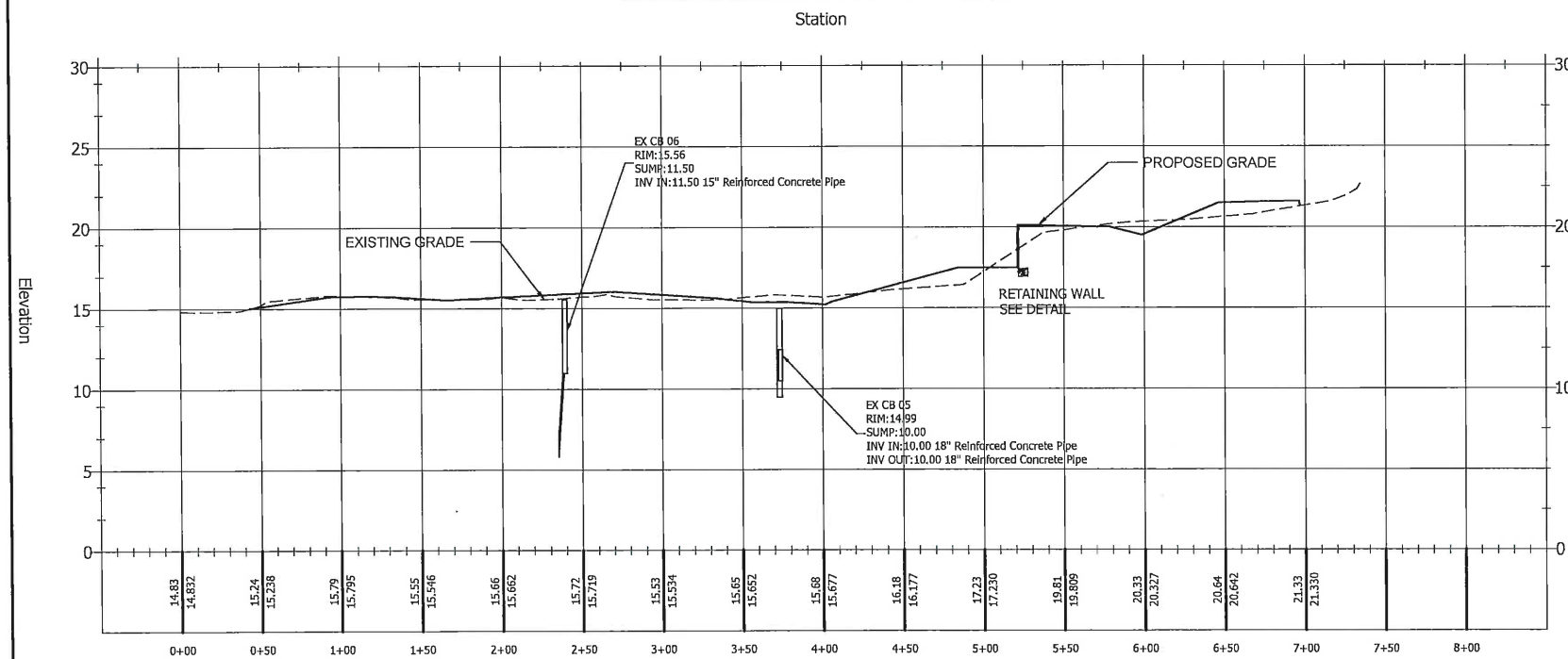


Parking Lot Section 2 PROFILE

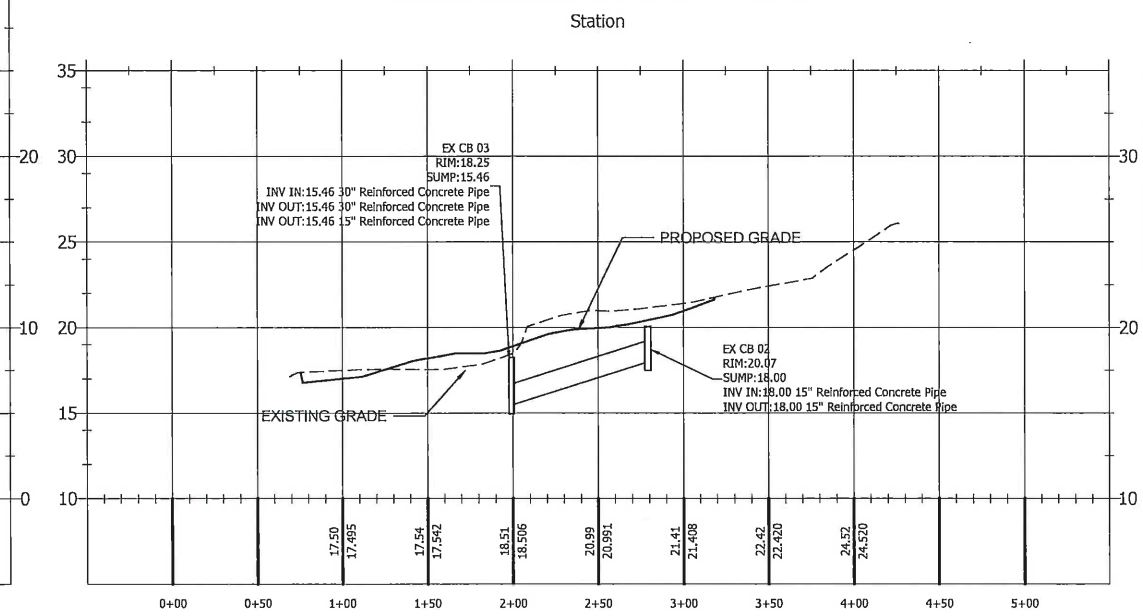


SUB-GRADE PREPARATION DETAIL

Parking Lot Section 3 PROFILE



Parking Lot Section 4 PROFILE



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 Email: info@emci-engineers.com • www.emci-engineers.com

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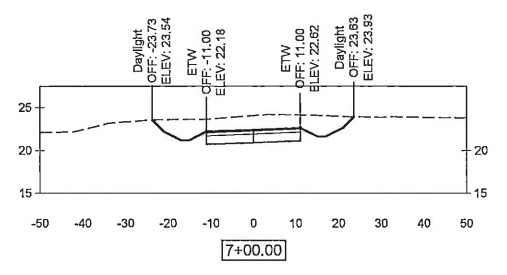
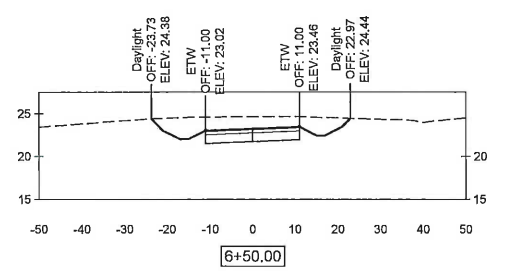
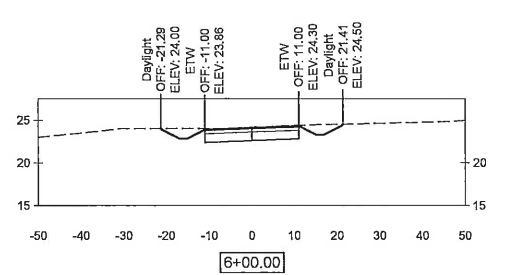
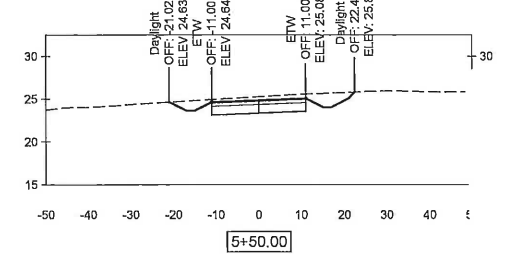
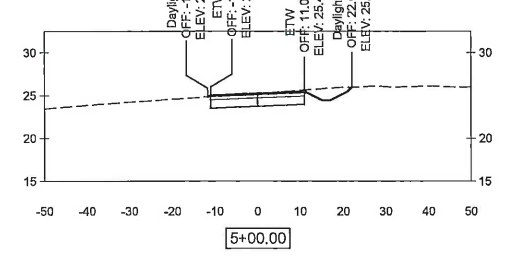
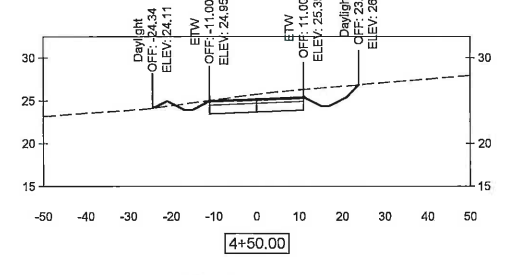
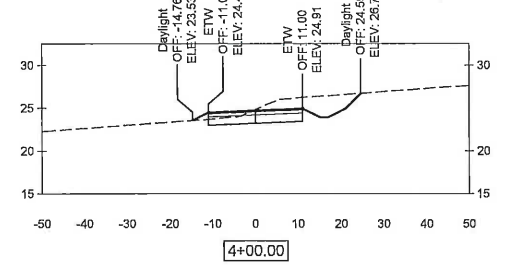
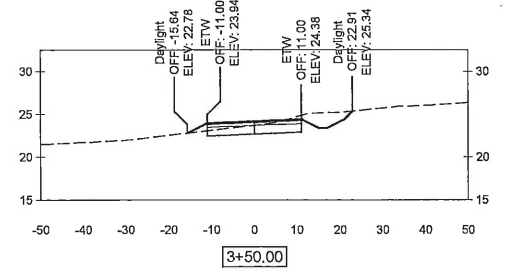
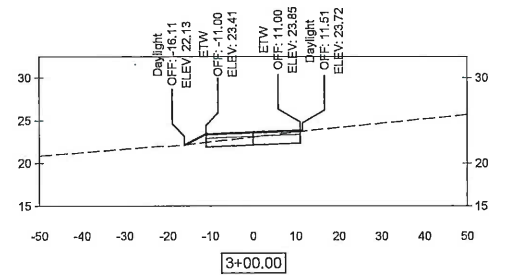
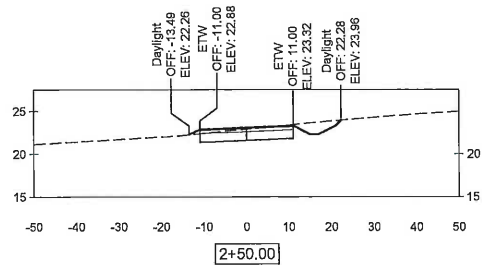
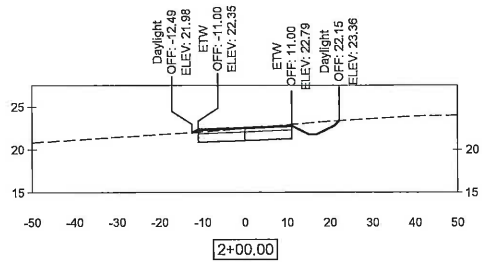
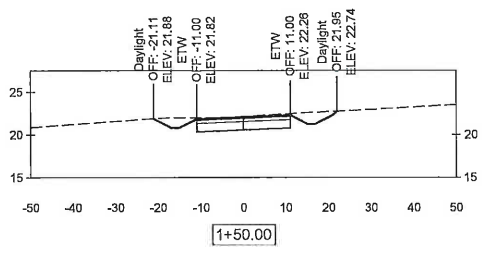
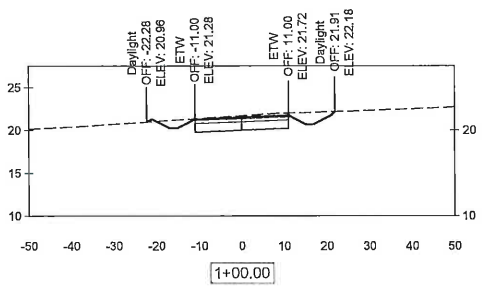
REGISTERED PROFESSIONAL ENGINEER  
 42,650 FE  
 OREGON  
 100M (JACK) X  
 N.W. X  
 EXPIRES DATE: 12/31/25

**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C2.9**  
 PARKING LOT SECTIONS



- NOTES:
1. FIELD VERIFY ALL EXISTING AND FINISH GRADES. CONSULT ENGINEER AS REQUIRED.
  2. SEE PLAN & PROFILE SHEETS C2.02 & C2.03 FOR LOCATION OF PROPOSED CURB & GUTTER INSTALLATION.

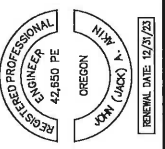


LEGEND  
 - - - - - EXISTING GRADE  
 \_\_\_\_\_ FINISH GRADE

1 ROAD 1 SECTIONS  
 SCALE - 1" = 20' H 1" = 10' V

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	JJA

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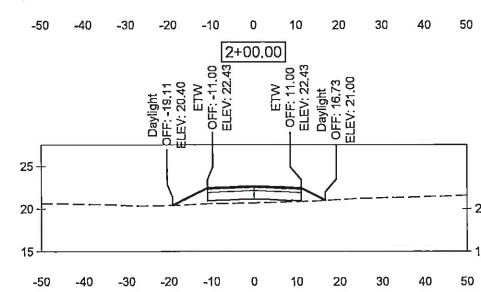
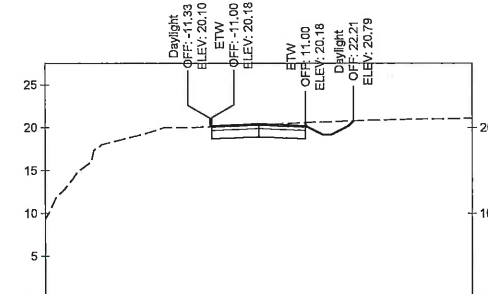
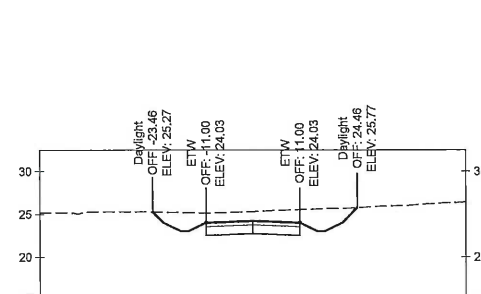
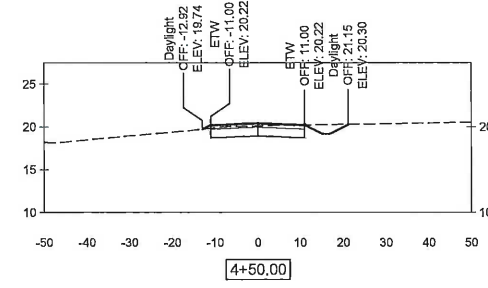
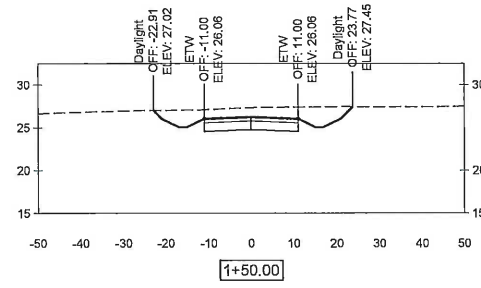
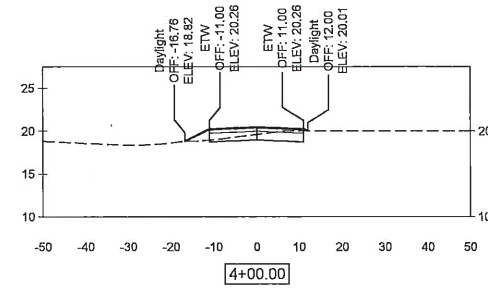
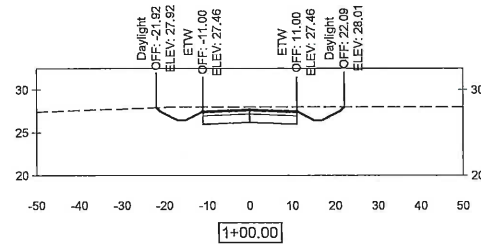
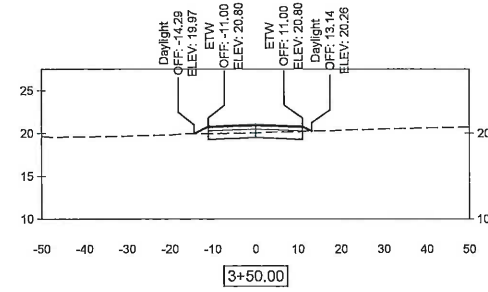
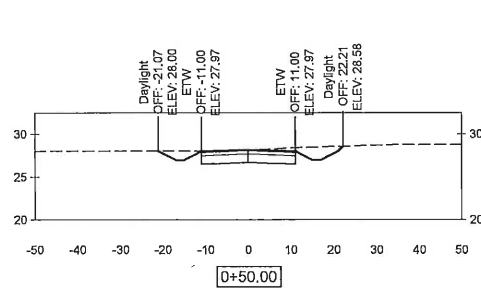


PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C3.0**  
 ROAD 1  
 SECTIONS

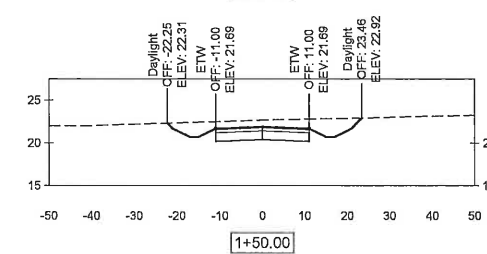
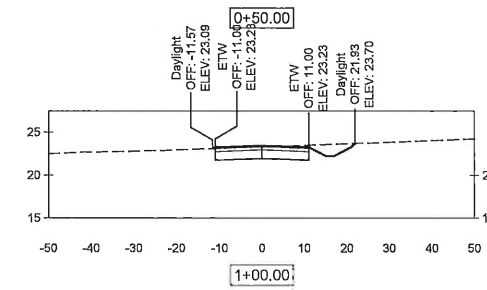
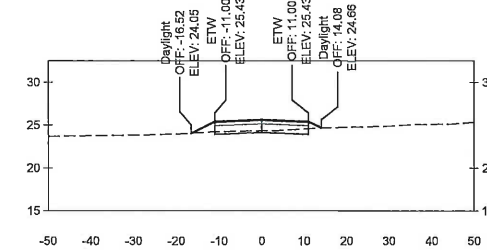
NOTES:

1. FIELD VERIFY ALL EXISTING AND FINISH GRADES. CONSULT ENGINEER AS REQUIRED.
2. SEE PLAN & PROFILE SHEETS C2.02 & C2.03 FOR LOCATION OF PROPOSED CURB & GUTTER INSTALLATION.



1 ROAD 2 SECTIONS  
SCALE - 1"=20' H 1"= 10' V

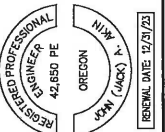
LEGEND  
 - - - - - EXISTING GRADE  
 ————— FINISH GRADE



2 ROAD 3 SECTIONS  
SCALE - 1"=20' H 1"= 10' V

REVISIONS	BY:
	JA

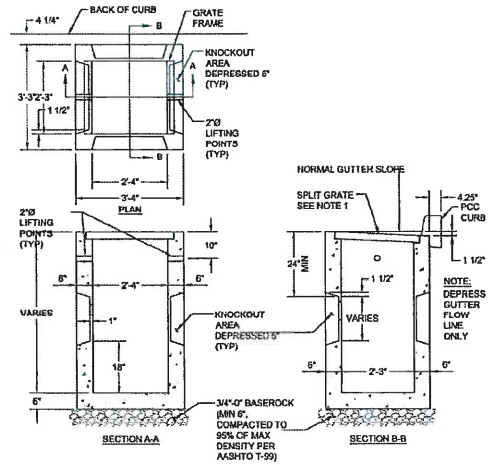
Grants Pass • Jacksonville • Medford, OR  
 425 W. Main St., Suite 116, Grants Pass, OR 97527  
 400 S. Commercial St., Medford, OR 97504  
 100 S. Commercial St., Jacksonville, OR 97531  
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**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**COMMERCIAL ROADS AND STOCKPILE**

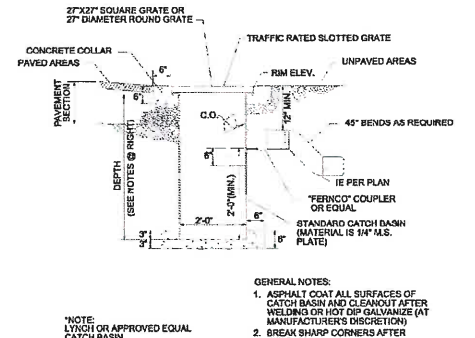
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 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C3.1**  
 ROAD 2 SECTIONS





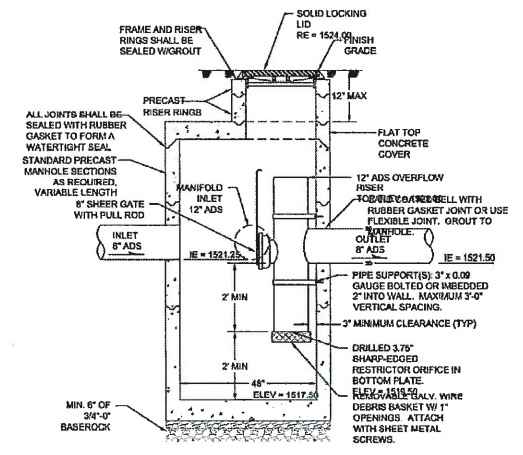
- NOTES**
1. GRATE AND FRAME MAY EACH BE OF CAST IRON OR WELDED STEEL CONSTRUCTION. GRATE AND FRAME TO BE COOT (3) TYPE 3 (BICYCLE SAFETY).
  2. FOR PRECAST BOX, CURB MUST BE HAND FORMED 1/2" EACH SIDE OF CATCH BASIN.
  3. CONCRETE STRENGTH SHALL BE 3000 PSI WITH FIBER MESH.
  4. CATCH BASIN AND GRATE SHALL MEET HS20 LOADING.
  5. A MINIMUM SLUMP DEPTH OF 18" IS REQUIRED.
  6. OUTLET PIPE SHALL HAVE A MINIMUM OF 24" OF COVER.

**2 CATCH BASIN**  
SCALE: NTS



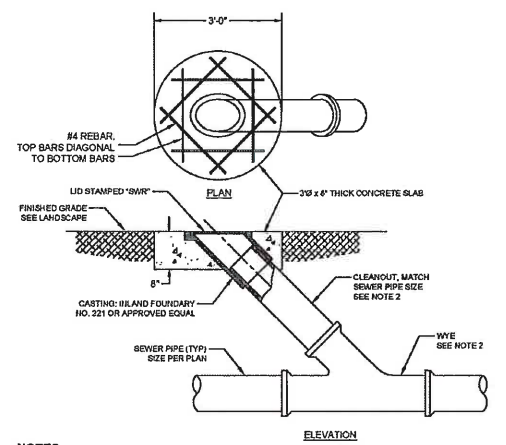
- GENERAL NOTES:**
1. ASPHALT COAT ALL SURFACES OF CATCH BASIN AND CLEANOUT AFTER WELDING OR HOT DIP GALVANIZE (AT MANUFACTURER'S DISCRETION).
  2. BREAK SHARP CORNERS AFTER WELDING.
  3. ALL WELDED STEEL CONSTRUCTION.
  4. 10 GAUGE STEEL MINIMUM THICKNESS.
  5. OPENING ON BOTTOM OF WATERSEAL TO BE GREATER OR EQUAL TO AREA IN OUTLET PIPE.
  6. STANDARD DEPTH: 48"

**3 LYNCH STYLE CATCH BASIN**  
SCALE: NTS



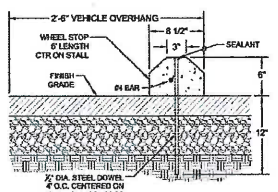
- NOTE**
- ALL PRECAST SECTIONS SHALL CONFORM TO REQUIREMENTS OF ASTM C-478.

**4 CONTROL STRUCTURE MANHOLE**  
SCALE: NTS (PONDS 2 & 3)

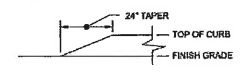


- NOTES**
1. CONCRETE TO BE COMMERCIAL GRADE CONCRETE (CGC) PER 2015 CDOT STANDARD SPECIFICATIONS SECTION 0040. MINIMUM 3000 PSI COMPRESSIVE STRENGTH AT 28 DAYS.
  2. FITTINGS AND PIPE TO BE GASKETED PVC SDR 28.

**5 TYPICAL CLEANOUT**  
SCALE: NTS

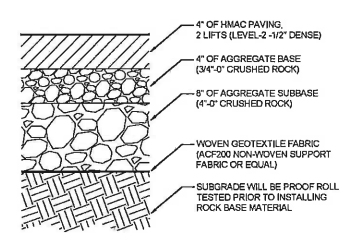


**WHEEL STOP**  
SCALE: NTS

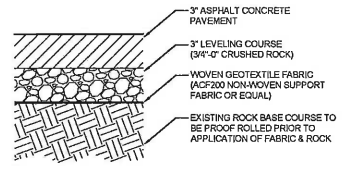


**CURB TAPER**  
SCALE: NTS

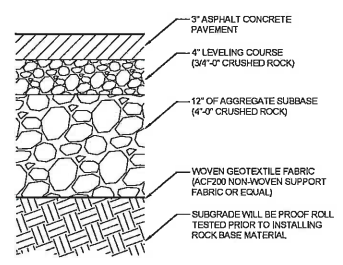
**PARKING LOT**



**PAVING OVER EXISTING ROAD OR PARKING LOT**

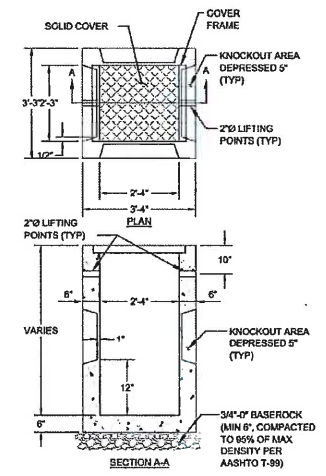


**TYPICAL ROAD PAVING**



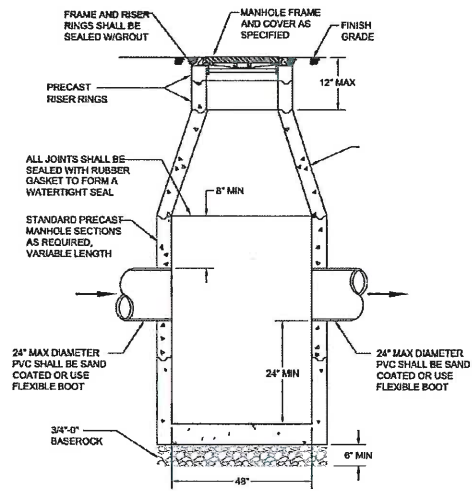
- NOTES**
1. AGGREGATE BASE AND SUBBASE SHALL BE INSTALLED IN MAX 6" LIFTS AND MECHANICALLY COMPACTED TO MAX 90% OF THE MAX DRY DENSITY IN ACCORDANCE WITH THE AASHTO T-99 METHOD.
  2. JUST PRIOR TO PAVING THE AGGREGATE BASE SHALL BE PROOF ROLLED. AGGREGATES THAT DO NOT PASS PROOF ROLL TESTING WILL BE REMOVED, RE-COMPACTED AND TESTED AGAIN.
  3. JUST PRIOR TO INSTALLING AGGREGATE BASE THE SUBGRADE SHALL BE PROOF ROLLED. SUBGRADE MATERIAL THAT DOES NOT PASS PROOF ROLL TESTING WILL BE REMOVED AND ADDITIONAL CRUSHED ROCK INSTALLED.
  4. PAVEMENT SECTION IS BASED ON THE ASSUMPTION THAT PAVEMENT CONSTRUCTION WILL BE ACCOMPLISHED DURING DRY SEASON.
  5. PAVEMENTS SUBJECT TO CONSTRUCTION TRAFFIC MAY REQUIRE REPAIR.

**1 PAVING SECTIONS**  
NO SCALE



- CONSTRUCTION NOTES:**
1. COVER AND FRAME MAY EACH BE OF CAST IRON OR WELDED STEEL CONSTRUCTION.
  2. CONCRETE STRENGTH SHALL BE 3000 PSI WITH FIBER MESH.
  3. JUNCTION BASIN AND GRATE SHALL MEET HS20 LOADING.
  4. A MINIMUM SLUMP DEPTH OF 18" IS REQUIRED.
  5. OUTLET PIPE SHALL HAVE A MINIMUM OF 24" OF COVER.

**6 JUNCTION BASIN**  
SCALE: NTS

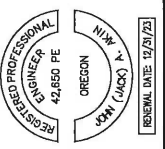


- NOTE**
- ALL PRECAST SECTIONS SHALL CONFORM TO REQUIREMENTS OF ASTM C-478.

**7 48" STORM MANHOLE**  
SCALE: NTS

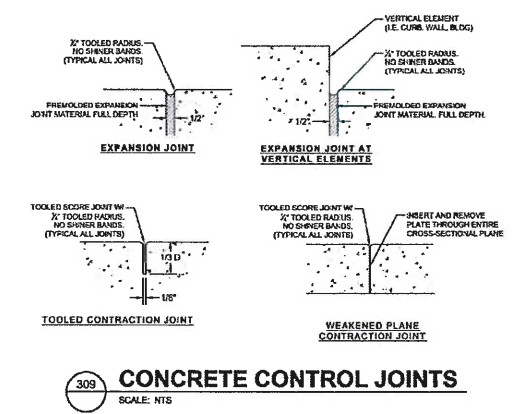
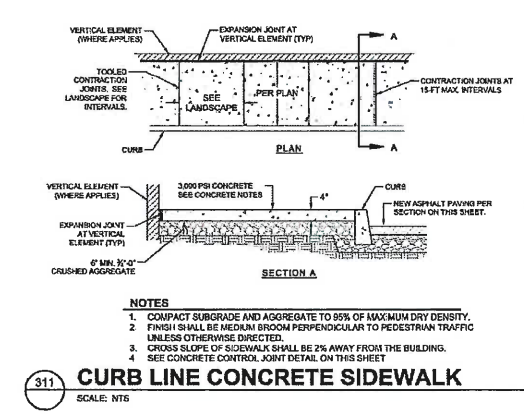
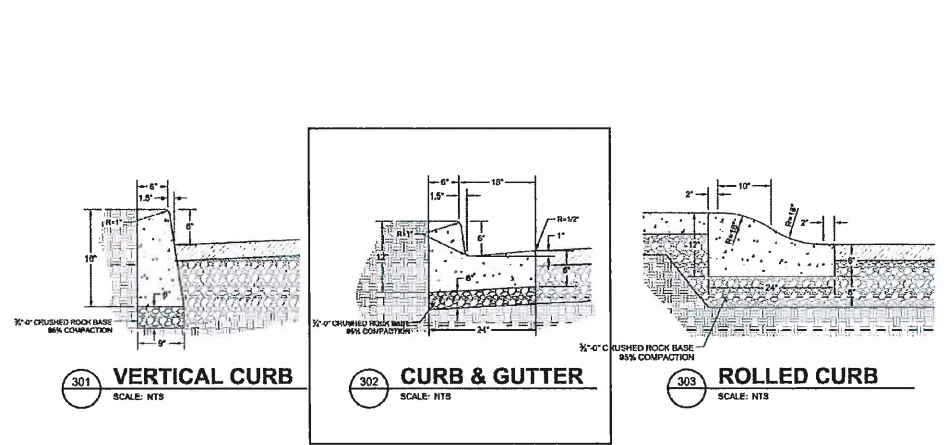
REVISIONS	BY:	DATE:
	JA	

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 Jville Office: 400 Coalinga Dr., Jacksonville, OR 97530  
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**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

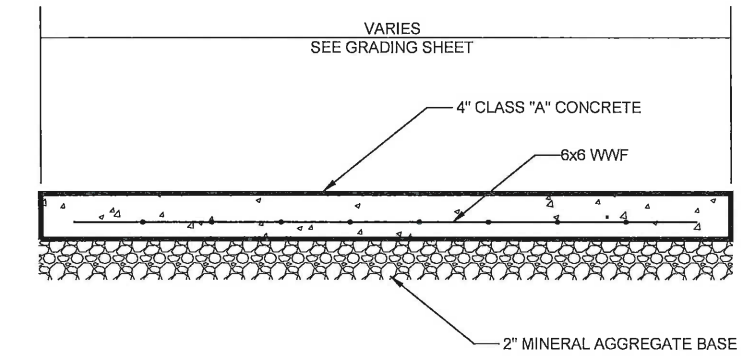
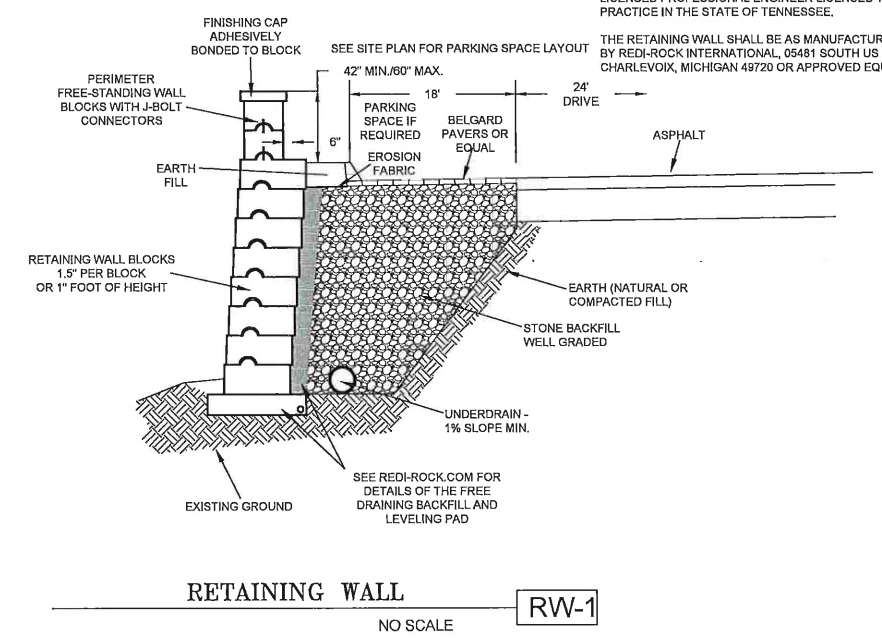
DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C5.0**  
 DETAILS



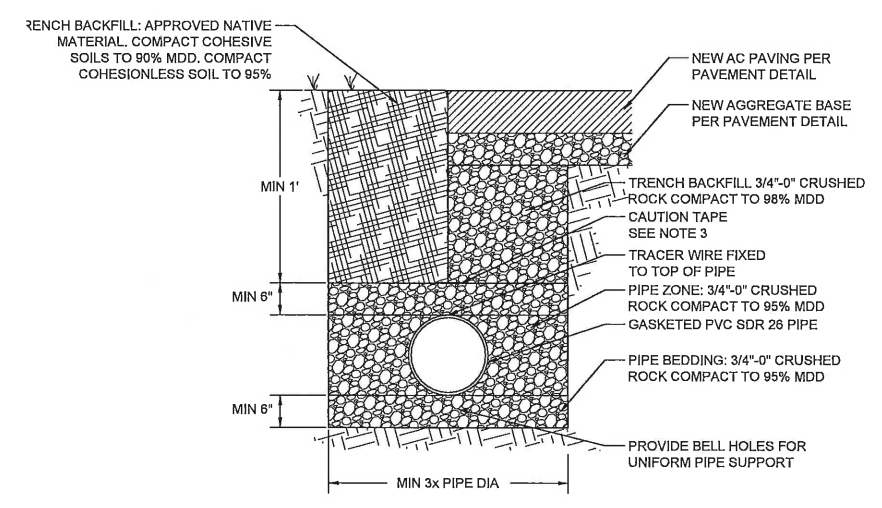
**NOTES:**  
TYPICAL DETAIL FOR RETAINING WALL. (IF WATERS OF THE STATE OR WETLANDS ARE NOT PRESENT SEE EXISTING GROUND/PORTION OF RETAINING WALL DETAIL, RW-1.)

THE CONTRACTOR SHALL FURNISH TO THE ENGINEER FOR APPROVAL A DETAILED STRUCTURAL DESIGN PLAN FOR THE WALL SHOWN STAMPED BY A LICENSED PROFESSIONAL ENGINEER LICENSED TO PRACTICE IN THE STATE OF TENNESSEE.

THE RETAINING WALL SHALL BE AS MANUFACTURED BY REDI-ROCK INTERNATIONAL, 05481 SOUTH US 31, CHARLEVOIX, MICHIGAN 49720 OR APPROVED EQUAL.



**NOTE:**  
1/2 INCH BITUMINOUS EXPANSION JOINT REQUIRED 25' ON CENTER  
1/2 INCH CONTRACTION JOINT REQUIRED 5' ON CENTER

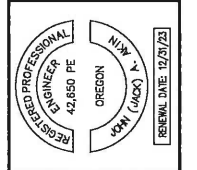


**NOTES:**  
COVER OVER PIPE MAY VARY FROM 36" TO 42" MAX UPON APPROVAL FROM ENGINEER  
REQUIRED COMPACTION ACCORDING TO ASTM D 1557 MAX DRY DENSITY  
SEWER: 3" POLYETHYLENE TAPE IMPRINTED "CAUTION: BURIED SEWER BELOW"

REVISIONS	BY:	JA

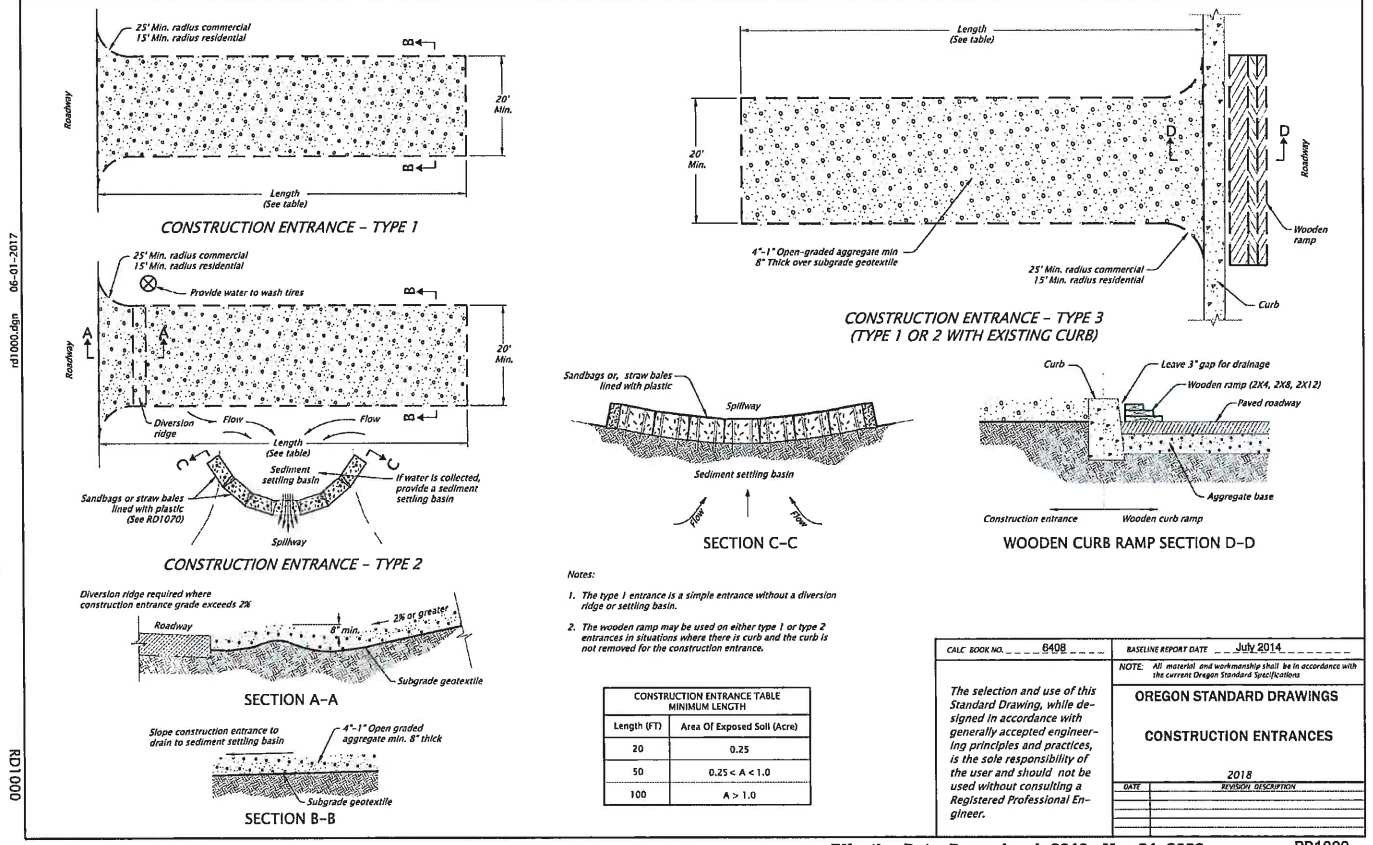
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**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
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**C5.1**  
 DETAILS



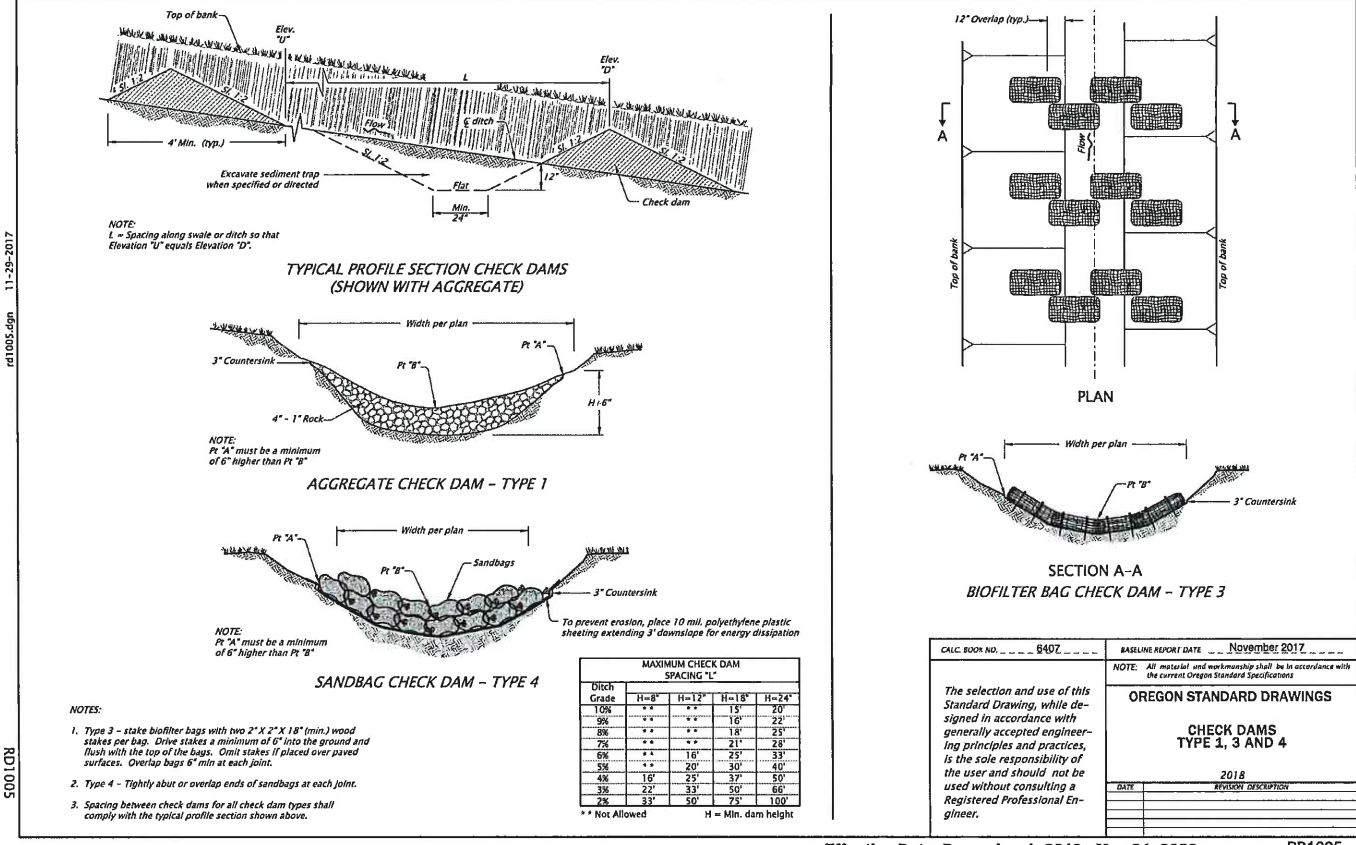
Effective Date: December 1, 2019 - May 31, 2020 RD1000

Calc. Book No. 8408 Baseline Report Date July 2014

OREGON STANDARD DRAWINGS

CONSTRUCTION ENTRANCES

2018



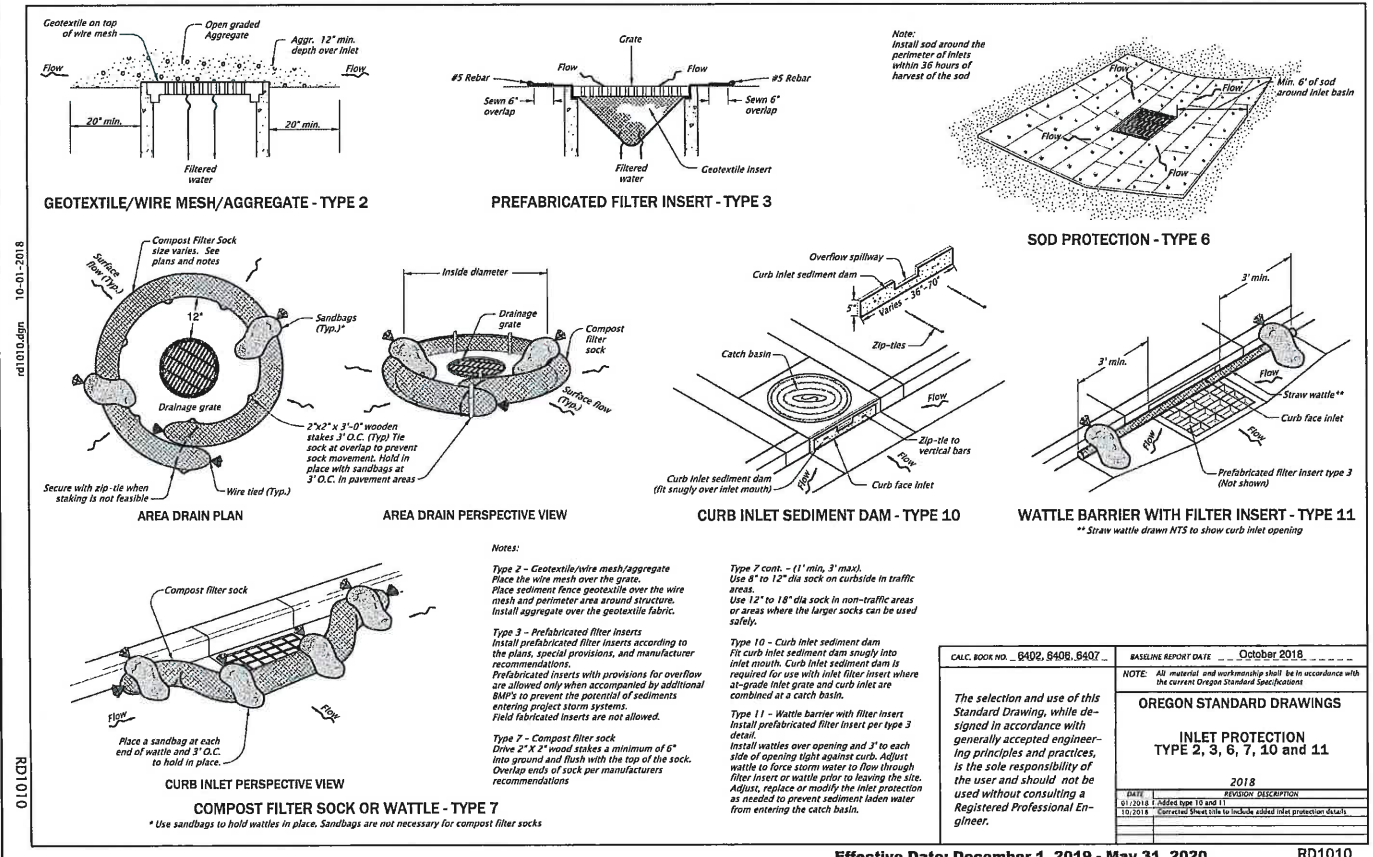
Effective Date: December 1, 2019 - May 31, 2020 RD1005

Calc. Book No. 8407 Baseline Report Date November 2017

OREGON STANDARD DRAWINGS

CHECK DAMS TYPE 1, 3 AND 4

2018



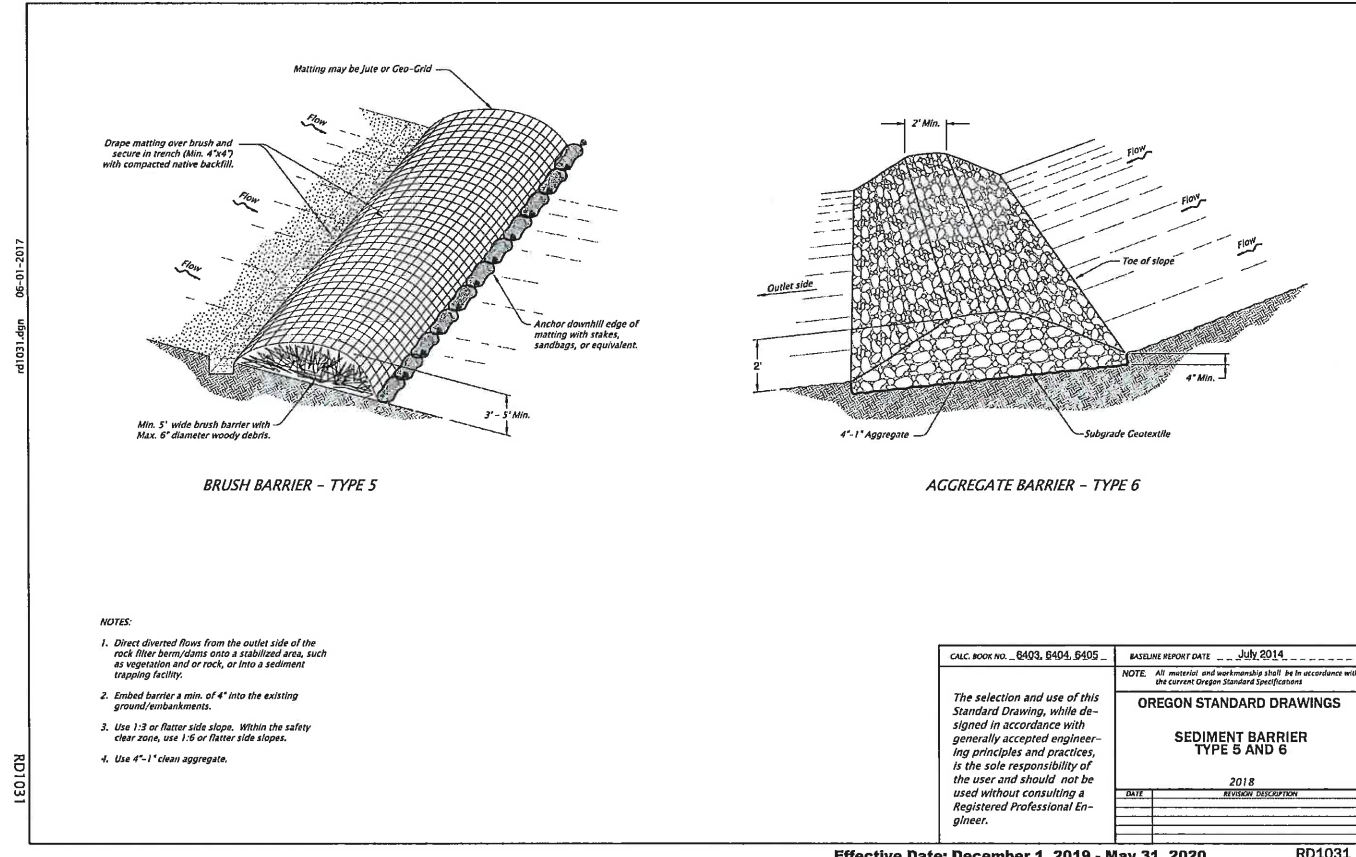
Effective Date: December 1, 2019 - May 31, 2020 RD1010

Calc. Book No. 8402, 8404, 8405 Baseline Report Date October 2018

OREGON STANDARD DRAWINGS

INLET PROTECTION TYPE 2, 3, 6, 7, 10 AND 11

2018



Effective Date: December 1, 2019 - May 31, 2020 RD1031

Calc. Book No. 8403, 8404, 8405 Baseline Report Date July 2014

OREGON STANDARD DRAWINGS

SEDIMENT BARRIER TYPE 5 AND 6

2018

REVISIONS

BY:	JA
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Grants Pass • Jacksonville • Medford, OR  
 GP Office: 1477 Williams Hwy, Suite 116, Grants Pass, OR, 97527  
 JV Office: 450 Cottage Dr., Jacksonville, OR, 97530  
 PH: 541-874-9434 • Cell: 541-261-9959 • Fax: 541-272-5488  
 email: grantspass@emcinc.com • medford@emcinc.com • jacksonville@emcinc.com

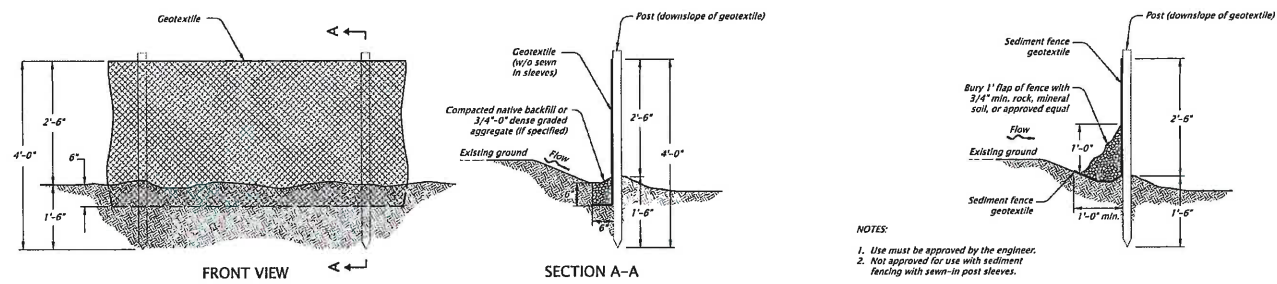
**EMC**  
 Engineers/Scientists, LLC

REGISTERED PROFESSIONAL ENGINEER  
 42,650 PE  
 OREGON  
 JOHN JACKSON  
 EXPIRES: 12/31/23

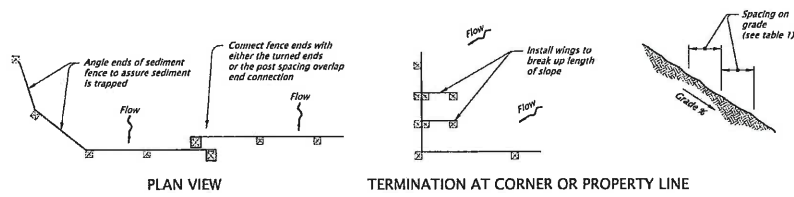
**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C5.2**  
 SWPPP  
 DETAILS

11-08-2017  
rd1060.dgn  
RD1040  
06-01-2017  
rd1055.dgn  
RD1055

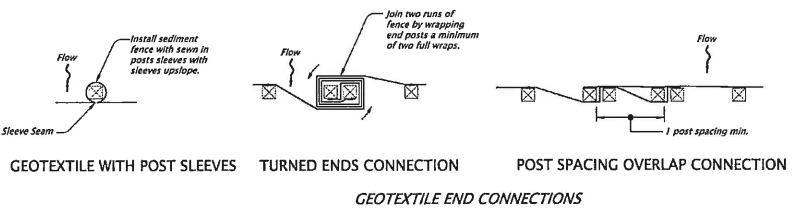


SEDIMENT FENCE AND GEOTEXTILE BURY DETAIL - TYPE 1



PLAN VIEW

TERMINATION AT CORNER OR PROPERTY LINE



GEOTEXTILE WITH POST SLEEVES

TURNED ENDS CONNECTION

POST SPACING OVERLAP CONNECTION

GEOTEXTILE END CONNECTIONS

- NOTES:
1. Use must be approved by the engineer.
  2. Not approved for use with sediment fencing with sewn-in post sleeves.

ALTERNATE SEDIMENT FENCE W/O TRENCHING - TYPE 2

- NOTES:
1. Use 2" X 2" wood fence posts.
  2. Posts to be installed on downhill side of sediment geotextile. Position posts to prevent separation from geotextile.
  3. Compact filter fabric trench backfill and soil on uphill side of fence.
  4. Locate fence no closer than three feet to the toe of a slope.
  5. Wing spacing shall comply with table 1.

TABLE 1  
FENCE SPACING FOR GENERAL APPLICATION  
INSTALL PARALLEL ALONG CONTOURS AS FOLLOWS

GRADE	MAXIMUM SPACING ON GRADE
Grade < 10%	300'
10% < Grade < 15%	150'
15% < Grade < 20%	100'
20% < Grade < 30%	50'
30% < Grade	25'

TABLE 2  
POST SPACING

POST SPACING	SELECTION
6'	Sediment Fence with Geotextile elongation less than 50%
4'	Sediment Fence with Geotextile elongation 50% or more

CALC. BOOK NO. 6403, 6404, 6405

BASELINE REPORT DATE: November 2017

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS

SEDIMENT FENCE

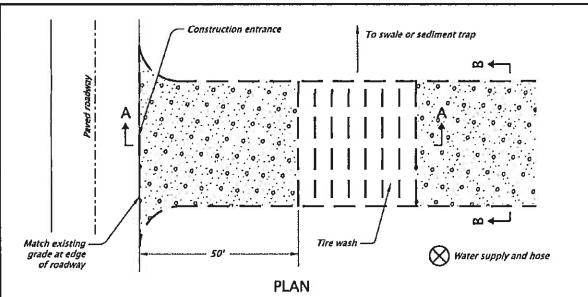
2018

DATE: \_\_\_\_\_

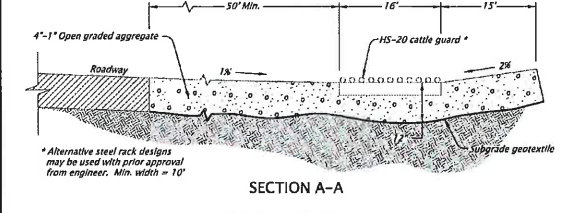
REVISION DESCRIPTION: \_\_\_\_\_

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

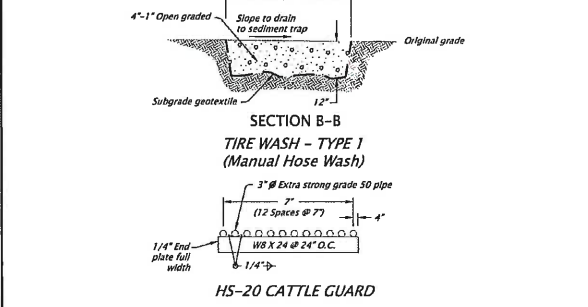
Effective Date: December 1, 2019 - May 31, 2020 RD1040



PLAN

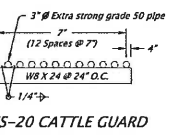


SECTION A-A



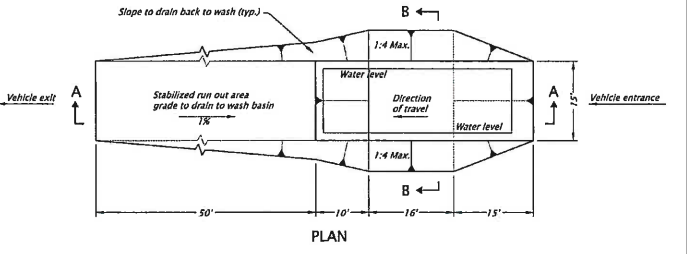
SECTION B-B

TIRE WASH - TYPE 1 (Manual Hose Wash)

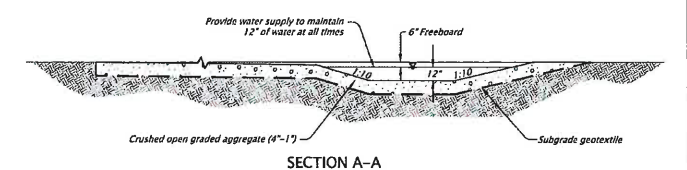


HS-20 CATTLE GUARD

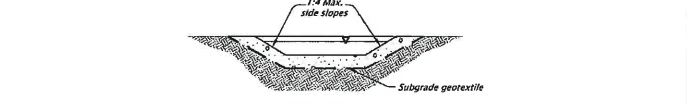
06-01-2017  
rd1060.dgn  
RD1060



PLAN



SECTION A-A



SECTION B-B

TIRE WASH - TYPE 2

CALC. BOOK NO. 6403, 6404, 6405

BASELINE REPORT DATE: July 2014

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS

TIRE WASH FACILITY TYPE 1 AND 2

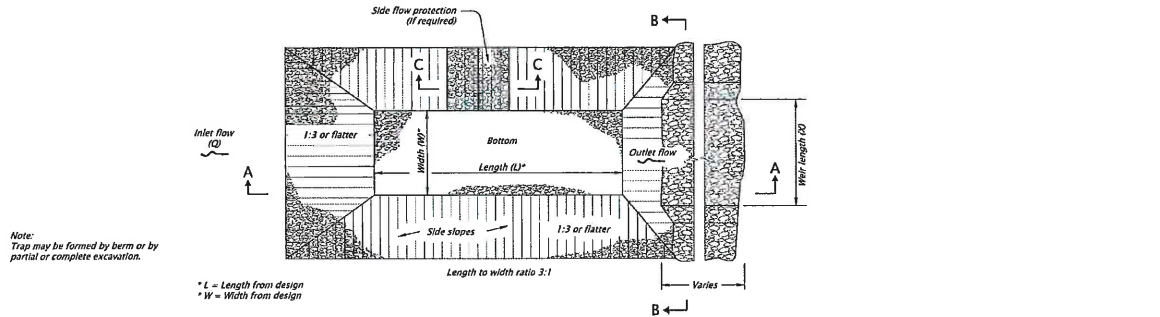
2018

DATE: \_\_\_\_\_

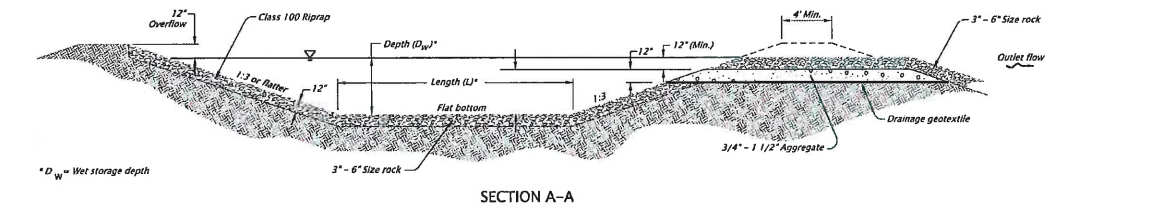
REVISION DESCRIPTION: \_\_\_\_\_

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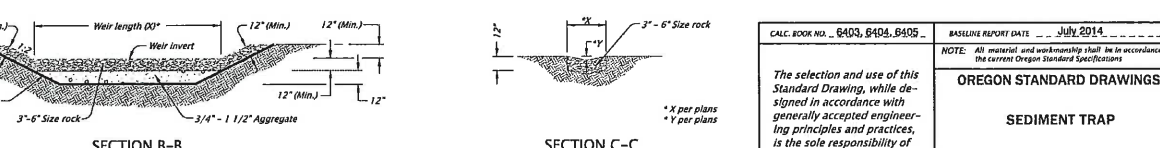
Effective Date: December 1, 2019 - May 31, 2020 RD1060



PLAN



SECTION A-A



SECTION B-B

SECTION C-C

SEDIMENT TRAP

CALC. BOOK NO. 6403, 6404, 6405

BASELINE REPORT DATE: July 2014

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.

OREGON STANDARD DRAWINGS

SEDIMENT TRAP

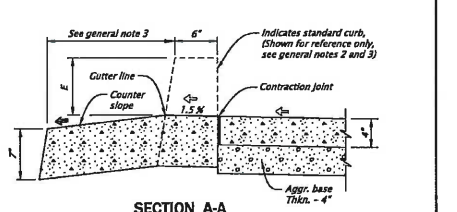
2018

DATE: \_\_\_\_\_

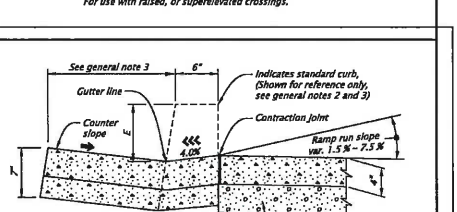
REVISION DESCRIPTION: \_\_\_\_\_

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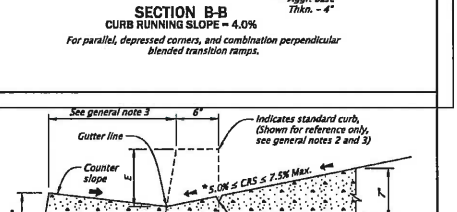
Effective Date: December 1, 2019 - May 31, 2020 RD1065



SECTION A-A  
CURB RUNNING SLOPE = 5.5%

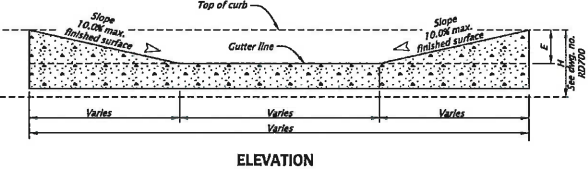


SECTION B-B  
CURB RUNNING SLOPE = 4.0%

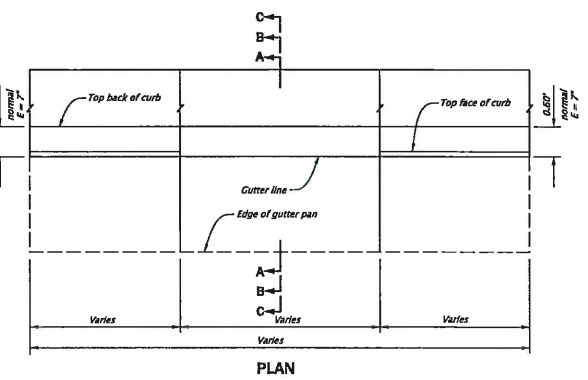


SECTION C-C  
CURB RUNNING SLOPE = 5.0%

06-01-2017  
rd1065.dgn  
RD1065



ELEVATION



PLAN

- LEGEND:
- ↘ Cross slope 1.5% maximum (Maximum 2.0% finished surface slope) (Normal sidewalk cross slope)
  - ↙ Running slope 4.0% maximum (Maximum 4.9% finished surface slope)
  - ↘ Running slope 7.5% maximum (Maximum 5.0% finished surface slope) Slope as required for drainage
  - ↙ Counter slope 4.0% maximum ascending or descending (Maximum 5.0% finished surface slope) Slope as required for drainage
  - ↘ Flare slope (Maximum 10.0% finished surface slope)

- GENERAL NOTES FOR DESIGNER:
1. Design curb exposure "E" between 3 inches and 7 inches based on site conditions. Label "E" height on curb ramp details per DET 1720 and DET 1721.
  2. See det. no. RD700, and project plans, for curb details.
  3. Width to match adjacent gutter pan, if present.
  4. For profile and cross slope, see project plans.

The selection and use of this detail, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

OREGON DEPARTMENT OF TRANSPORTATION  
TECHNICAL SERVICES  
DETAILS

CURB RAMP  
CURB AND GUTTER

DETAIL NO.  
DET1762

REVISIONS

BY:	DATE:	DESCRIPTION:
JA		

Grants Pass • Jacksonville • Medford, OR  
 GP Office: 1677 William Hwy, Suite 116, Grants Pass, OR, 97527  
 JVH Office: 460 Carnegie Dr., Jacksonville, OR, 97501  
 Ph: 541-871-9424 • Fax: 541-361-9929 • www.emc-engineers.com  
 EMC-Engineers/Scientists, LLC



REGISTERED PROFESSIONAL ENGINEER  
 12,650 PE  
 OREGON  
 5041 (JACK)  
 RENEWAL DATE: 12/31/21

PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 COMMERCIAL ROADS AND STOCKPILE

DRAWN BY: CD/JW  
 DATE: 15-JULY-2022  
 JOB No: 022-2202  
 SHEET No:  
**C5.3**  
 SWPPP  
 DETAILS



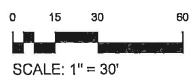
**EXISTING CONDITIONS NOTES**

- ① BOAT LAUNCHING RAMP
- ② BOAT STORAGE AREA (NOT A PART)
- ③ EXCESS SPOILS STORAGE PILE
- ④ BOAT LIFT DOCK
- ⑤ AC PAVING AREA
- ⑥ EXISTING AREA DRAIN
- ⑦ CONCRETE RETAINING WALL (HEIGHT VARIES)
- ⑧ CONCRETE PAD
- ⑨ CHAIN LINK FENCE
- ⑩ CHAIN LINK GATE
- ⑪ STORAGE CONTAINERS
- ⑫ ELECTRICAL TRANSFORMER
- ⑬ DOCK GANGWAY
- ⑭ MANHOLE
- ⑮ HOG WIRE FENCE
- ⑯ 7" DIA. CULVERT
- ⑰ EXISTING STORM DRAIN
- ⑱ PROPERTY LINE PER ASSESSOR MAP
- ⑲ ROAD R/W
- ⑳ TRENCH DRAIN
- ㉑ 30" STORM DRAIN EASEMENT
- ㉒ 2" WATER LINE
- ㉓ UG POWER & OTHER UTILITIES
- ㉔ COMPACTED GRAVEL
- ㉕ SLUDGE SUMP

NOTE:  
ALL EXISTING UG UTILITIES TO BE PROTECTED DURING CONSTRUCTION



VERTICAL DATUM  
MEAN LOWER LOW WATER EPOCH 1983-2001.  
BENCH MARK UTILIZED FOR THIS SURVEY  
US ARMY CORPS OF ENGINEERS  
BENCH MARK - "FUEL 2"  
ELEVATION - 21.65 FEET



① EXISTING CONDITIONS  
SCALE: 1" = 30' (24x36)

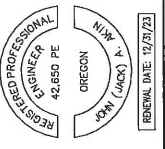


NOTE:  
EVERY EFFORT HAS BEEN MADE TO VERIFY ALL EXISTING SITE ELEMENTS AND CONDITIONS PER THE EXISTING SITE SURVEY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL EXISTING CONDITIONS, ELEVATIONS AND MEASUREMENTS. IF A DISCREPANCY IS FOUND, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY FOR RESOLUTION.



REVISIONS	BY:

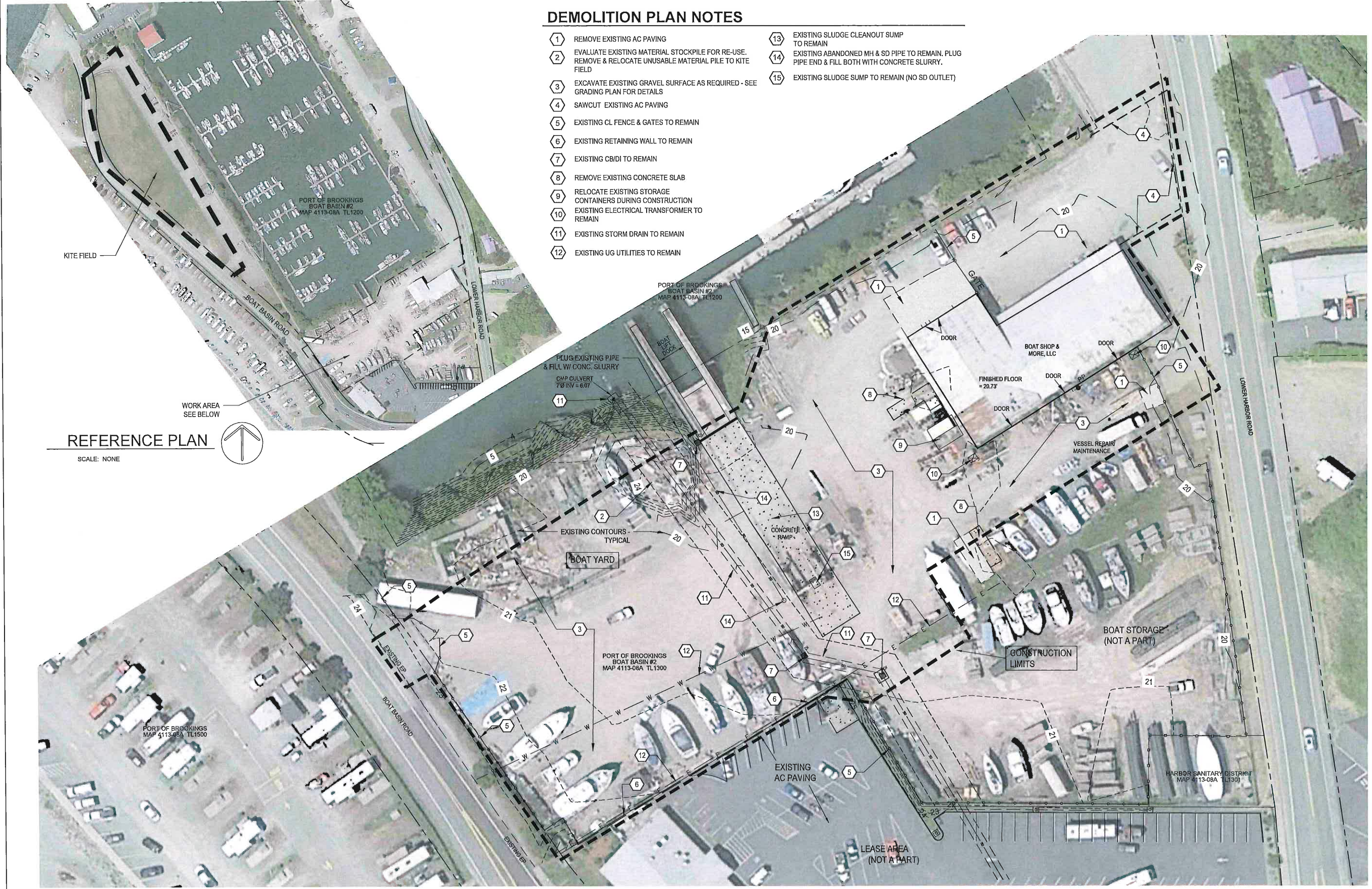
Grants Pass • Jacksonville • Medford, OR  
 2111 Grant Pass, OR 97527  
 4000 Commercial Dr., Jacksonville, OR 97530  
 Ph: 541-474-9434 • Cell: 541-261-9929 • Fax: 541-727-5488  
 emc@emcengineerscientists.com | http://www.emcengineerscientists.com  
**EMC** - Engineers/Scientists, LLC



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:

**C2.0**  
 EXISTING  
 CONDITIONS



**DEMOLITION PLAN NOTES**

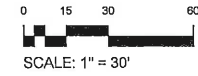
- 1 REMOVE EXISTING AC PAVING
- 2 EVALUATE EXISTING MATERIAL STOCKPILE FOR RE-USE. REMOVE & RELOCATE UNUSABLE MATERIAL PILE TO KITE FIELD
- 3 EXCAVATE EXISTING GRAVEL SURFACE AS REQUIRED - SEE GRADING PLAN FOR DETAILS
- 4 SAWCUT EXISTING AC PAVING
- 5 EXISTING CL FENCE & GATES TO REMAIN
- 6 EXISTING RETAINING WALL TO REMAIN
- 7 EXISTING CB/DI TO REMAIN
- 8 REMOVE EXISTING CONCRETE SLAB
- 9 RELOCATE EXISTING STORAGE CONTAINERS DURING CONSTRUCTION
- 10 EXISTING ELECTRICAL TRANSFORMER TO REMAIN
- 11 EXISTING STORM DRAIN TO REMAIN
- 12 EXISTING UG UTILITIES TO REMAIN
- 13 EXISTING SLUDGE CLEANOUT SUMP TO REMAIN
- 14 EXISTING ABANDONED MH & SD PIPE TO REMAIN. PLUG PIPE END & FILL BOTH WITH CONCRETE SLURRY.
- 15 EXISTING SLUDGE SUMP TO REMAIN (NO SD OUTLET)

**REFERENCE PLAN**

SCALE: NONE



VERTICAL DATUM  
MEAN LOWER LOW WATER EPOCH 1983-2001.  
BENCH MARK UTILIZED FOR THIS SURVEY  
US ARMY CORPS OF ENGINEERS  
BENCH MARK - "FUEL 2"  
ELEVATION - 21.65 FEET



**1 DEMOLITION PLAN**  
SCALE: 1" = 30' (24x36)



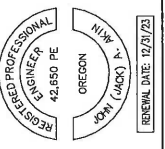
NOTE:  
ALL EXISTING UG UTILITIES TO BE PROTECTED DURING CONSTRUCTION



REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 GP Office: 1807 Williams Hwy, Suite 214, Grants Pass, OR, 97527  
 JV Office: 400 Conestoga Dr., Jacksonville, OR, 97530  
 PH: 541-474-9434 • Cell: 541-261-9929 • Fax: 541-737-4488  
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**EMC**  
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**PORT OF BROOKINGS HARBOR**  
16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415

**BOAT YARD PAVING**

DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:

**C2.01**  
DEMOLITION PLAN

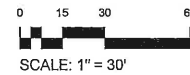
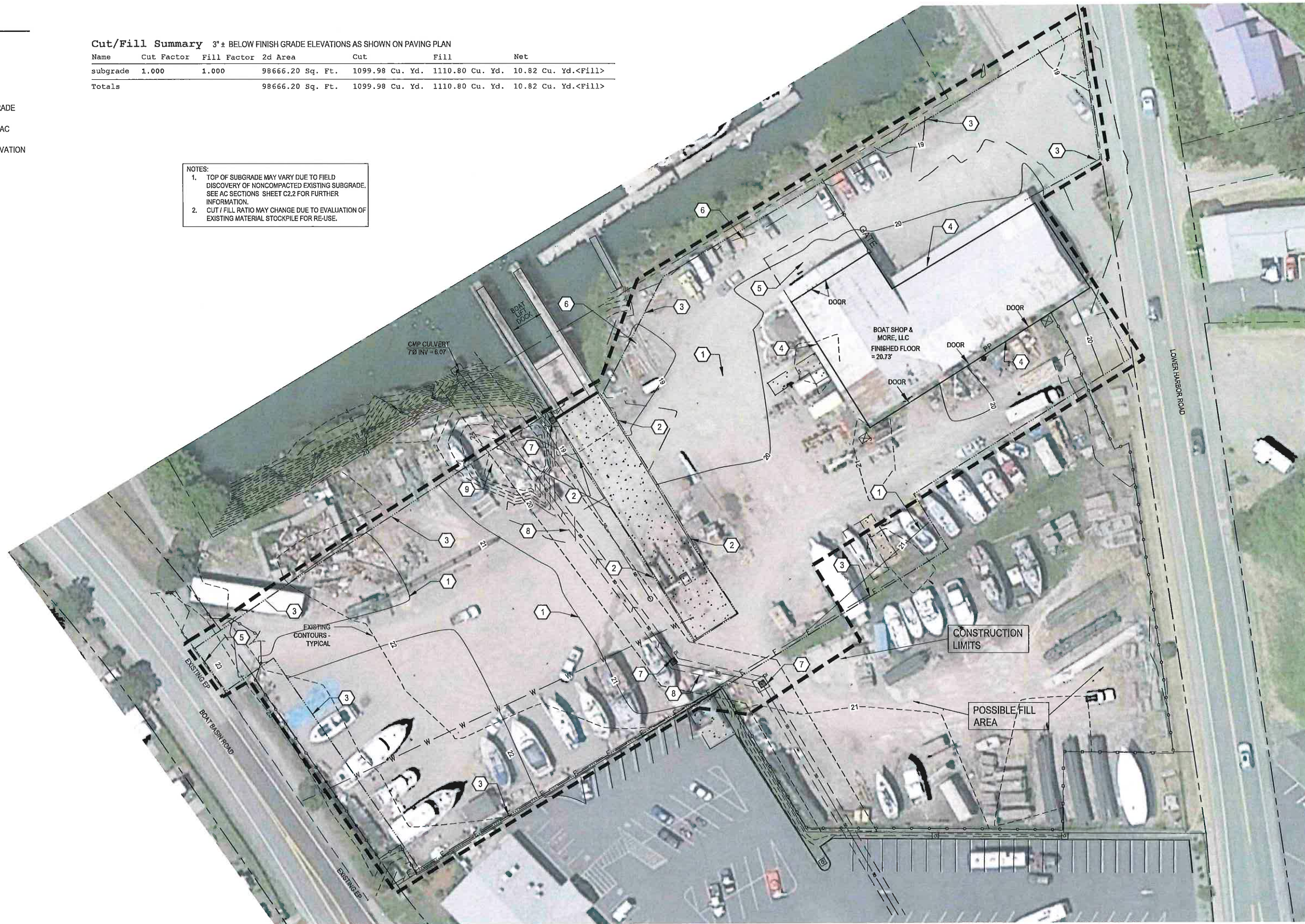
### GRADING PLAN NOTES

- ① SUBGRADE CONTOURS ±
- ② EXCAVATE SUBGRADE TO ±0.25' BELOW TOP OF CONC.
- ③ APPROX. PAVING LIMITS (ENGINEER FIELD VERIFY)
- ④ BEGIN SUBGRADE EXCAVATION AT EXISTING BUILDING GRADE
- ⑤ EXCAVATE SUBGRADE TO ±0.25' BELOW TOP OF EXISTING AC
- ⑥ EXCAVATE SUBGRADE TO ±0.25' BELOW TOP OF BANK ELEVATION
- ⑦ EXISTING DI OR CB
- ⑧ EXISTING 84" CULVERT
- ⑨ EVALUATE EXISTING MATERIAL STOCKPILE FOR RE-USE. REMOVE & RELOCATE UNUSABLE MATERIAL PILE TO KITE FIELD

**Cut/Fill Summary** 3"± BELOW FINISH GRADE ELEVATIONS AS SHOWN ON PAVING PLAN

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
subgrade	1.000	1.000	98666.20 Sq. Ft.	1099.98 Cu. Yd.	1110.80 Cu. Yd.	10.82 Cu. Yd.<Fill>
<b>Totals</b>			<b>98666.20 Sq. Ft.</b>	<b>1099.98 Cu. Yd.</b>	<b>1110.80 Cu. Yd.</b>	<b>10.82 Cu. Yd.&lt;Fill&gt;</b>

NOTES:  
 1. TOP OF SUBGRADE MAY VARY DUE TO FIELD DISCOVERY OF NONCOMPACTED EXISTING SUBGRADE. SEE AC SECTIONS SHEET C2.2 FOR FURTHER INFORMATION.  
 2. CUT / FILL RATIO MAY CHANGE DUE TO EVALUATION OF EXISTING MATERIAL STOCKPILE FOR RE-USE.



①

### SUB-GRADE GRADING PLAN

SCALE: 1" = 30' (24x36)

SEE PAVING PLAN FOR AC DESIGN SECTIONS

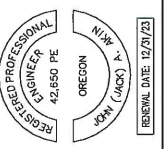


NOTE:  
 SUBGRADE EXCAVATION VOLUME MAY VARY DUE TO FIELD DISCOVERY OF NONCOMPACTED EXISTING SUBGRADE. SEE AC SECTIONS THIS SHEET FOR FURTHER INFORMATION.



REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 4200 NE Williams St., Grants Pass, OR, 97527  
 4th Fl. Office, 400 Congress Dr., Jacksonville, OR, 97530  
 Ph: 541-474-9434 • Call: 541-241-9929 • Fax: 541-727-5488  
[emc@emcengineerscientists.com](mailto:emc@emcengineerscientists.com) <http://www.emcengineerscientists.com>  
**-Engineers/Scientists, LLC**



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

DRAWN BY: CD/JJV  
 DATE: 15 JULY 2022  
 JOB No: 22-202201

SHEET No:  
**C2.1**  
 GRADING  
 PLAN

96



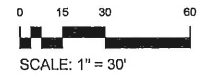
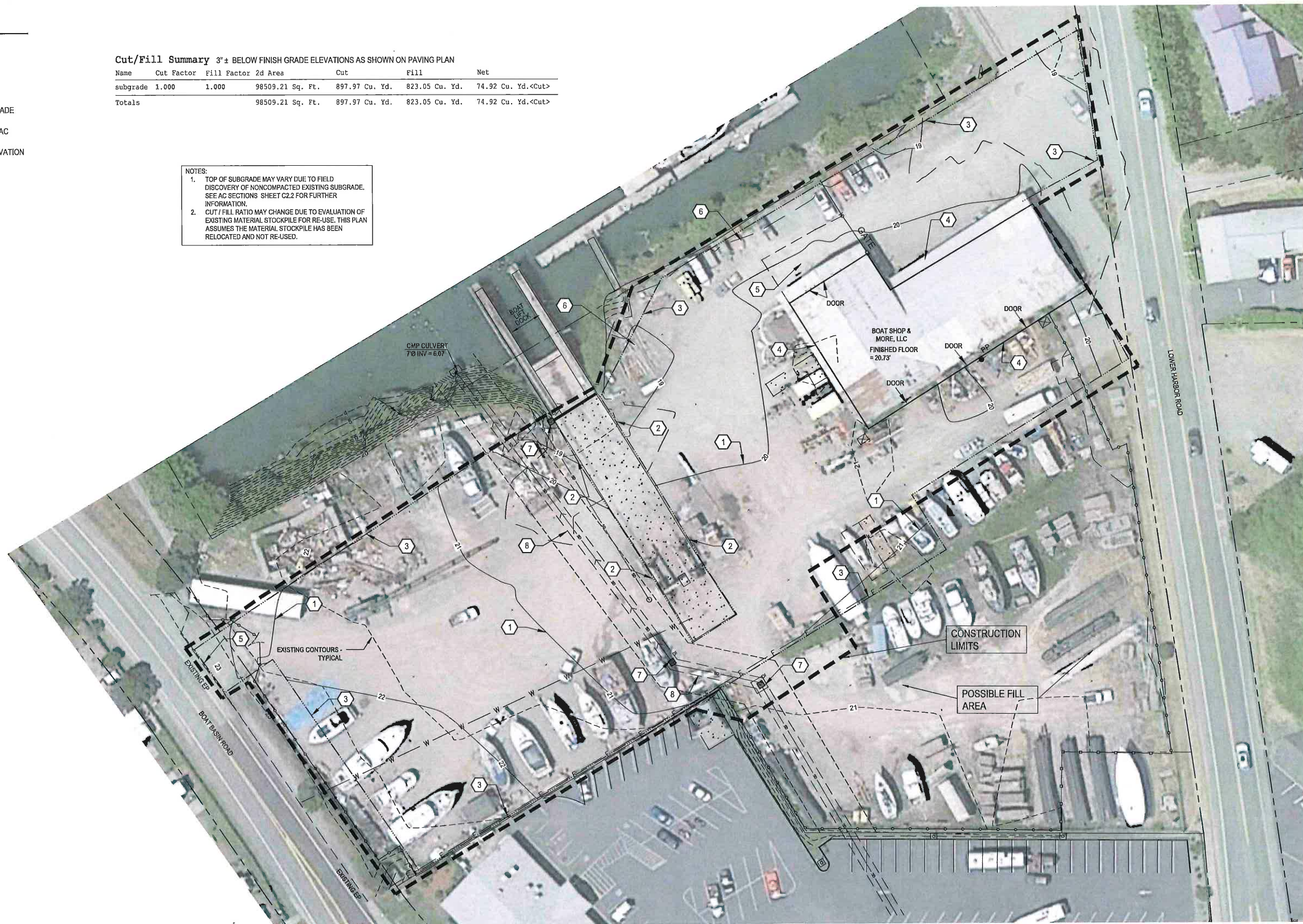
### GRADING PLAN NOTES

- 1 SUBGRADE CONTOURS = APPROX. -0.5' BELOW FG
- 2 EXCAVATE SUBGRADE TO ±0.25' BELOW TOP OF CONC.
- 3 APPROX. PAVING LIMITS (ENGINEER FIELD VERIFY)
- 4 BEGIN SUBGRADE EXCAVATION AT EXISTING BUILDING GRADE
- 5 EXCAVATE SUBGRADE TO ±0.25' BELOW TOP OF EXISTING AC
- 6 EXCAVATE SUBGRADE TO ±0.25' BELOW TOP OF BANK ELEVATION
- 7 EXISTING DI OR CB
- 8 EXISTING 84" CULVERT

#### Cut/Fill Summary 3"± BELOW FINISH GRADE ELEVATIONS AS SHOWN ON PAVING PLAN

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
subgrade	1.000	1.000	98509.21 Sq. Ft.	897.97 Cu. Yd.	823.05 Cu. Yd.	74.92 Cu. Yd.<Cut>
<b>Totals</b>			<b>98509.21 Sq. Ft.</b>	<b>897.97 Cu. Yd.</b>	<b>823.05 Cu. Yd.</b>	<b>74.92 Cu. Yd.&lt;Cut&gt;</b>

NOTES:  
 1. TOP OF SUBGRADE MAY VARY DUE TO FIELD DISCOVERY OF NONCOMPACTED EXISTING SUBGRADE. SEE AC SECTIONS SHEET C2.2 FOR FURTHER INFORMATION.  
 2. CUT / FILL RATIO MAY CHANGE DUE TO EVALUATION OF EXISTING MATERIAL STOCKPILE FOR RE-USE. THIS PLAN ASSUMES THE MATERIAL STOCKPILE HAS BEEN RELOCATED AND NOT RE-USED.



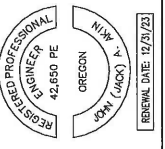
1 SUB-GRADE GRADING PLAN  
 SCALE: 1" = 30' (24x36)  
 SEE PAVING PLAN FOR AC DESIGN SECTIONS

NOTE:  
 SUBGRADE EXCAVATION VOLUME MAY VARY DUE TO FIELD DISCOVERY OF NONCOMPACTED EXISTING SUBGRADE. SEE AC SECTIONS THIS SHEET FOR FURTHER INFORMATION.



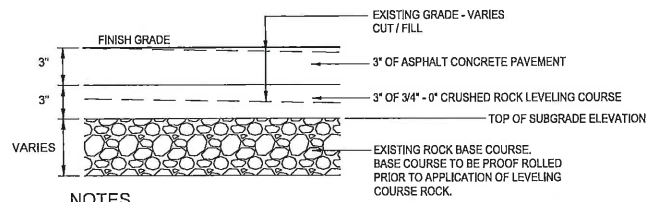
REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 CP Office: 1847 Williams Hwy., Suite 215, Grants Pass, OR, 97527  
 JY Office: 450 Constance Dr., Jacksonville, OR, 97530  
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**Engineers/Scientists, LLC**



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

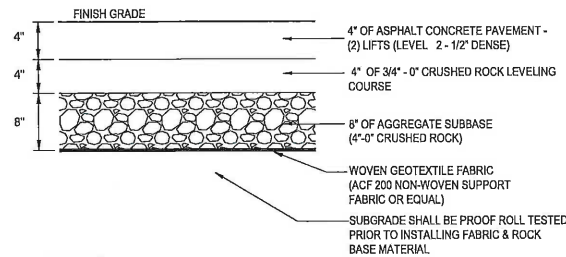
DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:  
**C2.1.1**  
 ALT. GRADING PLAN



**NOTES**

1. PROOF ROLL EXISTING SOILS TO BE USED AS SUBGRADE.
2. NOTIFY ENGINEER IMMEDIATELY OF ANY FAILED AREAS AND REPAIR AS DIRECTED.

**2 PAVING SECTION - ATOP WELL COMPACTED SUBGRADE**  
SCALE: N.T.S.



**NOTES**

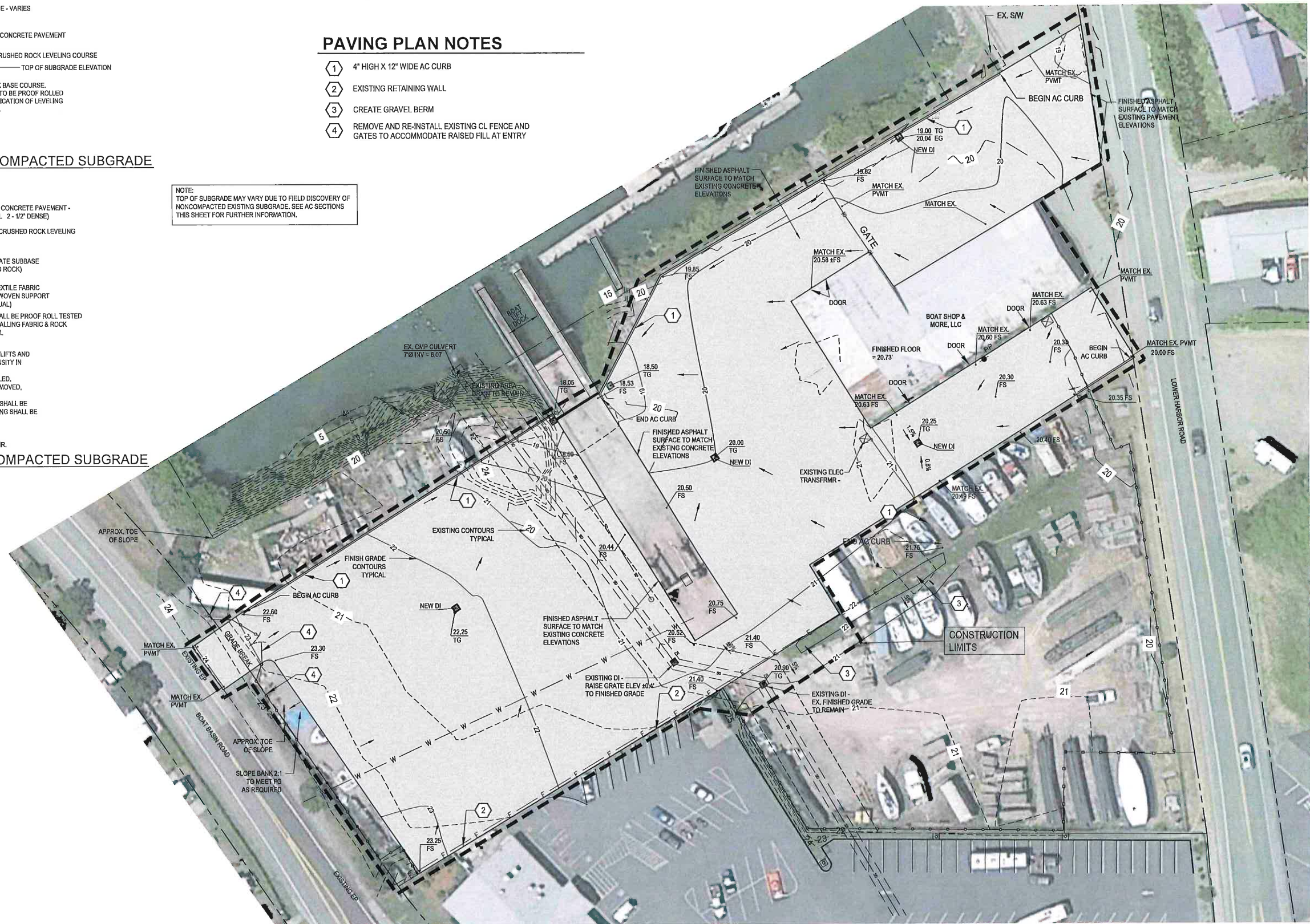
1. AGGREGATE BASE AND SUBBASE SHALL BE INSTALLED IN MAXIMUM 6" LIFTS AND MECHANICALLY COMPACTED TO A MINIMUM 98% OF THE MAXIMUM DENSITY IN ACCORDANCE WITH THE AASHTO T99 METHOD.
2. JUST PRIOR TO PAVING, THE AGGREGATE BASE SHALL BE PROOF ROLLED. AGGREGATES THAT DO NOT PASS PROOF ROLL TESTING SHALL BE REMOVED, RECOMPACTED AND TESTED AGAIN.
3. JUST PRIOR TO INSTALLING AGGREGATE BASE ROCK, THE SUBGRADE SHALL BE PROOF ROLLED. SUBGRADE THAT DOES NOT PASS PROOF ROLL TESTING SHALL BE REMOVED AND ADDITIONAL CRUSHED ROCK INSTALLED.
4. PAVEMENT SECTION IS BASED ON THE ASSUMPTION THAT PAVEMENT CONSTRUCTION WILL BE ACCOMPLISHED DURING THE DRY SEASON.
5. PAVEMENT SUBJECT TO CONSTRUCTION TRAFFIC MAY REQUIRE REPAIR.

**3 PAVING SECTION - ATOP NON-COMPACTED SUBGRADE**  
SCALE: N.T.S.

**PAVING PLAN NOTES**

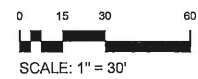
- 1 4" HIGH X 12" WIDE AC CURB
- 2 EXISTING RETAINING WALL
- 3 CREATE GRAVEL BERM
- 4 REMOVE AND RE-INSTALL EXISTING CL FENCE AND GATES TO ACCOMMODATE RAISED FILL AT ENTRY

NOTE:  
TOP OF SUBGRADE MAY VARY DUE TO FIELD DISCOVERY OF NONCOMPACTED EXISTING SUBGRADE. SEE AC SECTIONS THIS SHEET FOR FURTHER INFORMATION.



**LEGEND**

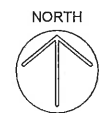
— APPROX. DIRECTION OF DRAINAGE FLOW



**1 FINISHED PAVING PLAN**

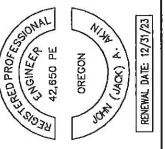
SCALE: 1" = 30' (24x36)

NOTE:  
TOP OF AC MAY VARY DUE TO FIELD DISCOVERY OF UNFORESEEABLE EXISTING CONDITIONS. CONTACT ENGINEER TO EVALUATE AND MAKE RECOMMENDATIONS REGARDING SUCH CONDITIONS OR SITUATIONS. SEE AC SECTIONS THIS SHEET FOR FURTHER INFORMATION.



BY:	
REVISIONS	

Grants Pass • Jacksonville • Medford, OR  
 GP Office: 1807 Williams Hwy, Suite 216, Grants Pass, OR, 97527  
 JV/LE Office: 450 Conestoga Dr., Jacksonville, OR, 97530  
 PH: 541-474-9434 • Cell: 541-361-9929 • Fax: 541-727-5488  
 emc@emcengineerscientists.com • http://www.emcengineerscientists.com



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:

**C2.2**  
 FINISHED PAVING PLAN

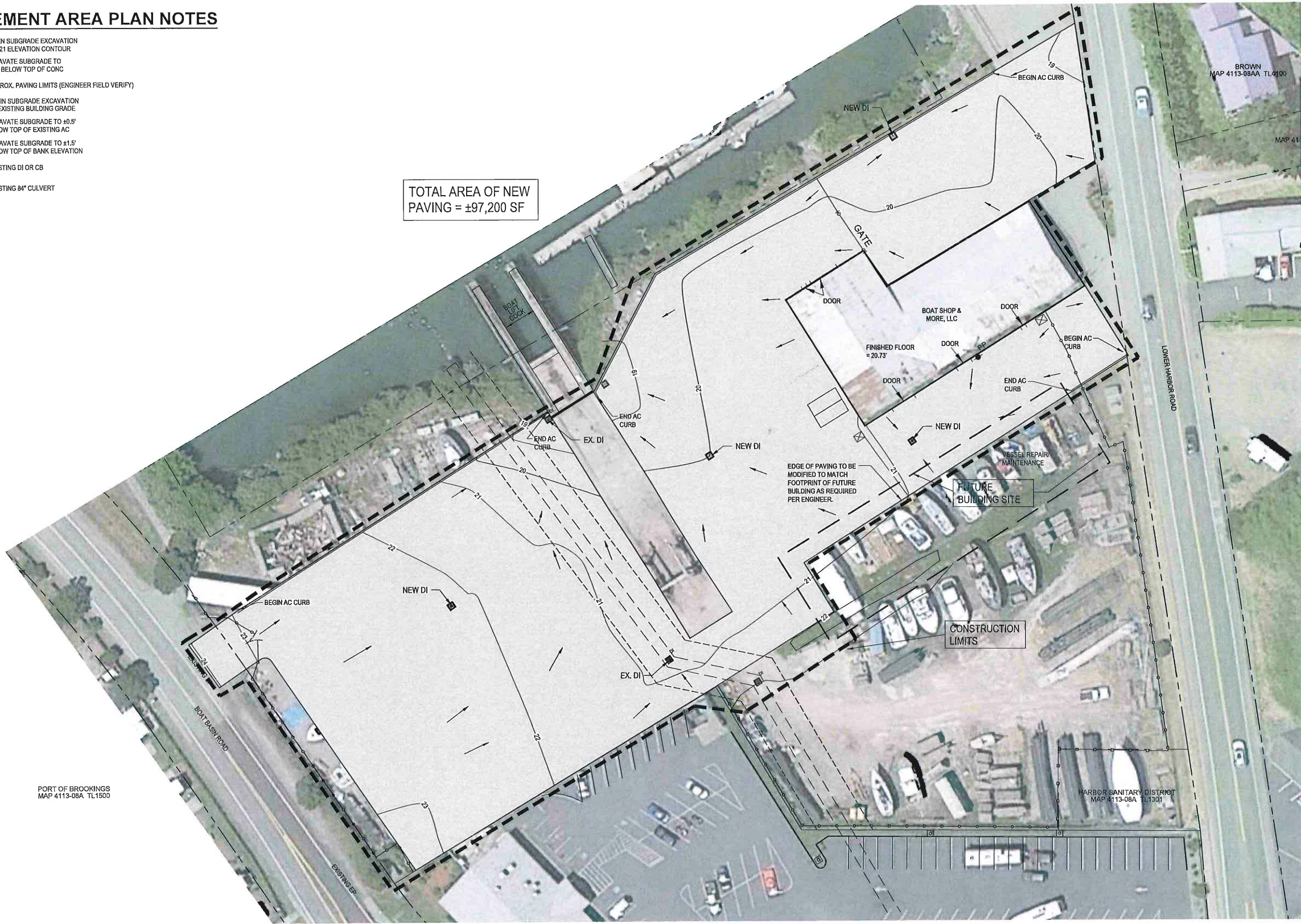


98

**PAVEMENT AREA PLAN NOTES**

- ① BEGIN SUBGRADE EXCAVATION AT ±21 ELEVATION CONTOUR
- ② EXCAVATE SUBGRADE TO ±1.5' BELOW TOP OF CONC
- ③ APPROX. PAVING LIMITS (ENGINEER FIELD VERIFY)
- ④ BEGIN SUBGRADE EXCAVATION AT EXISTING BUILDING GRADE
- ⑤ EXCAVATE SUBGRADE TO ±0.5' BELOW TOP OF EXISTING AC
- ⑥ EXCAVATE SUBGRADE TO ±1.5' BELOW TOP OF BANK ELEVATION
- ⑦ EXISTING DI OR CB
- ⑧ EXISTING 84" CULVERT

TOTAL AREA OF NEW PAVING = ±97,200 SF



PORT OF BROOKINGS  
MAP 4113-08A TL1500

BROWN  
MAP 4113-08AA TL4400

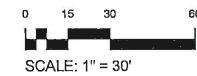
NIED  
MAP 4113-08AA TL4000

GRIFFITH  
MAP 4113-08AA TL4200

DRIFTWOOD, LLC  
MAP 4113-08AD TL4000

HARBOR SANITARY DISTRICT  
MAP 4113-08A TL1301

LEGEND  
— DIRECTION OF DRAINAGE FLOW

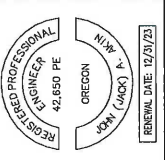


① PAVEMENT AREA PLAN  
SCALE: 1" = 30' (24x36)



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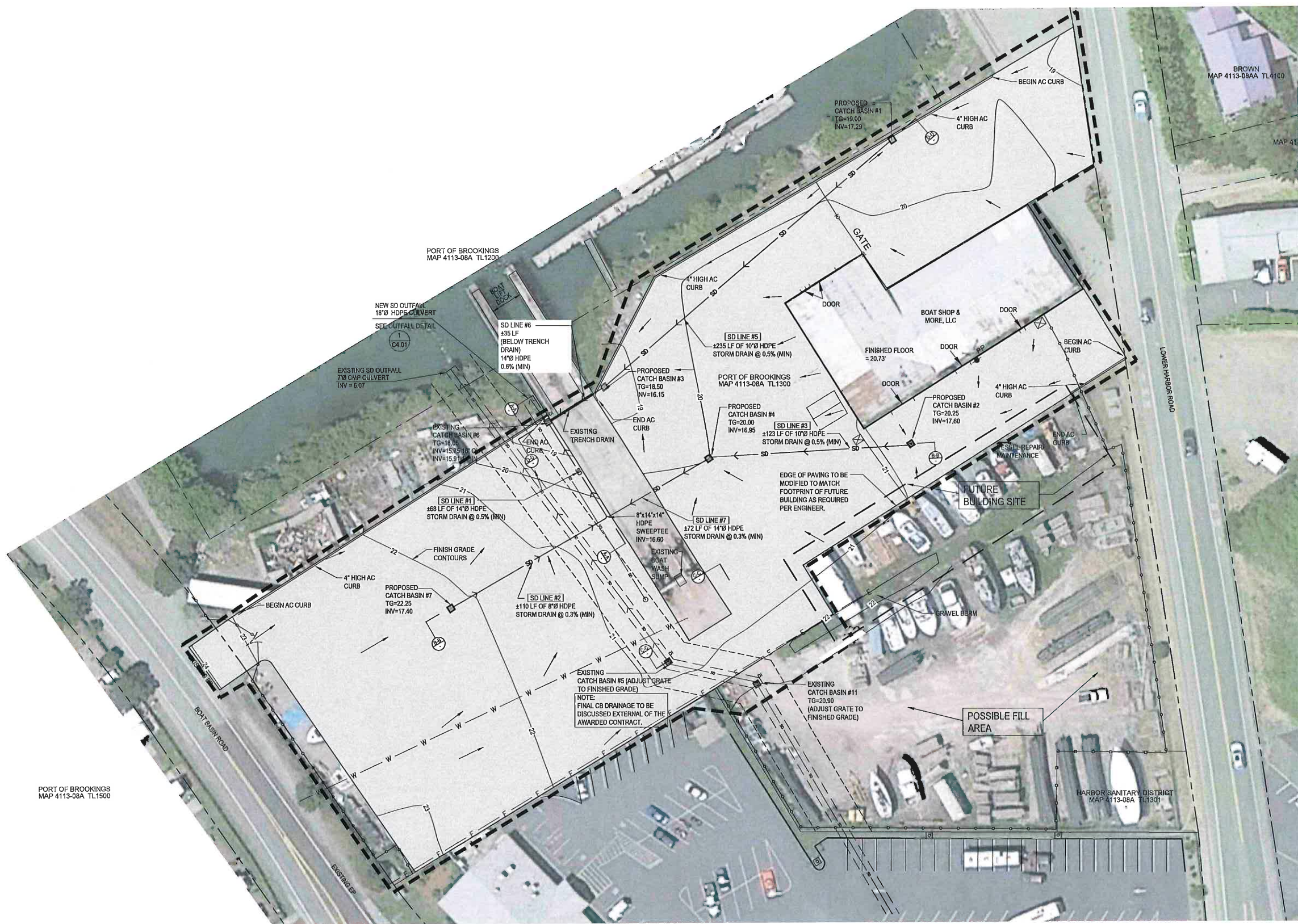
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 4000 N. Main Street, Suite 200, Medford, OR 97504  
 1000 N. Main Street, Suite 200, Medford, OR 97504  
 Ph: 541-774-9434 • Cell: 541-261-9929 • Fax: 541-737-5488  
 sms@emcengineerscientists.com | http://www.emcengineerscientists.com  
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**BOAT YARD PAVING**

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**C3.0**  
 PAVEMENT AREA



PORT OF BROOKINGS  
MAP 4113-08A TL1500

BROWN  
MAP 4113-08AA TL4100

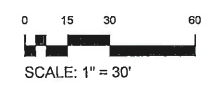
NIED  
MAP 4113-08AA TL4000

GRIFFITH  
MAP 4113-08AA TL4200

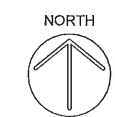
DRIFTWOOD, LLC  
MAP 4113-08AD TL4000

HARBOR SANITARY DISTRICT  
MAP 4113-08A TL1301

**LEGEND**  
 ———— APPROX. DIRECTION OF DRAINAGE FLOW

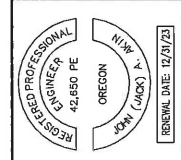


**1 STORMWATER CONVEYANCE**  
SCALE: 1" = 30' (24x36)



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Grants Pass \* Jacksonville \* Medford, OR  
 CP Office: 1807 Williams Hwy., Suite 215, Grants Pass, OR, 97527  
 Ph: 541-474-6434 \* Call: 541-261-9929 \* Fax: 541-727-5488  
 emc@emcengineers.com <http://www.emcengineers.com>  
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DRAWN BY: CD/JW  
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 SHEET No:

**C4.0**  
 STORMWATER  
 CONVEYANCE

Flow Pipe Size Locations

Catch Basin	L, ft, WC' Pathway	Est. Δ Elevation	Est. Slope	Assumed Tc Sheet, Minutes'	Assumed R <sub>100</sub> in/hr Zone 1	Surface n <sub> Manning</sub>	Calculated Tc Sheet, Minutes	Calculated R <sub>100</sub> Sheet, in/hr
CB1	138	0.5	0.004	5	4.0	0.013	4.1	4.0
CB2	142	1	0.007	3	5.0	0.013	3.1	5.0
CB3	185	1.2	0.006	5	4.0	0.013	4.1	4.0
CB4	150	1.7	0.011	3	5.0	0.013	2.8	5.0
CB5	208	2	0.010	4	4.2	0.013	3.8	4.2
CB6	248	5	0.020	3	5.0	0.013	3.2	5.0
CB7	115	2.5	0.022	2	6.0	0.013	1.8	6.0
TRENCH	160	2	0.013	4	4.2	0.02	3.9	4.2

	Area, ft. <sup>2</sup>	Area, AC	Invert Elevation, ft.	Surface Coefficient	Flow, CFS'
CB1	18926.0	0.43	17.29	0.9	1.56
CB2	12830.0	0.29	17.6	0.9	1.33
CB3	18459.0	0.42	16.15	0.9	1.53
CB4	15925.0	0.37	16.95	0.9	1.65
CB5	10652.0	0.24	15.6	0.9	0.92
CB6	35531.0	0.82	15.91	0.9	3.67
CB7	5803.0	0.13	17.4	0.9	0.72
TRENCH	4874.0	0.11	17.09	0.9	0.42

1-WC is Watercourse Pathway from the furthest distance within a zone to its respective catch basin.

2-Tc calculated via adjusting assumed value and iterative adjustment until assumed matches calculated, via  $T_c = 0.93 (L^{0.6} n^{0.5}) / (2.48 S^{0.5})$

3-Q<sub>100</sub> = Σ C L<sub>100</sub> A<sub>Area</sub>

4-Pipes Sized via Manning

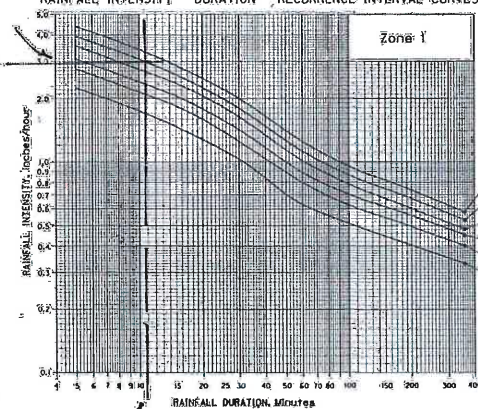
Pipe Flow	Δ Elevation, ft.	Pipe Length, ft.	Slope	Pipe n <sub>Manning</sub>	Flow, CFS, 80%	Pipe Dia., Inches'	Nominal, ID, Bell & Spigot	Notes
CB1-CB3	1.2	235	0.005	0.012	1.6	10	10	80% full
CB3-CB6	0.2	35	0.006	0.012	3.1	14	15	80% full
CB2-CB4	0.65	123	0.005	0.012	1.3	10	10	80% full
CB4-TEE	0.35	72	0.005	0.012	3.0	14	15	80% full
CB7-TEE	0.8	118	0.007	0.012	0.7	8	8	80% full
TEE-CB6	0.35	68	0.005	0.012	3.7	14	15	80% full
CB6-OUT			0.005	0.012	6.8	18	18	80% full
TRENCH					0.42	6.0	8	8" ok

Ref: ODOT Hydrology Manual, Appendix F-Rational Method

SW Calculations  
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*If it is found to be 11 minutes, the L<sub>100</sub> is 21 ft. The L<sub>100</sub> is to be about 20 ft.*

RAINFALL INTENSITY - DURATION - RECURRENCE INTERVAL CURVES



Dual-Wall (Type S) Bell and Spigot Style Specification Chart

Nominal Inside Diameter (Inches)	Outside Diameter (Inches)	Minimum Wall Thickness (Inches)	Minimum Wall Stiffness (psi)	Lengths (Feet)	Weight (lbs./ft.)	Solid, Perforated or Both	Perforation Class 1,2 or Both'
4	4.7	34.0	0.002	20	0.5	Both	Class 2 Slotted
6	6.9	34.0	0.002	20	1.0	Both	Class 2 Slotted
8	9.4	50.0	0.025	20	1.6	Both	Class 2 Slotted
10	11.8	50.0	0.025	20	2.5	Both	Class 2 Slotted
12	14.8	50.0	0.035	20	3.3	Both	Both
15	17.5	42.0	0.040	20	4.9	Both	Both
18	21.5	40.0	0.050	20	6.5	Both	Both
24	27.9	34.0	0.060	20	11.4	Both	Both
30	35.7	29.0	0.060	20	15.0	Both	Both
36	42.1	22.5	0.070	20	19.6	Both	Both
42	48.2	21.0	0.070	20	25.2	Both	Both
48	54.3	20.0	0.070	20	34.5	Both	Both
60	Call	Call	Call	20	Call	Both	Both

ASTM M 252 - Standard Specification for Corrugated Polyethylene Drainage Pipe 3'-10" (75mm to 250mm)

ASTM M 294 - Standard Specification for Corrugated Polyethylene Pipe, 12'-60" (300mm to 1500mm)

ASTM F 405 - Standard Specification for Corrugated Polyethylene (PE) Pipes and Fittings, 3'-6"

ASTM F 887 - Standard Specification for 8 through 24 inch Corrugated Polyethylene Pipe and Fittings

ASTM F 2308 - Standard Specification for 12 to 60 inch (300mm to 1500mm) Annular Corrugated Polyethylene Polyethylene Pipe and Fittings for Gravity-Flow Storm Sewer and Subsurface Drainage Applications

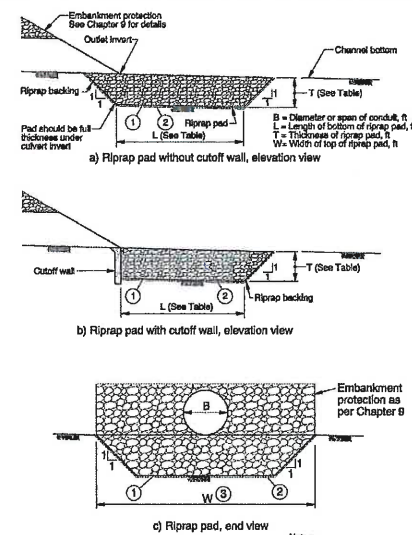
ASTM F 2648 - Standard Specification for 2 to 60 inch (50 to 1500mm) Annular Corrugated Polyethylene (PE) Pipe and Fittings for Land Drainage Applications (rigid and recycled resin)

ASTM D 2321 - Standard Practice for Underground Installation of Thermoplastic Pipe for Sewer and Other Gravity-Flow Applications

ASTM D 3390 - Standard Specification for Polyethylene Fittings, Pipe and Fittings Materials

ASTM F 477 - Standard Specification for Elastomeric Seals (Gaskets) for Sewing Plastic Pipe

ASTM D 3212 - Standard Specification for Joints for Drain and Sewer Plastic Pipes Using Flexible Elastomeric Seals



Riprap Class	L'	T	W
50	48 or 1.3	2.3	
100	48 or 1.6	3.3	
200	48 or 2.0	4.3	
700	48 or 3.3	5.6	

\* L' is the greater of 48 or the listed dimension.

Figure 11-16 Dimensions of Riprap Pads

Class 50 Riprap Pad  
Class 100 Riprap Pad  
Class 200 Riprap Pad  
Class 700 Riprap Pad

Example 1 - Embankment Brink Depth = 1.5 feet  
Example 2 - Riprap pad equivalent Embankment Brink Depth = 1.3 feet  
Example 3 - Riprap pad equivalent Embankment Brink Depth = 1.1 feet  
Example 4 - Small Basin minimum Embankment Brink Depth = 1.0 feet  
Example 5 - Large Riprap Basin Embankment Brink Depth = 1 foot

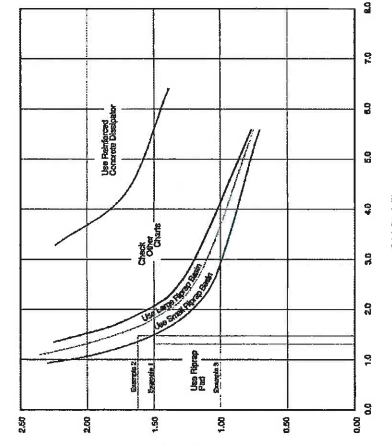


Figure 11-17 ODOT Class 50 Riprap Energy Dissipator Selection Chart

OUTFALL-ENERGY DISSIPATION, RIPRAP PAD

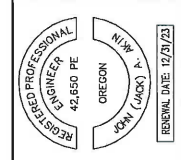
Design Routine

- H = V<sup>2</sup>/2G & Yc = (a/2)0.5, where V = outfall velocity from CB#6 in ft/s, G = 32.2 ft/s<sup>2</sup>, Yc = Equivalent Brink (Pad Depth), ft., and Qcfs = Va; given 6.8 cfs from 1.5ft. diam. Pipe, with a = (1.52)(3.14)/4 = 1.77 ft.2 and Yc = (1.77/2)0.5 = 0.94ft. V = Q/a = 6.8 cfs/1.77 = 3.84 ft/s
- Froude # = V/(32.2Yc)0.5 = 0.698
- Froude # < 1 indicates subcritical flow (can be considered tranquil)
- Utilizing Figure 11-17, with ODOT Class 50 Riprap, ODOT Hydraulics Manual, Riprap Pad use is acceptable.
- Utilizing Figure 11-16, ODOT Hydraulics Manual, Length of Pad should at least be 4D, and width at least 5D, where D = pipe/culvert diameter, or 6 ft. long by 7.5 ft. wide. Depth of Pad should replace Brink calculation, and specified to be 2.5 ft.
- The Riprap Pad is not provided with a cutoff wall. Use ODOT Class 700 for Pad backing, as shown in Figure 11-16.
- Pad must be placed on a gentle slope (less than or equal to 15%)

1 OUTFALL DETAIL  
SCALE: N.T.S.

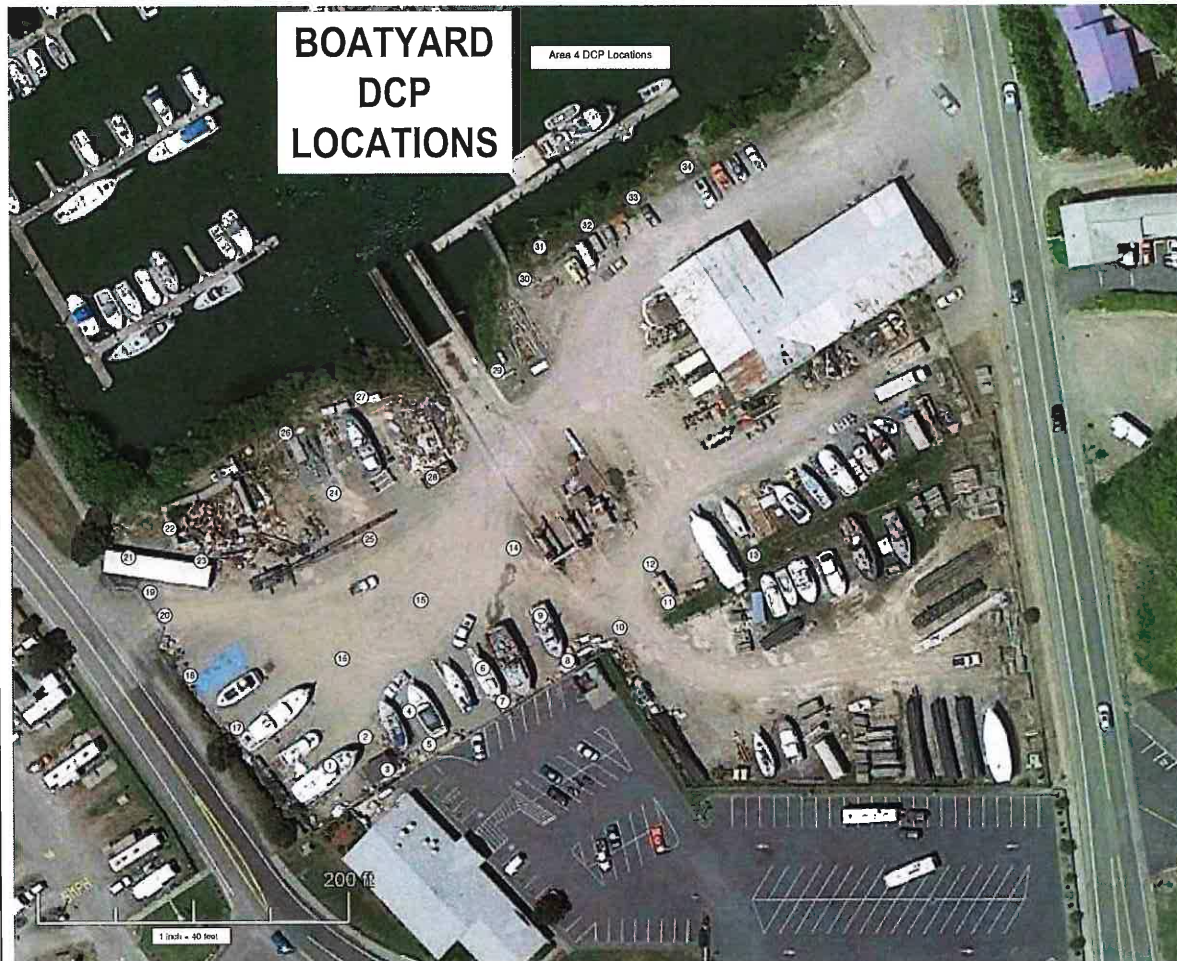
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 BOAT YARD PAVING

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**C4.01**  
 STORMWATER CALCULATIONS



**Table 3 - Tabulated Correlation of blows per 2' penetration versus CBR and PSF**

Hammer 17.6 lbs Blows/2'	Hammer 10.1 lbs Blows/2'	CBR			PSF		
		Soil type			Soil type		
		Other	CL	CH	Other	CL	CH
1	1	2	0	3	760	260	1240
1	2	4	1	7	1270	660	1960
1	3	6	3	10	1720	1130	2560
2	4	8	5	14	2130	1660	3100
2	5	10	8	17	2520	2230	3600
3	6	12	12	21	2880	2840	4060
3	7	15	15	24	3240	3240	4500
4	8	17	17	27	3570	3570	4920
4	9	19	19	31	3900	3900	5320
5	10	22	22	34	4220	4220	5700
5	11	24	24	38	4530	4530	6070
6	12	27	27	41	4830	4830	6430
6	13	29	29	45	5130	5130	6790
7	14	32	32	48	5420	5420	7130
7	15	34	34	51	5700	5700	7460
8	16	37	37	55	5980	5980	7790
8	17	39	39	58	6260	6260	8110
9	18	42	42	62	6530	6530	8420
9	19	45	45	65	6800	6800	8730
10	20	47	47	69	7060	7060	9030
10	21	50	50	72	7320	7320	9330
11	22	53	53	75	7580	7580	9620
11	23	55	55	79	7840	7840	9910
12	24	58	58	82	8090	8090	10200
12	25	61	61	86	8340	8340	10480
13	26	63	63	89	8580	8580	10750
13	27	66	66	92	8830	8830	11020
14	28	69	69	96	9070	9070	11290
14	29	72	72	99	9310	9310	11560
15	30	74	74	100	9550	9550	11820
15	31	77	77		9780	9780	
16	32	80	80		10020	10020	
16	33	83	83		10250	10250	
17	34	86	86		10480	10480	
17	35	89	89		10710	10710	
18	36	91	91		10930	10930	
18	37	94	94		11160	11160	
19	38	97	97		11380	11380	
19	39	100	100		11600	11600	
20	40	100	100		11820	11820	

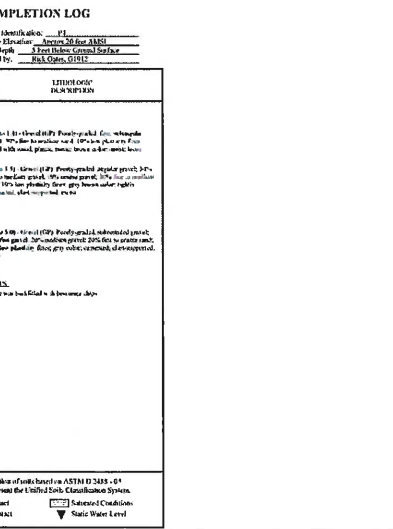
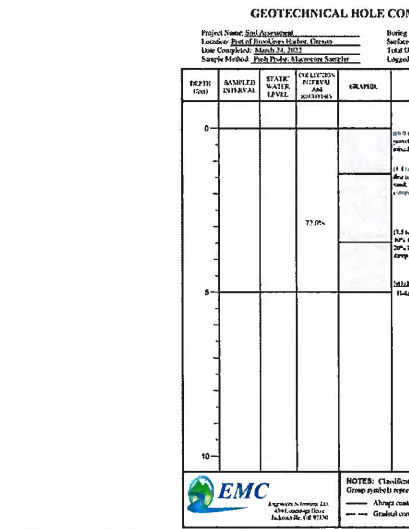
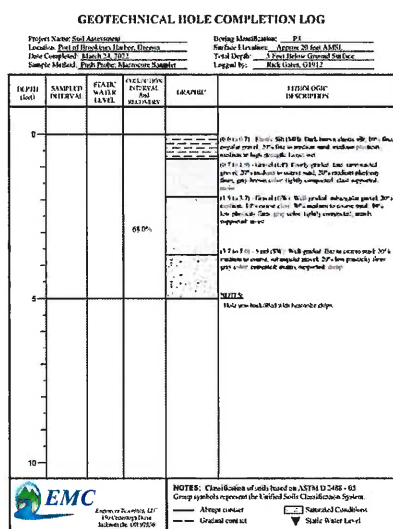
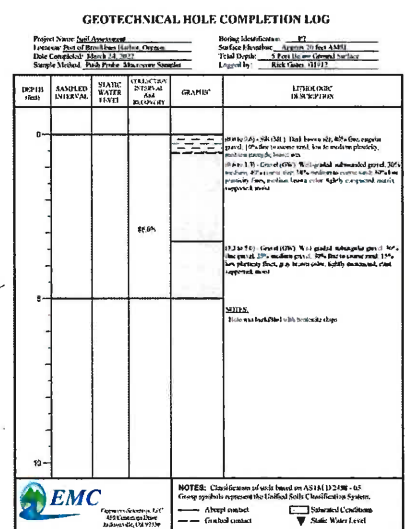
**Table 4 - Tabulated Correlation of blows per 4' penetration versus CBR and PSF**

Hammer 17.6 lbs Blows/4'	Hammer 10.1 lbs Blows/4'	CBR			PSF		
		Soil type			Soil type		
		Other	CL	CH	Other	CL	CH
1	1	1	0	2	460	110	780
1	2	2	0	3	760	260	1240
2	3	3	1	5	1030	450	1620
2	4	4	1	7	1280	660	1960
3	5	5	2	9	1510	890	2270
3	6	6	3	10	1730	1140	2570
4	7	7	4	12	1940	1390	2840
4	8	8	5	14	2140	1660	3110
5	9	9	7	15	2330	1950	3360
5	10	10	8	17	2520	2240	3600
6	11	11	11	19	2710	2710	3840
6	12	12	12	21	2890	2890	4070
7	13	13	13	22	3070	3070	4290
7	14	15	15	24	3240	3240	4500
8	15	16	16	26	3410	3410	4720
8	16	17	17	27	3580	3580	4920
9	17	18	18	29	3740	3740	5120
9	18	19	19	31	3910	3910	5320
10	19	21	21	33	4070	4070	5520
10	20	22	22	34	4220	4220	5710
11	21	23	23	36	4380	4380	5900
11	22	24	24	38	4530	4530	6080
12	23	25	25	39	4690	4690	6260
12	24	27	27	41	4840	4840	6440
13	25	28	28	43	4990	4990	6620
13	26	29	29	45	5130	5130	6790
14	27	30	30	46	5280	5280	6970
14	28	32	32	48	5420	5420	7140
15	29	33	33	50	5570	5570	7310
15	30	34	34	51	5710	5710	7470
16	31	36	36	53	5850	5850	7640
16	32	37	37	55	5990	5990	7800
17	33	38	38	57	6130	6130	7960
17	34	39	39	58	6270	6270	8120
18	35	41	41	60	6400	6400	8280
18	36	42	42	62	6540	6540	8430
19	37	43	43	63	6670	6670	8590
19	38	45	45	65	6810	6810	8740
19	39	46	46	67	6940	6940	8890
20	40	47	47	69	7070	7070	9040

**Table 5 - Tabulated Correlation of blows per 6' penetration versus CBR and PSF**

Hammer 17.6 lbs Blows/6'	Hammer 10.1 lbs Blows/6'	CBR			PSF		
		Soil type			Soil type		
		Other	CL	CH	Other	CL	CH
1	1	0	0	1	340	60	600
1	2	1	0	2	560	150	950
2	3	2	0	3	760	260	1240
2	4	2	1	5	940	390	1500
3	5	3	1	7	1110	520	1740
3	6	4	1	7	1280	660	1960
4	7	4	2	8	1430	810	2170
4	8	5	2	9	1580	970	2370
5	9	6	3	10	1730	1140	2570
5	10	6	4	11	1870	1310	2750
6	11	7	4	13	2000	1480	2930
6	12	8	5	14	2140	1660	3110
7	13	9	5	15	2270	1850	3280
7	14	9	7	16	2400	2040	3440
8	15	10	8	17	2520	2240	3600
8	16	11	11	18	2650	2450	3760
9	17	12	12	19	2770	2770	3920
9	18	12	19	21	2890	2890	4070
10	19	13	21	22	3010	3010	4220
10	20	14	22	23	3120	3120	4360
11	21	15	23	24	3240	3240	4500
11	22	15	24	25	3350	3350	4630
12	23	16	25	26	3470	3470	4790
12	24	17	27	27	3580	3580	4920
13	25	18	28	29	3690	3690	5060
13	26	19	29	30	3800	3800	5190
14	27	19	30	31	3910	3910	5320
14	28	20	32	32	4010	4010	5450
15	29	21	33	33	4120	4120	5580
15	30	22	34	34	4220	4220	5710
16	31	23	36	35	4330	4330	5830
16	32	23	37	37	4430	4430	5960
17	33	24	38	38	4530	4530	6080
17	34	25	39	39	4640	4640	6200
18	35	26	41	40	4740	4740	6320
18	36	27	42	41	4840	4840	6440
19	37	28	43	42	4940	4940	6560
19	38	28	45	43	5040	5040	6680
19	39	29	46	45	5130	5130	6790
20	40	30	47	46	5230	5230	6910

NOTE:  
DCP AREAS REPRESENTED BY POINTS LESS THAN  
2000 LBS/FT WILL NEED SUBBASE/SUBGRADE  
REINFORCEMENT

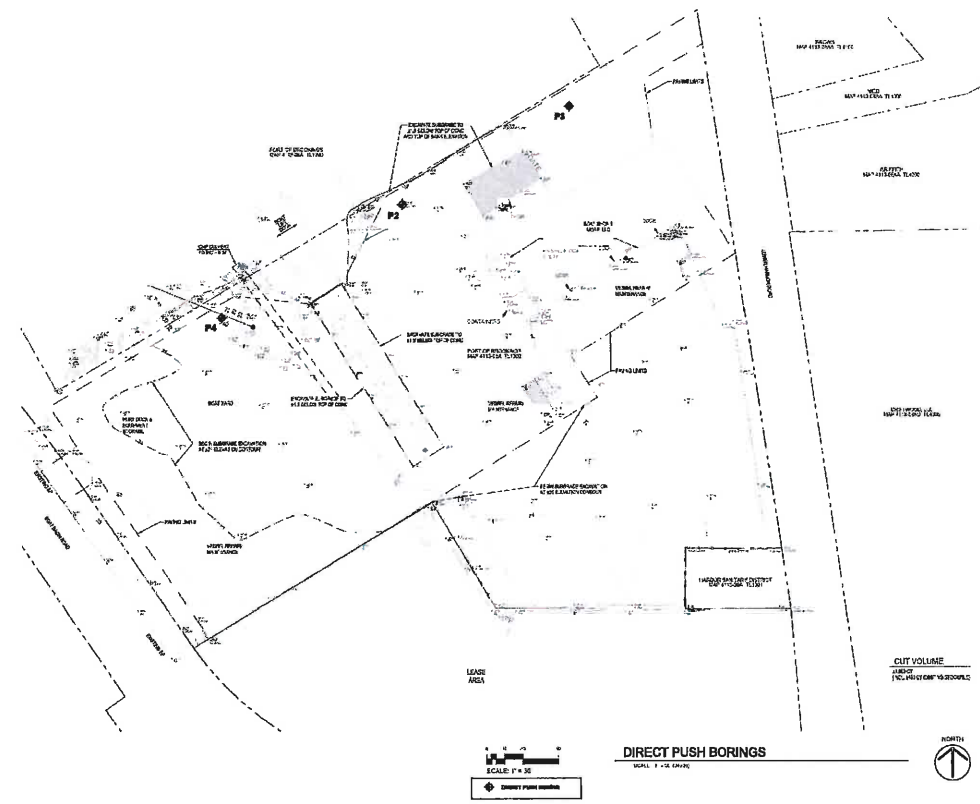


**DCP DATA SHEET**

Project Port of Brookings Boat Yard  
Material Class GW-CL  
Pavement None  
Moisture 10.50%  
Date/Time 1/19/2022 0930  
Personal RG  
Hammer Wt. 10 lbs  
Weather Clear, Dry

Site ID	Station	Depth	Blows	PSF
Area 4	1	2	12	4830
Area 4	2	2	10	4220
Area 4	3	4	12	2890
Area 4	4	2	9	3900
Area 4	5	4	14	3240
Area 4	6	2	10	4220
Area 4	7	2	10	4220
Area 4	8	6	12	2140
Area 4	9	2	15	5700
Area 4	10	2	10	4220
Area 4	11	4	11	2710
Area 4	12	2	11	4530
Area 4	13	4	11	2710
Area 4	14	2	>12	>5420
Area 4	15	2	>15	>5700
Area 4	16	2	>16	>5700
Area 4	17	4	15	3410
Area 4	18	6	10	1870
Area 4	19	4	10	2520
Area 4	20	2	>15	>5700
Area 4	21	2	10	4220
Area 4	22	4	14	3240
Area 4	23	6	12	2140
Area 4	24	4	12	2890
Area 4	25	2	15	5700
Area 4	26	6	9	1730
Area 4	27	6	5	1110
Area 4	28	2	10	4220
Area 4	29	2	10	4220
Area 4	30	4	14	3240
Area 4	31	6	9	1730
Area 4	32	4	9	2330
Area 4	33	4	12	2890
Area 4	34	2	10	4220

PFS: Pounds per square Foot  
PFS Values from Table 3, Table 4, and Table 5 of KSE DCP Owners Manual



REVISIONS


Grants Pass \* Jacksonville \* Medford, OR  
 CP Office: 1867 Williams Hwy., Suite 216, Grants Pass, OR, 97527  
 Phone: 541-776-5434 \* Fax: 541-777-5488  
 Email: emc@emcengineers.com \* Website: www.emcengineers.com

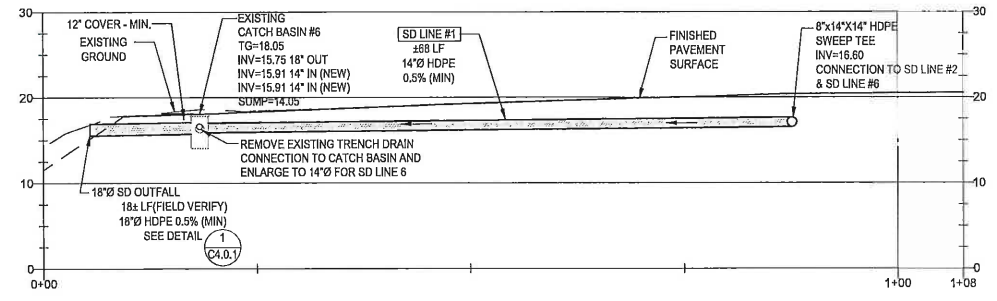
**EMC**  
 ENGINEERS/SCIENTISTS, LLC

REGISTERED PROFESSIONAL  
 ENGINEER  
 42,680 PE  
 OREGON  
 NEW YORK  
 (LACK)

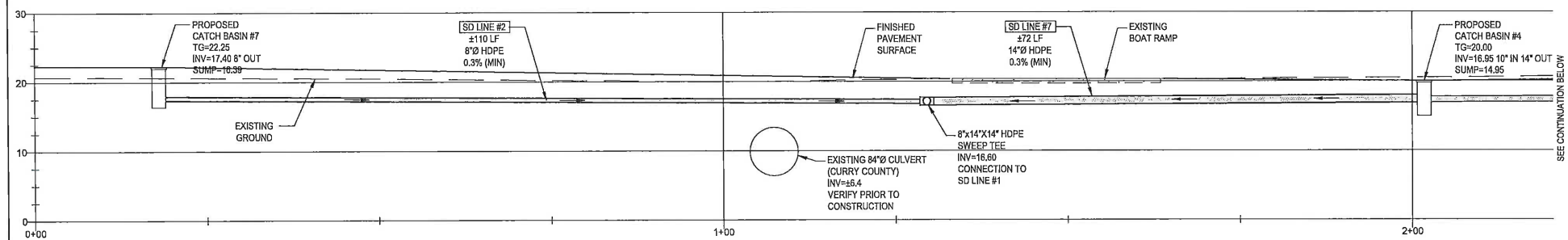
EXPIRES: 12/31/23

**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

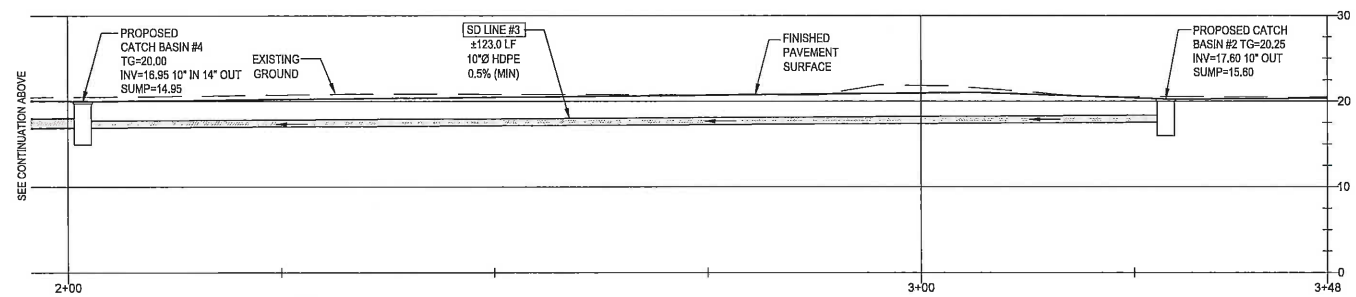
DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:  
**C4.02**  
 DCP DATA



**A-A** STORMDRAIN LINE 1 - PROFILE "A - A"  
SCALE: 1" = 10' (H & V)



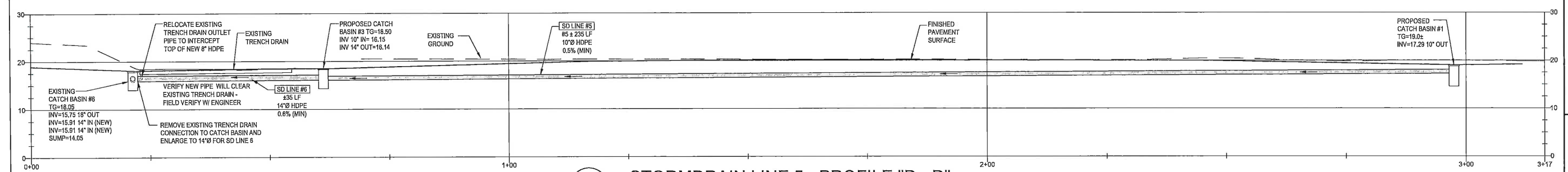
**B-B** STORMDRAIN LINE 2 - PROFILE "B - B"  
SCALE: 1" = 10' (H & V)



**B-B** STORMDRAIN LINE 3 - PROFILE "B - B"  
SCALE: 1" = 10' (H & V)

**C-C** NOT USED  
SCALE: -

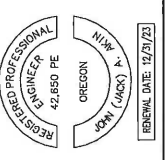
NOTE: PROVIDE 12" MIN. COVER FOR ALL SD PIPES, TYPICAL. VERIFY AND CORRECT IF REQUIRED.



**D-D** STORMDRAIN LINE 5 - PROFILE "D - D"  
SCALE: 1" = 10' (H & V)

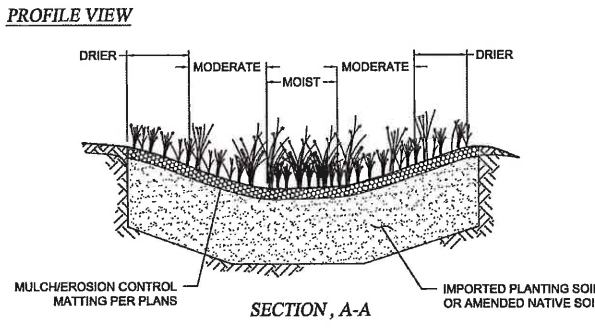
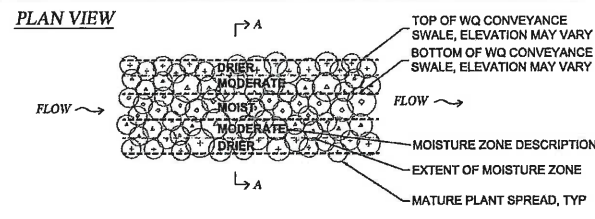
REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 1647 Williams Hwy., Suite 215 Grants Pass, OR 97527  
 541-747-9434 • Cell: 541-261-9929 • Fax: 541-727-5488  
 emc@emcengineerscientists.com • http://www.emcengineerscientists.com  
**Engineers/Scientists, LLC**



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
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**C4.1**  
 STORMDRAIN  
 PROFILES



**LEGEND:**  
 --- INDICATES GRADE BREAK  
 --- MOISTURE ZONE  
 PLANT SPECIES APPROPRIATE FOR MOISTURE ZONE:  
 ○ DRIER  
 ○ MODERATE  
 ○ MOIST

**NOTES:**  
 1. THIS DETAIL IS PROVIDED AS A SCHEMATIC EXAMPLE OF THE RANDOM PLANT PLACEMENT AND 90% COVERAGE AFTER ESTABLISHMENT PERIOD DESIRED TO REDUCE EROSION AND WEEDS.  
 2. INSTALL PLANTS PER PLANS, ACCORDING TO LANDSCAPE DESIGN PLANT TABLE, WHICH SHOULD INCLUDE PLANT SPECIES, SPACING, AND QUANTITIES IN EACH MOISTURE ZONE.  
 3. MOISTURE ZONES VARY FROM THOSE SHOWN DEPENDING ON GRADING PLAN, LOCATION OF INLET(S) AND OUTLET(S) AND FACILITY SHAPE.

Rogue Valley Stormwater Design Manual | Water Quality Conveyance Swale Planting Schematic | BMP 8.03 1 of 1 Scale: NTS

**General Notes for Vegetated BMPs**

- Excluding construction of the facility itself, exposed treatment area subgrade shall be fenced to prohibit impacts from construction (including materials and equipment storage).
- Build and vegetate swales as early as possible to establish plantings before directing stormwater runoff to it or divert stormwater around facility. Preferably, vegetation will be given a minimum of 3 months to become established, or per landscape architect/designer guidelines.
- Call the reviewing agency 48 hours in advance of constructing this facility so construction observation may be performed to identify variations in the field that may affect design and verify proper construction.
- Over-excavate within the swale to allow for placement of amended or imported soil to final grade.
- Placement of amended native or imported soil mix shall occur as follows:
  - Place soil in 6 inch maximum lifts (i.e. depths).
  - Do not place if soil is saturated.
  - Lightly compact each lift, (e.g. a water filled landscape roller) to achieve 85% compaction. Do not compact with heavy machinery or vibratory compaction.
- Install energy dissipation below all outfalls per approved plans.
- If unprotected soil has been exposed to rainfall, excavate the surface to a depth of 4 inches to restore filtration capacity.
- Install ODOT Type E erosion control matting, if specified in approved plans.
- Landscaping plan must adhere to one of the scenarios in Tables 1 and 2 of section 4.5.2. Plant per Landscaping plan and standard detail 4.5.2C. Contact approving jurisdiction 48 hours in advance of planting so that jurisdiction can review plant placement prior to plant installation.
- Install mulch, if specified in approved plans. Use either shredded wood chips or coarse compost. Mulch must be eye, pesticide and weed free. Spread in a minimum two inch layer over bare soil or in a ring around plants to increase water retention. Ensure that mulch does not touch plant stems.
- Side slopes outside of flow area must be permanently stabilized with mulch and vegetation.

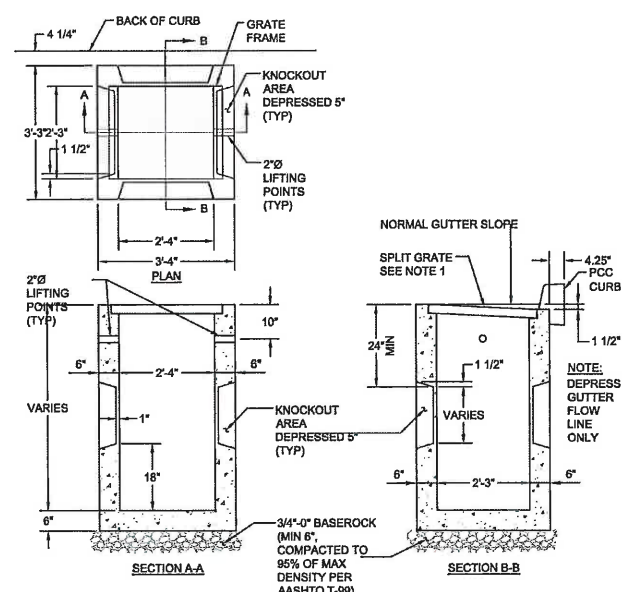
**AMENDED PLANTING SOIL MIX SPECIFICATIONS**  
 1. Planting soil may be either amended native or imported soil mix with the following characteristics:  
 A. Infiltrate between 0.5 and 12 inches/hour.  
 B. Be free of weed seeds, contaminants, and hazardous materials.  
 C. Organic content matter from 8-10% by weight  
 D. Cation exchange capacity (CEC) greater than or equal to 5 milliequivalents/100 grams of dry soil  
 E. 2-5% clayey fines content  
 F. pH between 5.5 and 8.0  
 G. Conform to the following gradation for the mix:

US Standard Sieve Size	Percent Passing
3/8"	100
#4	95-100
#10	75-90
#40	25-40
#100	4-10
#200	2-5

- Imported soil shall be roughly 1/3 plant derived compost, 1/3 topsoil and 1/3 gravelly sand.
- Amended native planting soil mix shall be created by blending compost into the native soil at a rate of 1 part compost to two parts soil. Soil mix must still meet the specifications in Note 1 above.
- Amended native or imported soil mix shall be uniformly mixed.

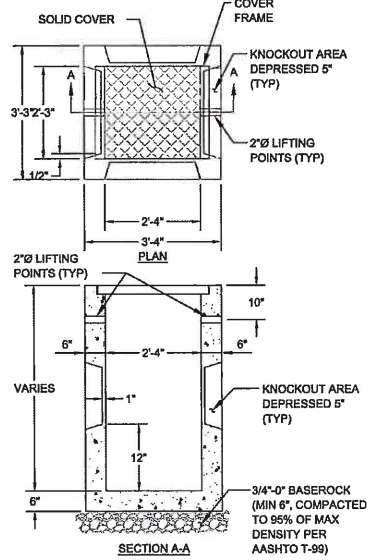
**Compost specifications**  
 Must be derived from plant material and fully composted. Must be certified weed seed free. A Technical Data Sheet from the US Composting Council Seal of Testing Assurance must be provided to the approving jurisdiction. The Data Sheet must show that the compost meets the following criteria:

- Organic matter content between 40 and 50 percent.
- pH between 5.5 and 8.0. If the pH isn't quite right, it may be lowered by adding iron sulfate and sulfur or raised by adding lime. If lime is used, incorporate first into the compost, wet the compost down, and then fork mixture into the soil.
- Soluble salt content shall be less than 0.06mmhos/cm.
- 100% should pass a 1/2-inch screen.
- Stability Test Result shall Be Stable or Very Stable.
- Maturity Indicator for emergence and vigor shall be  $\geq 80\%$ .
- Trace Metals Test Result shall Be Pass.
- Carbon nitrogen ratio between 30:1 and 35:1.



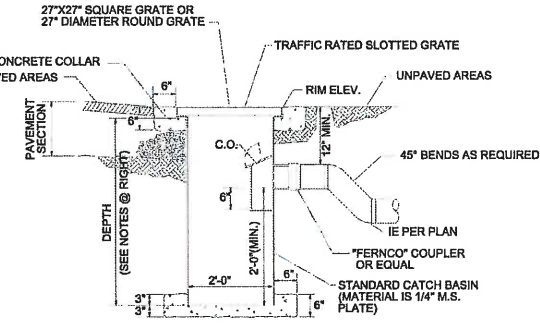
**NOTES**  
 1. GRATE AND FRAME MAY EACH BE OF CAST IRON OR WELDED STEEL CONSTRUCTION. GRATE AND FRAME TO BE ODOT G-2 TYPE 2 (BICYCLE SAFE).  
 2. FOR PRECAST BOX, CURB MUST BE HAND FORMED 10" EACH SIDE OF CATCH BASIN.  
 3. CONCRETE STRENGTH SHALL BE 5000 PSI WITH FIBER MESH.  
 4. CATCH BASIN AND GRATE SHALL MEET HS20 LOADING.  
 5. A MINIMUM SUMP DEPTH OF 18" IS REQUIRED.  
 6. OUTLET PIPE SHALL HAVE A MINIMUM OF 24" OF COVER.

**2 CATCH BASIN**  
 SCALE: NTS



**CONSTRUCTION NOTES:**  
 1. COVER AND FRAME MAY EACH BE OF CAST IRON OR WELDED STEEL CONSTRUCTION.  
 2. CONCRETE STRENGTH SHALL BE 5000 PSI WITH FIBER MESH.  
 3. JUNCTION BASIN AND GRATE SHALL MEET HS20 LOADING.  
 4. A MINIMUM SUMP DEPTH OF 18" IS REQUIRED.  
 5. OUTLET PIPE SHALL HAVE A MINIMUM OF 24" OF COVER.

**6 JUNCTION BASIN**  
 SCALE: NTS

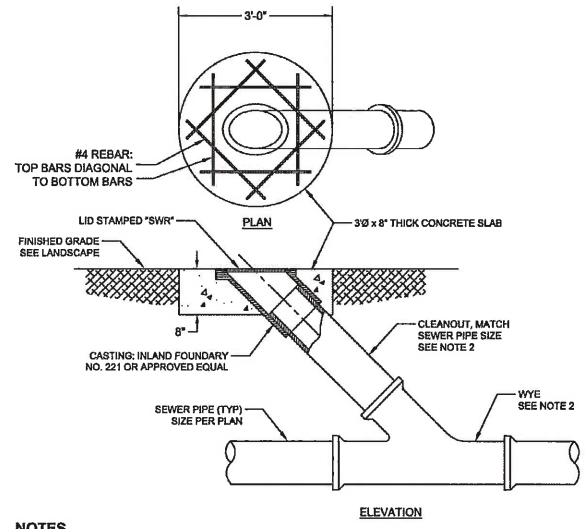


**GENERAL NOTES:**  
 1. ASPHALT COAT ALL SURFACES OF CATCH BASIN AND CLEANOUT AFTER WELDING OR HOT DIP GALVANIZE (AT MANUFACTURER'S DISCRETION).  
 2. BREAK SHARP CORNERS AFTER WELDING.  
 3. ALL WELDED STEEL CONSTRUCTION  
 4. 10 GAUGE STEEL MINIMUM THICKNESS  
 5. OPENING ON BOTTOM OF WATERSEAL TO BE GREATER, OR EQUAL TO AREA IN OUTLET PIPE.  
 6. STANDARD DEPTH: 48"

**NOTE:**  
 LYNCH OR APPROVED EQUAL CATCH BASIN.

**NOTE:**  
 LOCATE ROUND BASIN IN PAVED AREAS, AND SQUARE BASINS ARE TO BE USED WHEN LOCATED NEXT TO CURB.

**3 LYNCH STYLE CATCH BASIN**  
 SCALE: NTS



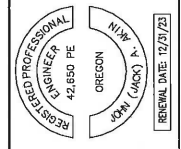
**NOTES**  
 1. CONCRETE TO BE COMMERCIAL GRADE CONCRETE (CGC) PER 2015 ODOT STANDARD SPECIFICATIONS SECTION 0040. MINIMUM 3000 PSI COMPRESSIVE STRENGTH AT 28 DAYS.  
 2. FITTINGS AND PIPE TO BE GASKETED PVC SDR 28.

**5 TYPICAL CLEANOUT**  
 SCALE: NTS

REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 Grants Pass • Medford, OR  
 Phone: 541-774-9434 • Cell: 541-261-9929 • Fax: 541-727-5488  
 email: emc@emcengineers.com • http://www.emcengineers.com

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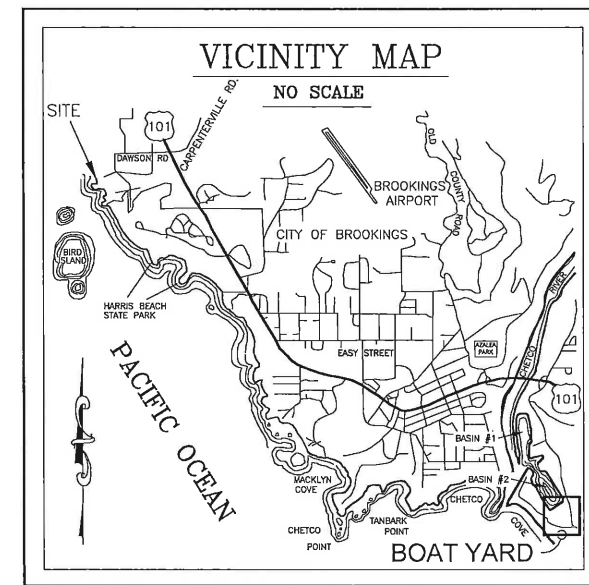
**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:

**C6.1**  
 PROJECT DETAILS



# EROSION & SEDIMENT CONTROL PLANS BOAT YARD PAVING PROJECT PORT OF BROOKINGS - HARBOR



NARRATIVE DESCRIPTIONS	
PROPERTY LOCATION/DESCRIPTION	41S 13W 08A, TAX LOT 1300 WILLAMETTE MERIDIAN, CURRY COUNTY, OREGON
LATITUDE/LONGITUDE	42.043516N, 124.264357W
EXISTING SITE CONDITIONS	GRAVEL STORAGE AND MAINTENANCE FOR BOATS / EQUIPMENT
DEVELOPED CONDITIONS	PAVED STORAGE AND MAINTENANCE FOR BOATS / EQUIPMENT
DISTURBED AREA	2.46± Acres
SITE SOIL DESCRIPTION	AGED COMPACTED 3/4-0 GRAVEL - 2% +/- PERCENT SLOPES
ON-SITE SOILS HAVE A SLIGHT TO MINIMUM EROSION POTENTIAL. EXCESS SOILS GENERATED FROM EXCAVATION, GRADING AND TRENCHING ACTIVITIES FOR EACH PHASE MAY BE TEMPORARILY STORED ON THE SITE WITHIN UNDEVELOPED AREAS. EXCESS SOILS AT THE TIME OF THE FINAL PHASE OR AT ANY TIME PRIOR MAY BE REMOVED AND PLACED OFFSITE.	
RECEIVING WATER BODY	PORT OF BROOKINGS - HARBOR

**SITE INSPECTION INFORMATION**

PERMITEE'S SITE INSPECTOR \_\_\_\_\_  
 COMPANY/AGENCY \_\_\_\_\_  
 PHONE \_\_\_\_\_  
 FAX \_\_\_\_\_  
 E-MAIL \_\_\_\_\_  
 DESCRIPTION OF EXPERIENCE \_\_\_\_\_

**INSPECTION SCHEDULE**

SITE CONDITION	MINIMUM FREQUENCY
ACTIVE PERIOD	DAILY WHEN STORMWATER RUNOFF, INCLUDING RUNOFF FROM SNOW MELT, IS OCCURRING.
PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE BEFORE LEAVING THE SITE.
INACTIVE PERIODS GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS	ONCE EVERY TWO (2) WEEKS.
PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION. (Schedule A.7.d.iii.(1))

HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE EC INSPECTOR. ALL INSPECTIONS MUST BE MADE IN ACCORDANCE WITH DEQ 1200-C PERMIT REQUIREMENTS.

INSPECTION LOGS MUST BE KEPT IN ACCORDANCE WITH DEQ'S 1200-C PERMIT REQUIREMENTS.

CHANGES TO THE APPROVED ESC PLAN MUST BE SUBMITTED TO RVS IN THE FORM OF AN ACTION PLAN.

**RATIONALE STATEMENT**

A COMPREHENSIVE LIST OF AVAILABLE BEST MANAGEMENT PRACTICES (BMP) OPTIONS BASED ON DEQ'S GUIDANCE MANUAL HAS BEEN REVIEWED TO COMPLETE THIS EROSION AND SEDIMENT CONTROL PLAN. SOME OF THE ABOVE LISTED BMP'S WERE NOT CHOSEN BECAUSE THEY WERE DETERMINED TO NOT EFFECTIVELY MANAGE EROSION PREVENTION AND SEDIMENT CONTROL FOR THIS PROJECT BASED ON SPECIFIC SITE CONDITIONS, INCLUDING SOIL CONDITIONS TOPOGRAPHIC CONSTRAINTS, ACCESSIBILITY TO THE SITE, AND OTHER RELATED CONDITIONS. AS THE PROJECT PROGRESSES AND THERE IS A NEED TO REVISE THE ESC PLAN, AN ACTION PLAN WILL BE SUBMITTED.

**GENERAL NOTES**

- Hold a pre-construction meeting of project construction personnel that includes the inspector to discuss erosion and sediment control measures and construction limits. (Schedule A.5.b.i.(3))
- The ESCP must be kept onsite and all erosion and sediment control measures shown on the plan must be installed in such a manner to ensure that sediment or sediment laden water that enters or is likely to enter surface waters or conveyance systems leading to surface water, roadway, or other properties does not occur. (Schedule A.3.a.) and (Schedule B.3.b.)
- The implementation of the ESCP and construction, maintenance, replacement, and upgrading of the erosion and sediment control measures is the responsibility of the permit registrant until all construction is completed and approved by the local development agency and vegetation/landscaping is established. The permit registrant shall be responsible for maintenance after the plans are approved, until the project is completed and the 1200-C permit is terminated. (Schedule A.4.a.) and (Schedule D.3.)
- The permit registrant must be responsible for proper installation and maintenance of all erosion and sediment control measures, in accordance with local, state, or federal regulations. (Schedule A.5.a.) and (Schedule A. 6.a.)
- Erosion and sediment control measures including perimeter sediment control must be in place before vegetation is disturbed and must remain in place and be maintained, repaired, and promptly implemented following procedures established for the duration of construction, including protection for active storm drain inlets and catch basins and appropriate nonstormwater pollution controls. (Schedule A.5.b.i.(2)), (Schedule A.5.b.ii.(7)), (Schedule A.7.d.i.(2)) & (Schedule A.7.f.)
- Begin land clearing, excavation, trenching, cutting or grading and earthwork-surface roughing after installing applicable sediment, erosion prevention and runoff control measures not in the direct path of work. (Schedule A.5.b.ii.(5)(a)), (Schedule A.7.c.i.(1)) and (Schedule A.7.c.ii.(1))
- Apply temporary and/or permanent soil stabilization measures immediately on all disturbed areas as grading progresses and for all roadways including gravel roadways. (Schedule A.5.b.i.(5)(b), Schedule A.5.b.ii.(5)(c) & Schedule A.5.b.ii.(6))
- Wet Weather BMPs: Construction activities must avoid or minimize excavation and creation of bare ground on slopes greater than five (5) percent from October 1 through May 31 each year. (Schedule A.7.a.i.)
- Wet Weather BMPs: Temporary stabilization of the site must be installed at the end of the shift before a holiday or weekend or at the end of each workday if rainfall is forecast in the next 24 hours and each weekend and holiday. (Schedule A.7.a.ii.)
- Identify, mark, and protect (by fencing off or other means) critical riparian areas and vegetation including important trees and associated rooting zones and vegetation areas to be preserved. Identify vegetative buffer zones between the site and sensitive areas (e.g., wetlands), and other areas to be preserved, especially in perimeter areas. Preserve existing vegetation and revegetate open areas when practicable before and after grading or construction. (Schedule A.5.b.i.(1) & (2)) and (Schedule A.7.c.iii.(1))
- Provide permanent erosion prevention measures on all exposed areas to prevent from becoming a source of erosion and remove all temporary control measures, unless local ordinances require otherwise, as areas are stabilized. (Schedule A.5.b.ii.(8)) and (Schedule A.7.c.ii.(2))
- All temporary sediment controls must remain in place until permanent vegetation or other permanent covering of exposed soil is established. Identify the type of vegetative seed mix used. (Schedule A.7.c.ii.(3)) & (Schedule A.7.c.iii.(4))
- Sediment controls must be installed and maintained along the site perimeter on all down gradient sides of the construction site and at all active and operational internal storm drain inlets at all times during construction. (Schedule A.7.d.i.(1) - (2))
- Prior to any land disturbing activities each site must have gravelled, paved, or constructed entrances, exits and parking areas with exit fire wash to reduce the tracking of sediment onto public or private roads. (Schedule A.7.d.iii.(1))
- When trucking saturated soils from the site, either watertight trucks must be used or loads must be drained on-site until dripping has been reduced to minimize spillage on road. (Schedule A.7.d.iii.(3))
- Temporary stabilization or covering of soil stockpiles and protection of stockpile located away from construction activity must occur at the end of each workday or other BMPs, such as diversion of uncontaminated flows and installation of sediment fences around stockpiles, must be implemented to prevent turbid discharges to surface waters. (Schedule A.7.e.i.(1)) & (Schedule A.7.e.ii.(1) - (3)).
- BMPs that will be used to prevent or minimize stormwater from being exposed to pollutants from spills, no discharge of concrete truck wash water, vehicle and equipment cleaning, vehicle and equipment fueling, maintenance, and storage, other cleaning and maintenance activities, and waste handling activities. These pollutants include fuel, hydraulic fluid, and other oils from vehicles and machinery, as well as debris, leftover paints, solvents, and glues from construction operations. (Schedule A.7.e.i.(2))
- Any use of toxic or other hazardous materials must include proper storage, application, and disposal. (Schedule A.7.e.iii.(2))
- Solid Waste and Hazardous Materials Management: Follow project written spill prevention and response procedures, employee training on spill prevention and proper disposal procedures; regular maintenance schedule for vehicles and machinery; and material delivery and storage controls, training and signage, material use, covered storage areas for waste and supplies. (Schedule A.7.e.iii.(3))
- The permittee must properly manage hazardous wastes, used oils, contaminated soils, concrete waste, sanitary waste, liquid waste, or other toxic substances discovered or generated during construction and meet all state and federal regulations and approvals. (Schedule A.7.e.iii.(4))
- The ESCP measures shown on this plan are minimum requirements for anticipated site conditions. During the construction period, these measures must be upgraded as needed to comply with all applicable local, state, and federal erosion and sediment control regulations. Changes to the ESCP must also be submitted in the form of an Action Plan to DEQ or its Agent for approval. (Schedule A.7.f.)
- Significant amounts of sediment, which leaves the site, must be cleaned up within 24 hours and placed back on the site and stabilized or properly disposed. The cause of the sediment release must be found and prevented from causing a recurrence of the discharge within the same 24 hours. Any in-stream clean up of sediment shall be performed according to the Oregon Division of State Lands required time frame. (Schedule A.7.f.i.(1))
- Vacuuming or dry sweeping must be used to clean-up released sediment and must not be intentionally washed into storm sewers, drainage ways, or water bodies. (Schedule A.7.f.i.(2))
- The application rate of fertilizers used to reestablish vegetation must follow manufacturer's recommendations to minimize nutrient releases to surface waters. Time-release fertilizers should be used with care within any waterway riparian zone. (Schedule A.7.f.i.(3))
- Sediment must be removed from behind a Sediment Fence when it has reached a height of 1/3 the height of the fence aboveground and before fence removal. (Schedule A.7.f.ii.(1))
- Sediment must be removed from behind Bio Bogs and other barriers it has reached a height of two (2) inches and before BMP removal. (Schedule A.7.f.ii.(2))
- Removal of trapped sediment in a Sediment Basin or Sediment Trap or Catch Basins must occur when the sediment retention capacity has been reduced by fifty (50)% and at completion of project. (Schedule A.7.f.ii.(3) & (4))
- DEQ must approve of any treatment system and operational plan that may be necessary to treat contaminated construction dewatering or sediment and turbidity in stormwater runoff. (Schedule A.7.f.iii.)
- Should all construction activities cease for thirty days or more, the entire site must be temporarily stabilized using vegetation or a heavy mulch layer, temporary seeding, or other method. (Schedule A.8.a.)
- Should construction activities cease for fifteen (15) days or more on any significant portion of a construction site temporary stabilization is required for that portion of the site with straw, compost, or other tackified covering that prevent soil or wind erosion until work resumes on that portion of the site. (Schedule A.8.b.)
- Daily inspections when rainfall and runoff occurs of the BMPs and discharge outfalls must be the project ESCP Inspector. These inspections and observations must be recorded in a log that is available on site. (Schedule A.6.b.i.) & (Schedule B.1.b.(1))
- BMPs must be inspected before, during, and after significant storm events. (Schedule A.7.f.)
- All ESCP controls and practices must be inspected visually once to ensure that BMPs are in working order prior to the site becoming inactive or in anticipation of site inaccessibility and must be inspected visually once every two (2) weeks during inactive periods greater than seven (7) consecutive calendar days. (Schedule B.1.b.(2)-(3))
- If practical, inspections must occur daily at a relevant and accessible discharge point or downstream location during periods which the site is inaccessible due to inclement weather. (Schedule B.1.b.(4)).

**CONTACTS**

<b>SURVEYOR</b> ROBERTS & ASSOCIATES LAND SURVEYING, INC. 611 SPRUCE STREET BROOKINGS, OR 97415 (541) 469-0162 CONTACT: RICH ROBERTS	<b>OWNER</b> PORT OF BROOKINGS - HARBOR 16330 LOWER HARBOR ROAD BROOKINGS, OR 97415 (541) 469-2218 CONTACT: _____
<b>PROJECT ENGINEER</b> EMC ENGINEERS / SCIENTISTS 450 CONESTOGA DRIVE JACKSONVILLE, OR 97530 (541) 281-9929 CONTACT: JACK AKIN, P.E.	<b>GEOTECHNICAL ENGINEER</b> EMC ENGINEERS / SCIENTISTS 450 CONESTOGA DRIVE JACKSONVILLE, OR 97530 (541) 281-9929 CONTACT: JACK AKIN, P.E.

**SHEET INDEX**

ECP1	COVER SHEET / NOTES - EROSION AND SEDIMENT CONTROL PLAN
ECP2	SITE PLAN - EROSION & SEDIMENT CONTROL PLAN
ECP3	DETAILS - EROSION & SEDIMENT CONTROL PLAN

### BMP MATRIX FOR CONSTRUCTION PHASES

REFER TO DEQ GUIDANCE MANUAL FOR A COMPREHENSIVE LIST OF AVAILABLE BMP'S.

\*X INDICATES THE APPROPRIATENESS OF A BMP TO BE PRESENT DURING THE CORRESPONDING CONSTRUCTION PHASE

	CLEARING	MASS GRADING	N/A	PAVEMENT CONSTRUCTION	FINAL STABILIZATION
TEMPORARY "WET WEATHER" EROSION PREVENTION*					
PRESERVE EXISTING VEGETATION	** X	X		X	X
PLASTIC SHEETING	X	X		X	X
MATTING		X			X
TEMPORARY SEEDING		X			X
PERMANENT EROSION PREVENTION*					
GROUND COVER					X
MATTING					X
PERMANENT SEEDING		X			X
DUST CONTROL	X	X		X	X
SEDIMENT CONTROL					
SEDIMENT FENCE (PERIMETER)	** X	X		X	X
STRAW WATTLES		X			X
INLET PROTECTION	** X	X		X	X
RUN OFF CONTROL					
CONSTRUCTION ENTRANCE	** X	X		X	X
SURFACE ROUGHENING					X
CHECK DAMS					
POLLUTION PREVENTION					
PROPER SIGNAGE	X	X		X	X
HAZ WASTE MGMT	X	X		X	X
SPILL KIT ON-SITE	X	X		X	X
CONCRETE WASHOUT AREA	X	X		X	X

\* ALL BMP'S TO BE REMOVED DURING FINAL STABILIZATION EXCEPT FOR PERMANENT EROSION PREVENTION MEASURES

\*\* SIGNIFIES BMP THAT WILL BE INSTALLED PRIOR TO ANY GROUND DISTURBING ACTIVITY.

REVISIONS

BY:	

Grants Pass • Jacksonville • Medford, OR  
 CPD Office 1847 Williams St. Suite 214 Grants Pass, OR, 97527  
 Phone Office: 541-864-9434 • Cell: 541-261-9929 • Fax: 541-727-5488  
 emc@emcengineersandscientists.com • http://www.emcengineersandscientists.com

**EMC**  
 Engineers/Scientists, LLC



REGISTERED PROFESSIONAL ENGINEER  
 42,650 PE  
 OREGON  
 JACK AKIN (JACK)  
 EXPIRES DATE: 12/31/25

**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:  
**ECP1**  
 EROSION CONTROL PLAN

NOTE:  
 ALL EXISTING UTILITIES TO BE PROTECTED DURING CONSTRUCTION



**GENERAL EROSION CONTROL NOTES**

- Seed used for temporary or permanent erosion control seeding may be any seed mix that is generally used by local landscape professionals or may consist of the following:
  - Dwarf grass mix (min. 100 lb./ac.)
    - Dwarf perennial ryegrass (80% by weight)
    - Creeping red fescue (20% by weight)
  - Standard height grass mix (min. 100lb./ac.)
    - Annual ryegrass (40% by weight)
    - Turf-type fescue (60% by weight)
 Permanent seeding that is part of site landscaping shall conform to the specifications of the project landscape architect.
- Slope to receive temporary or permanent seeding shall have the surface roughened by means of track-walking or the use of other approved implements. Surface roughening improves seed bedding and reduces run-off velocity.
- Long term slope stabilization measures shall include the establishment of permanent vegetative cover via seeding with approved mix and application rate.
- Temporary slope stabilization measures shall include: covering exposed soil with plastic sheeting, erosion matting, straw ground cover, wood chips, mulch, or other approved measures. Sheet piling or matting shall be weighted or staked down to prevent movement due to wind or water. Straw or other materials (as applicable) applied to slopes greater than 3:1 shall be track-walked or held in place by a tackifier.
- Stockpiled soil or strippings shall be placed in a stable location and configuration. During "wet weather" periods, stockpiles shall be covered with plastic sheeting or straw mulch. Sediment fence is required around the perimeter of the stockpile unless the stockpile is located in a flat (<2% slope) or depressed area with about 20 feet of grassy or vegetated buffer zone on all sides.
- Exposed cut or fill areas shall be stabilized through the use of temporary seeding and mulching, erosion control blankets or mats, mid-slope sediment fences or wattles, or other appropriate measures. Slopes exceeding 25% may require additional erosion control measures.

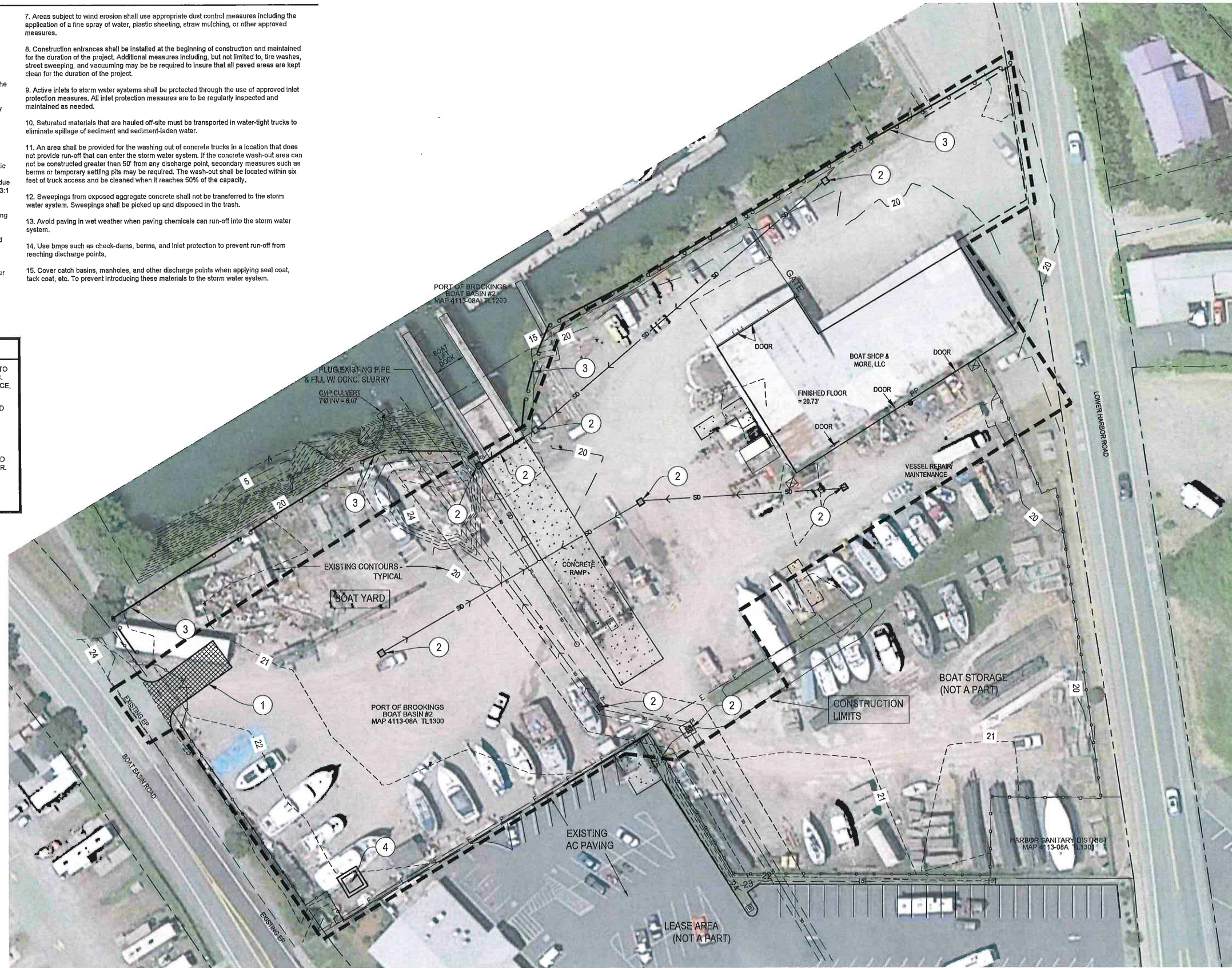
- Areas subject to wind erosion shall use appropriate dust control measures including the application of a fine spray of water, plastic sheeting, straw mulching, or other approved measures.
- Construction entrances shall be installed at the beginning of construction and maintained for the duration of the project. Additional measures including, but not limited to, tire washes, street sweeping, and vacuuming may be required to insure that all paved areas are kept clean for the duration of the project.
- Active inlets to storm water systems shall be protected through the use of approved inlet protection measures. All inlet protection measures are to be regularly inspected and maintained as needed.
- Saturated materials that are hauled off-site must be transported in water-tight trucks to eliminate spillage of sediment and sediment-laden water.
- An area shall be provided for the washing out of concrete trucks in a location that does not provide run-off that can enter the storm water system. If the concrete wash-out area can not be constructed greater than 50' from any discharge point, secondary measures such as berms or temporary settling pits may be required. The wash-out shall be located within six feet of truck access and be cleaned when it reaches 50% of the capacity.
- Sweepings from exposed aggregate concrete shall not be transferred to the storm water system. Sweepings shall be picked up and disposed in the trash.
- Avoid paving in wet weather when paving chemicals can run-off into the storm water system.
- Use bmps such as check-dams, berms, and inlet protection to prevent run-off from reaching discharge points.
- Catch basins, manholes, and other discharge points when applying seal coat, tack coat, etc. To prevent introducing these materials to the storm water system.

**CONSTRUCTION NOTES**

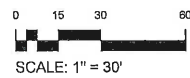
- ROCK ENTRANCE FOR 50' MIN. TO PREVENT TRACKING OF SOIL ONTO ROADWAY, 8" MIN. THICKNESS OF SHALE OR 4"-0 ON FILTER FABRIC. LINE AND GRADE SHALL CONFORM TO FUTURE DRIVEWAY ENTRANCE, SEE RD 1000, SHEET ECP3.
- FUTURE AND EXISTING STORM DRAIN INLETS SHALL BE PROTECTED BY BIOFILTER BAGS OR EQUIVALENT, SEE RD 1015, SHEET ECP3.
- CONSTRUCT SEDIMENT FENCE, SEE DETAIL RD 1040, SHEET ECP3. ASSURE CLEARANCE OUTSIDE OF PROPOSED AC EDGE.
- CONSTRUCT CONCRETE TRUCK WASHOUT FACILITY. SEE DETAIL RD 1070, SHEET ECP3. MINIMUM 10' X 10' OR AS DIRECTED BY ENGINEER.

**NOTES:**

- SEE BMP MATRIX ON SHEET ECP1 FOR TIMING OF EROSION CONTROL MEASURES & ADDITIONAL EROSION CONTROL CONSIDERATIONS
- SEE SHEET C6.1 FOR ADDITIONAL BMP DETAILS



**VERTICAL DATUM**  
 MEAN LOWER LOW WATER EPOCH 1983-2001.  
 BENCHMARK UTILIZED FOR THIS SURVEY  
 US ARMY CORPS OF ENGINEERS  
 BENCHMARK - "FUEL 2"  
 ELEVATION - 21.65 FEET



**1 EROSION CONTROL PLAN**  
 SCALE: 1" = 30' (24x36)

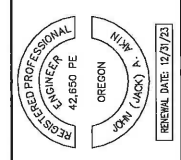


NOTE:  
 ALL EXISTING UG UTILITIES TO BE PROTECTED DURING CONSTRUCTION



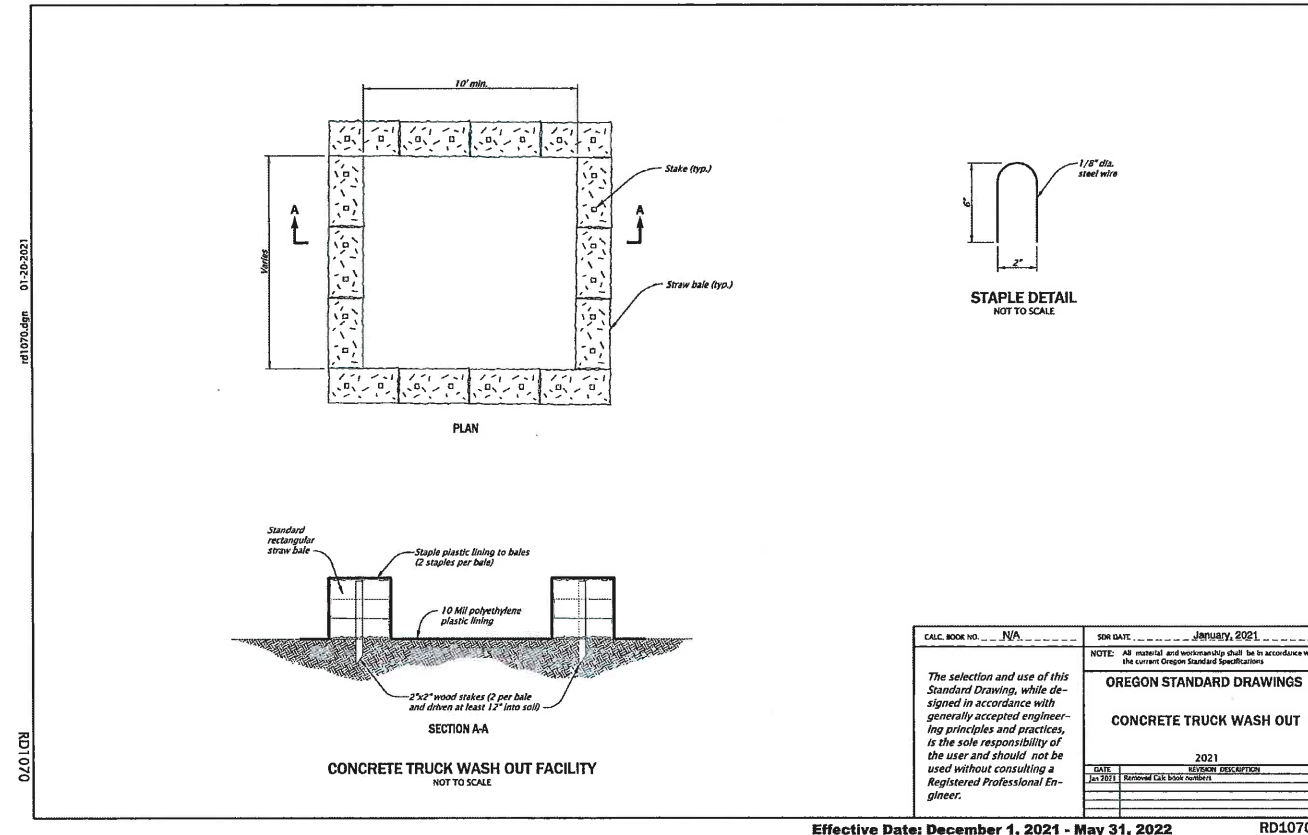
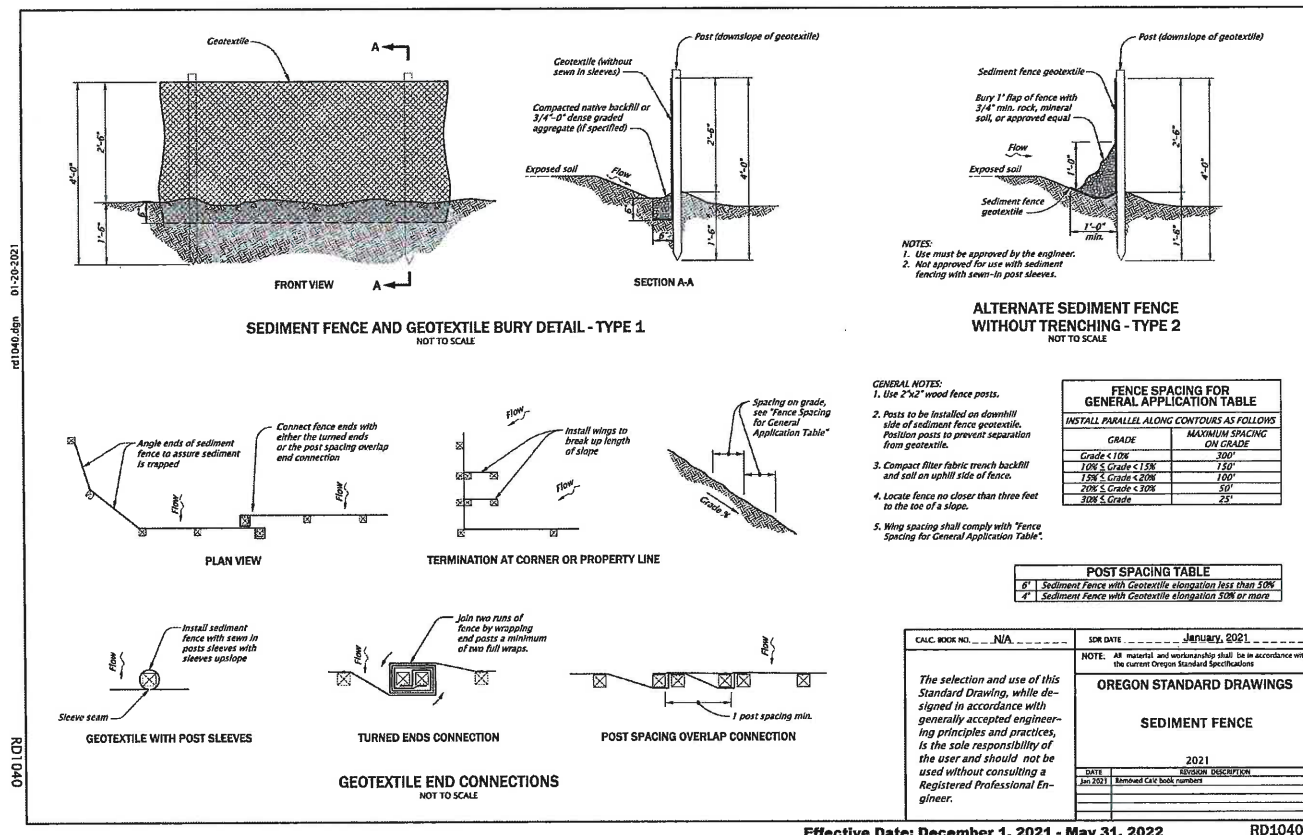
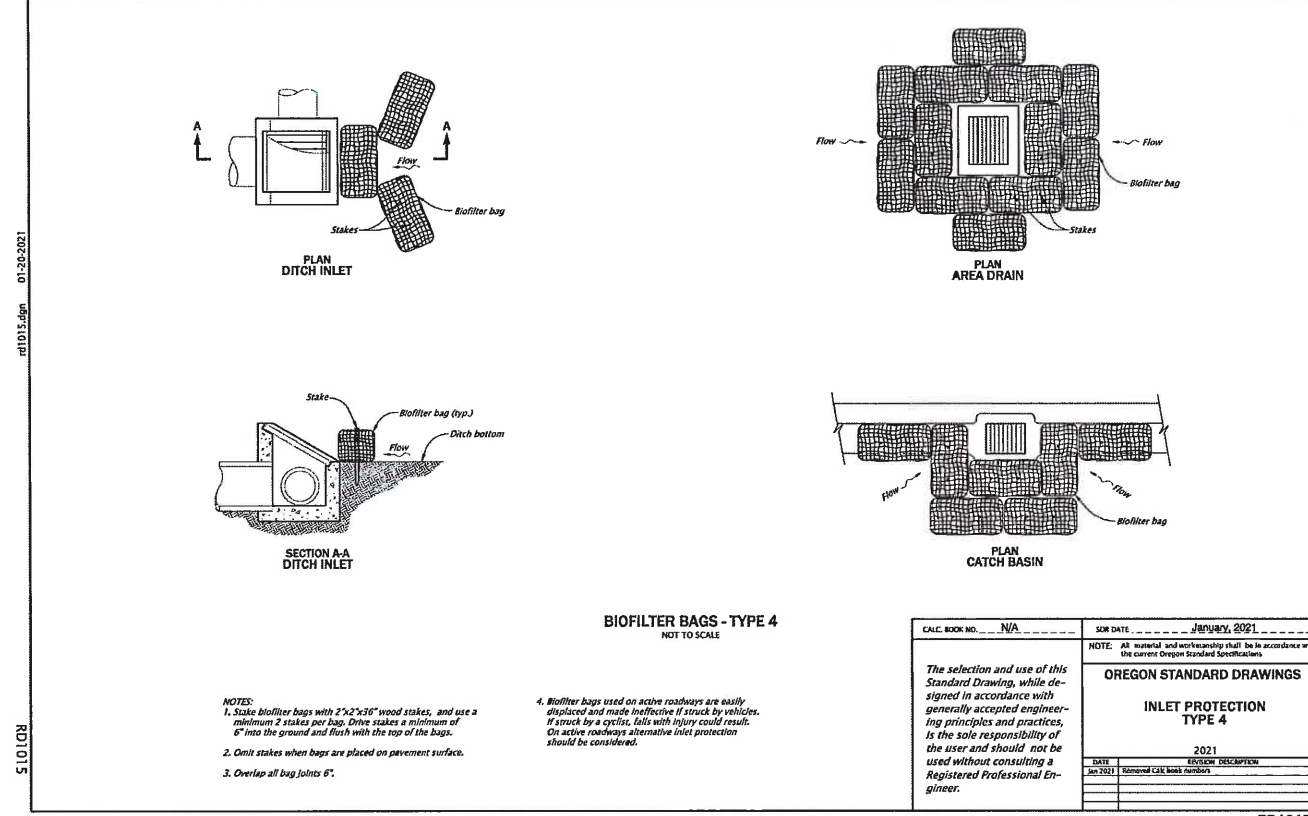
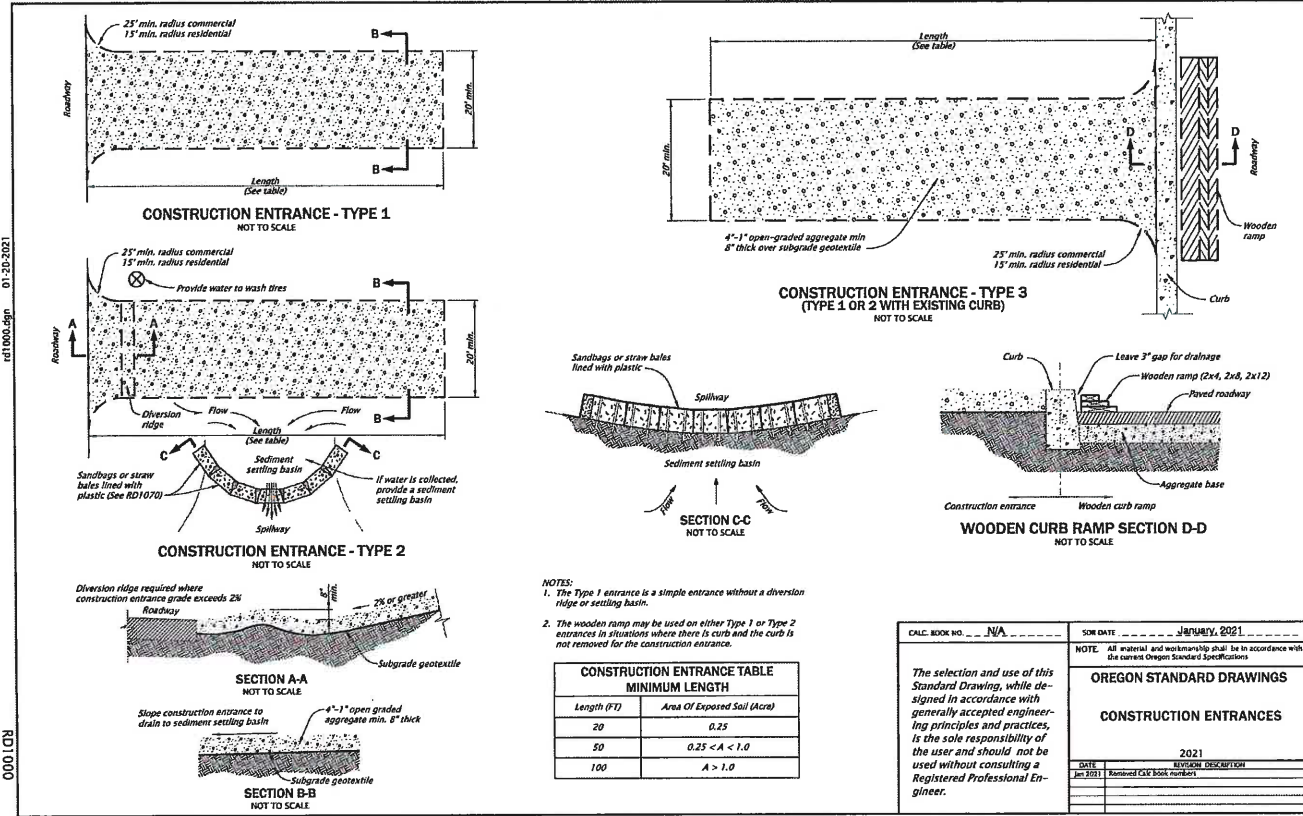
REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 1000 Columbia Ave., Suite 2100 Grants Pass, OR 97527  
 Phone: 541-774-9434 • Fax: 541-727-5488  
[www.emcengineers.com](http://www.emcengineers.com)  
**Engineers/Scientists, LLC**



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**BOAT YARD PAVING**

DRAWN BY: CD/JW  
 DATE: 15 JULY 2022  
 JOB No: 22-202201  
 SHEET No:  
**ECP2**  
 EROSION CONTROL PLAN

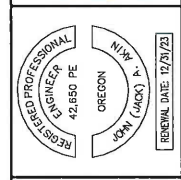


NO.	DATE	DESCRIPTION

BY: \_\_\_\_\_

Grants Pass • Jacksonville • Medford, OR  
 21000 NE Williams Hwy., Suite 210, Grants Pass, OR 97527  
 PH: 541-747-9434 • Cell: 541-261-9929 • Fax: 541-727-5488  
 emc@emcengineerscientists.com • http://www.emcengineerscientists.com

EMC  
 EngineersScientists, LLC



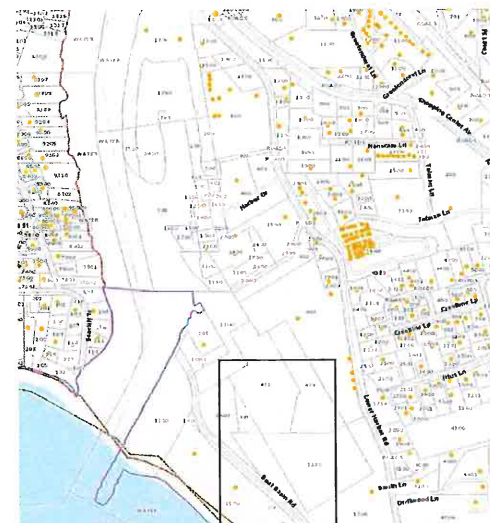
PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 BOAT YARD PAVING

DRAWN BY: CD/JW  
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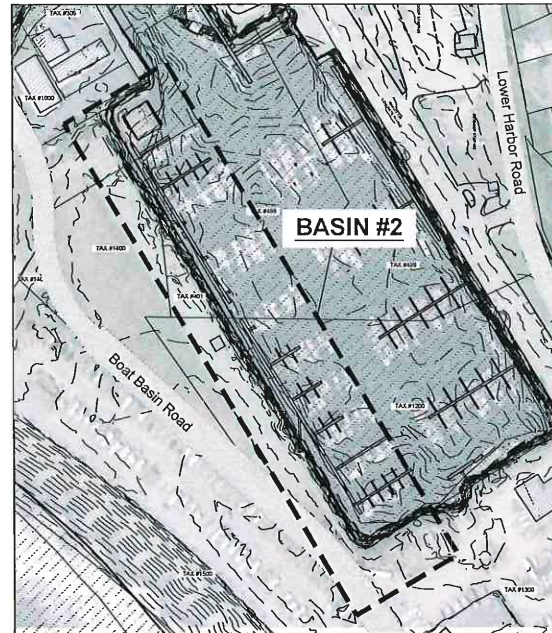
**ECP3**  
 EROSION CONTROL DETAILS



# PORT OF BROOKINGS - HARBOR EMBANKMENT ROCK RECONSTRUCTION



RECONSTRUCTION AREA  
PORT OF BROOKINGS HARBOR  
MAP OF TAX LOTS



PROJECT OVERVIEW

## PROJECT DESCRIPTION

TITLE: EMBANKMENT ROCK RECONSTRUCTION  
 REFERENCE: 113  
 LOCATION: BASIN BOAT ROAD  
 TAX LOT(S): 1400, 1300, 401

## PROJECT NOTES

**NATURAL FEATURES**  
 EXISTING NATURAL RESOURCES OR NATURAL HAZARDS ON THE SUBJECT PROPERTY, INCLUDING WETLANDS, STREAMS, RIPARIAN AREAS, FLOOD PLAINS, OR FLOODWAYS TO BE DETERMINED BY ENGINEER

**EXISTING TREE CANOPY**  
 THERE ARE NO EXISTING TREES ON THE SUBJECT PROPERTY

**CULTURAL RESOURCES**  
 LOCALLY, OR FEDERALLY DESIGNATED HISTORIC AND/OR CULTURAL RESOURCES ON THE SITE OR ON ADJACENT PARCELS TO BE DETERMINED BY ENGINEER.

**PUBLIC SERVICES**  
 PUBLIC UTILITY SERVICES, INCLUDING WATER, SEWER, STORM DRAINAGE, POWER, TELEPHONE, CABLE INTERNET, AND GAS ARE AVAILABLE TO THE SUBJECT PROPERTY.

**UTILITY STATEMENT**  
 EXISTING UNDERGROUND UTILITIES ILLUSTRATED IN THESE PLANS ARE APPROXIMATED BASED ON MAPS OBTAINED FROM CURRY COUNTY GIS ELEVATIONS ESTIMATES, OR HAVE BEEN LOCATED BY A UTILITY LOCATE COMPANY. LAYOUT INDICATED IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. ALL LINES WITHIN PROJECTED WORK ZONE SHALL BE FIELD VERIFIED AS REQUIRED PRIOR TO CONSTRUCTION.

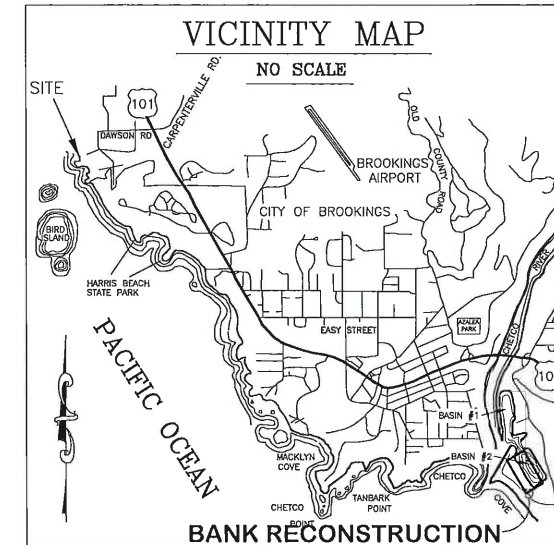
## GENERAL GRADING NOTES

- DEQ 1200-C PERMIT IS REQUIRED.
- UNLESS DIRECTED OTHERWISE, REMOVE CLEARED AND GRUBBED MATERIAL FROM THE SITE AND DISPOSE AT AN APPROVED LOCATION.
- PRIOR TO THE START OF CONSTRUCTION, VERIFY GRADES AT SAWCUT LOCATIONS AND MATCHING OF EXISTING GRADE LOCATIONS.
- MINIMIZE TRAFFIC ON SOIL AREAS DURING WET WEATHER. IF THE SITE SOILS ARE EXPOSED DURING WET WEATHER, THE USE OF CRUSHED ROCK PLACED AS ENGINEERED FILL IN THE BOTTOM OF THE EXCAVATIONS MAY BE NECESSARY TO PROTECT THE SUBGRADE. TAKE ALL PRECAUTIONS TO LIMIT SURFACE DISTURBANCE AND PROTECT THE SITE GRADING AREA FROM EROSION AND RUNOFF.
- UNLESS OTHERWISE NOTED, THE SAMPLING AND TESTING OF MATERIALS FOR USE ON THE JOBSITE SHALL BE AT THE EXPENSE OF THE CONTRACTOR. ALL TESTING OF MATERIALS AND WORKMANSHIP SHALL BE PERFORMED BY A CERTIFIED TESTER. RESULTS OF THE TESTS SHALL BE SENT DIRECTLY TO THE PROJECT ENGINEER AS WELL AS THE CONTRACTOR, BY THE LABORATORY. LOCATION AND FREQUENCY OF TESTS SHALL BE DESIGNATED BY THE GENERAL CONTRACTOR.
- ALL CUT AND FILL SLOPES SHALL BE MAXIMUM OF 2:1.

	AGENCY	APPROVED BY	DATE
POWER	PACIFIC POWER		
DOMESTIC WATER	CITY OF BROOKINGS		
SANITARY SEWER	HARBOR SANITARY		
STORM DRAINAGE	CURRY COUNTY		
STREETS	CITY OF BROOKINGS		
ENGINEERING	PORT OF BROOKINGS HARBOR		

CONSTRUCTION AUTHORIZED TO PROCEED IN ACCORDANCE WITH APPROVED PLANS WHEN ALL PERMITS HAVE BEEN ISSUED AND PRE-CONSTRUCTION MEETING HAS CONCLUDED.

BY \_\_\_\_\_ TITLE \_\_\_\_\_ DATE \_\_\_\_\_



## INSPECTION AND TESTING NOTES

- CONTRACTOR IS RESPONSIBLE FOR SCHEDULING ALL TESTING, INSPECTIONS, AND SPECIAL INSPECTIONS, AND SPECIAL INSPECTIONS AS REQUIRED BY PROJECT ENGINEER, CURRENT BUILDING CODES, OR JURISDICTIONS HAVING AUTHORITY. ALL TESTING MUST BE COMPLETED AND APPROVED PRIOR TO SUBSEQUENT WORK. ADDITIONAL OR FREQUENT TESTS MAY BE REQUIRED BY AGENCY, BUILDING OFFICIAL OR ENGINEER.
- TESTING MUST BE PERFORMED BY AN APPROVED INDEPENDENT TESTING LABORATORY RETAINED BY THE CONTRACTOR.
- IN ADDITION TO IN-PLACE DENSITY TESTING, THE SUB-GRADE AND BASE ROCK SHALL BE PROOF-ROLLED WITH A LOADED DUMP TRUCK OR HEAVY NON-VIBRATORY ROLLER. SOILS SHALL BE REMOVED AND RE-COMPACTED OR REPLACED WITH APPROVED IMPORTED STRUCTURAL FILL IF THEY DO NOT DEMONSTRATE A FIRM, UNYIELDING CONDITION. BASE ROCK PROOF-ROLL SHALL TAKE PLACE LESS THAN 24 HOURS PRIOR TO PAVING AND SHALL BE WITNESSED BY THE ENGINEER OR GOVERNING AGENCY.
- THE APPROVED INDEPENDENT LABORATORY SHALL PROVIDE CLARIFICATION STAMPED BY AN ENGINEER LICENSED IN THE STATE OF OREGON THAT THE SUB-GRADE IS PREPARED AND ALL ENGINEERED FILLS ARE PLACED IN ACCORDANCE WITH THE CONTRACT DRAWINGS AND DOCUMENTS.
- PROVIDE ENGINEER WITH SPOT ELEVATION VERIFICATION FOR SUB-GRADE AND TOP OF AGGREGATE PRIOR TO PLACING CONCRETE, ASPHALT, AND/OR OTHER STRUCTURES (WHEN INCLUDED IN THE PROJECT).

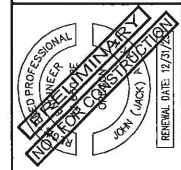
PROJECT ENGINEER  
 EMC ENGINEERS / SCIENTISTS  
 450 CONESTOGA DRIVE  
 JACKSONVILLE, OR 97530  
 (541) 261-9929  
 CONTACT: JACK AKIN, P.E.

## LEGEND

	EXIST. FIRE HYDRANT		FIRE HYDRANT
	EXIST. WATER VALVE		WATER VALVE
	EXIST. BLOW OFF		WATER METER
	EXIST. WATER METER		BACKFLOW DEVICE
	EXIST. HOSE BIB		IRRIGATION WATER METER
	EXIST. IRRIGATION VALVE		AIR RELEASE VALVE
	EXIST. AIR RELEASE VALVE		BLOWOFF DEVICE ASSEMBLY
	EXIST. STORM DRAIN MANHOLE		FIRE DEPARTMENT CONNECTION
	EXIST. CLEANOUT		END PLUG
	EXIST. SANITARY SEWER MANHOLE		TEE
	EXIST. MAILBOX		SANITARY SEWER CLEANOUT
	CURB RAMP		SANITARY SEWER MANHOLE
	PARALLEL RAMP		STORM DRAIN CLEANOUT
	HANDICAP PARKING SYMBOL		STORM DRAIN MANHOLE
	PARALLEL PARKING STRIPING		ATRIUM DRAIN / BUBBLER
	BICYCLE LANE SYMBOL		CATCH BASIN INLET
	EXIST. SANITARY SEWER		CURB INLET
	EXIST. STORM DRAIN		STORM DRAIN CONTROL STRUCTURE
	EXIST. WATER		ELECTRIC BOX
	EXIST. GAS		ELECTRIC TRANSFORMER
	EXIST. ELECTRIC		LIGHTS
	EXIST. OVERHEAD POWER		CONTROL POINT
	EXIST. TELEPHONE		MONUMENT
	EXIST. FIBER OPTIC		TRAFFIC SIGNAL LIGHT
	EXIST. CURB AND GUTTER		SIGN (TRAFFIC, INFORMATION)
	EXIST. CENTERLINE		BOLLARD
	EXIST. RIGHT OF WAY		BICYCLE PARKING SPACE
	EXIST. CONTOUR		DECIDUOUS TREE
	PROPOSED CONTOUR		CONIFER TREE
	EXIST. EDGE OF PAVEMENT		NEW CONCRETE
	CHAIN LINK FENCE		NEW GRAVEL PAVING
	BARB WIRE FRANC		NEW HMAC PAVING - STANDARD
	SANITARY SEWER		NEW HMAC PAVING - HEAVY
	STORM DRAIN		NEW LANDSCAPING
	WATER		NEW RIP RAP
	GAS		
	ELECTRIC		
	CURB AND GUTTER		
	PROPOSED RIGHT OF WAY		
	FLOW LINE		
	PROPERTY LINE		

BY:	
REVISIONS	

Grants Pass • Jacksonville • Medford, OR  
 CP Office: 1867 William Hwy., Suite 216, Grants Pass, OR 97527  
 PH: 541-774-8484 • Cell: 541-261-9929 • Fax: 541-777-5488  
 emc@emcengineerscientists.com • http://www.emcengineerscientists.com  
 -Engineers/Scientists, LLC



PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 EMBANKMENT ROCK RECONSTRUCTION

DRAWN BY: CD/JW  
 DATE: 4 MAY 2022  
 JOB No: 22-202203  
 SHEET No:  
**C1.0**  
 COVER SHEET



## GENERAL NOTES

THESE NOTES ARE INTENDED FOR USE IN INTERPRETING AND IMPLEMENTING THE TASKS SHOWN ON THE FOLLOWING CONSTRUCTION PLANS AND SPECIFICATIONS SHEETS. THESE ARE IN ADDITION TO THE OVERALL PROJECT SPECIFICATION AND BID DOCUMENTS AND CONTRACTUAL ITEMS.

- THESE ARE INTENDED FOR THE USE OF THE GENERAL CONTRACTOR AND HIS/HER SUBCONTRACTORS IN THE DEMOLITION AND RECONSTRUCTION OF THE SUBJECT AREA OF THE PORT OF BROOKINGS HARBOR. USE FOR OTHER PURPOSES OR AT OTHER SITES IS NOT RECOMMENDED AND IS ACCOMPLISHED AT THE SOLE RISK OF THE USER.
- THESE ITEMS ARE TO BE USED AS A SUPPLEMENT TO THE DETAILS PROVIDED ON THE PLAN SHEETS AND SPECIFICATION PAGES. ANY DISCREPANCIES FOUND AMONG THE DRAWINGS, THE SPECIFICATIONS, REFERENCED REPORTS, THESE GENERAL NOTES AND OTHER ITEMS LISTED ON THIS SHEET AND THE SITE CONDITIONS SHALL BE REPORTED TO THE ENGINEER, WHO SHALL CORRECT SUCH DISCREPANCY IN WRITING. ANY WORK DONE BY THE GENERAL CONTRACTOR AFTER DISCOVERY OF SUCH DISCREPANCY SHALL BE DONE AT THE GENERAL CONTRACTOR'S RISK. THE GENERAL CONTRACTOR SHALL VERIFY AND COORDINATE DIMENSIONS AMONG ALL DRAWINGS PRIOR TO PROCEEDING WITH ANY WORK.
- THE EMBANKMENT REPAIR HAS BEEN DESIGNED TO RESIST ANTICIPATED VERTICAL AND LATERAL FORCES AFTER THE CONSTRUCTION OF ALL STRUCTURAL ELEMENTS HAS BEEN COMPLETED. STABILITY OF THE STRUCTURE AND SLOPE AREAS PRIOR TO COMPLETION IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR. THIS RESPONSIBILITY INCLUDES, BUT IS NOT LIMITED TO JOBSITE SAFETY, CONSTRUCTION MEANS, METHODS, AND SEQUENCES, TEMPORARY SHORING, SLOPE STABILITY, FORMWORK AND BRACING, USE OF EQUIPMENT AND CONSTRUCTION PROCEDURES.
- CONSTRUCTION OBSERVATION BY THE ENGINEER IS FOR CHECKING FOR CONFORMANCE WITH DESIGN ASPECTS ONLY AND IS NOT INTENDED IN ANY WAY TO REVIEW AND/OR APPROVE THE GENERAL CONTRACTOR'S CONSTRUCTION PROCEDURES OR RELIEVE THE CONTRACTOR FROM PROVIDING A COMPLETED PROJECT, CONSISTENT WITH THE PLANS AND SPECIFICATIONS AND GOOD CONSTRUCTION PRACTICES. SPECIAL INSPECTION BY THE ENGINEER DOES NOT PROVIDE A CERTIFICATION OF THE PROJECT OR RELIEVE THE CONTRACTOR OF ALL RESPONSIBILITY FOR A PROPERLY CONSTRUCTED PROJECT.

### STANDARDS USED FOR DESIGN

- ALL METHODS, MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE PLANS AND SPECIFICATIONS AND ODOT STANDARD SPECIFICATIONS, UNLESS ELSEWHERE HEREIN SPECIFIED OTHERWISE. INTERNATIONAL BUILDING CODE (IBC) 2021; 2019 OREGON STRUCTURE SPECIALTY CODE (OSSC); AMERICAN SOCIETY OF CIVIL ENGINEERS (ASCE)

### OTHER NOTES

- JOBSITE SAFETY IS THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL PRODUCTS AND WORKMANSHIP SHALL BE NEW MATERIALS OF GOOD QUALITY, ACCEPTABLE FOR THIS TYPE OF CONSTRUCTION. WORK TO BE ACCOMPLISHED IN A GOOD AND WORKMANLIKE MANNER.
- ALL MATERIALS TO BE SHIPPED, HANDLED AND STOCKPILED IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS AND GOOD CONSTRUCTION PRACTICES.
- LOCATIONS MUST BE VERIFIED AT THE SITE WITH THE ENGINEER AND THE PORT OF BROOKINGS HARBOR REPRESENTATIVE PRIOR TO PLACEMENT.
- ABIDE BY LOCAL, STATE AND FEDERAL BUILDING ORDINANCES, INCLUDING ALL SAFETY REQUIREMENTS, IN ALL PHASES OF THE PROJECT.
- ALL PHASES OF THE PROJECT ARE TO CONFORM TO THE PLANS AND SPECIFICATIONS ATTACHED HERETO AND SPECIFICATIONS PROVIDED BY THE OWNER AND THE ENGINEER-OF-RECORD.
- PROPOSED CHANGES TO PROJECT PLANS AND SPECIFICATIONS MUST BE APPROVED BY THE DESIGNER PRIOR TO ACCEPTANCE AND IMPLEMENTATION AT THE SITE.
- PROPOSED CHANGES MUST BE SUBMITTED IN WRITING FOR REVIEW AND APPROVAL/DISAPPROVAL BY THE DESIGNER AND THE OWNER.
- IN NO CASE SHALL CHANGES, SUBSTITUTIONS OR OMISSIONS BE MADE TO THE DESIGN OR MATERIALS WITHOUT THE WRITTEN AUTHORIZATION OF THE DESIGNER AND THE OWNER.
- AUTHORIZATION OF A DESIGN CHANGE BY THE ENGINEER DOES NOT CONSTITUTE ACCEPTANCE BY THE PORT OF BROOKINGS HARBOR, NOR DOES IT AUTHORIZE ADDITIONAL FUNDS FOR THE CHANGES. THE PORT'S REPRESENTATIVE MUST AUTHORIZE IN WRITING THE DESIGN CHANGE AND APPLICABLE CHANGES IN THE CONTRACT AMOUNT AND/OR CONSTRUCTION TIME PERIOD PRIOR TO IMPLEMENTATION OF SUCH CHANGES.
- PROJECT SCHEDULE AND GENERAL SEQUENCING OF ALL WORK MUST BE REVIEWED AND APPROVED BY THE DESIGN ENGINEER AND THE OWNER. SUCH APPROVAL DOES NOT RELIEVE THE CONTRACTOR OR HIS/HER SUBCONTRACTORS OF ALL RESPONSIBILITIES FOR PROPER EXECUTION OF THE SUBJECT PROJECT CONSTRUCTION.
- SEQUENCING OF TASKS THAT REQUIRES VARYING THE INSTALLED SIZES OF PROJECT MATERIALS MUST BE REVIEWED AND APPROVED BY THE DESIGN ENGINEER AND OWNER.
- TRAFFIC CONTROL AND SIGNAGE MUST BE PROVIDED BY THE CONTRACTOR UNLESS OTHERWISE SO STATED IN THE CONTRACT. ACCESS TO THE US COAST GUARD FACILITIES MUST BE MAINTAINED DURING CONSTRUCTION.
- CONTRACTOR MUST UNDERSTAND THAT THE PROJECT SITE IS IN A HARBOR AREA SUBJECT TO TIDAL FLUCTUATIONS. THEREFORE, SEQUENCING AND PROJECT WORK MUST TAKE INTO ACCOUNT EFFECTS OF HIGH AND LOW TIDES.
- REPLACEMENT OF SPECIFIED PRODUCTS BY AN "EQUIVALENT" PRODUCT MUST BE APPROVED BY THE DESIGN ENGINEER AND THE OWNER. REDESIGN REQUIRED FOR USE OF ALTERNATE "EQUIVALENT" MATERIALS IS TO BE BORNE BY THE CONTRACTOR.

## GRADING NOTES

PRIOR TO THE CONSTRUCTION OF EMBANKMENTS, THE CONTRACTOR SHALL EXCAVATE UNSUITABLE FOUNDATION MATERIAL. BASEMENTS, TRENCHES AND HOLES ENCOUNTERED WITHIN EMBANKMENT LIMITS SHALL BE FILLED WITH APPROVED MATERIAL. PRIOR TO BACKFILLING THE CONTRACTOR SHALL BREAK CONCRETE FLOORS OF BASEMENTS AS DIRECTED. THE CONTRACTOR SHALL BREAK UP AND ROUGHEN THE GROUND SURFACE BEFORE EMBANKMENTS MATERIAL IS PLACED. THE NATURAL GROUND UNDERLYING EMBANKMENTS SHALL BE COMPACTED TO THE DENSITY SPECIFIED FOR THE EMBANKMENT MATERIALS TO BE PLACED.

- EMBANKMENT CONSTRUCTION SHALL INCLUDE PREPARATION OF THE AREAS UPON WHICH EMBANKMENTS ARE PLACED, THE PLACEMENT AND COMPACTION OF APPROVED EMBANKMENT MATERIALS AND FILLING OF HOLES, PITS AND OTHER DEPRESSIONS WITHIN THE SUBDIVISION.
- THE CONTRACTOR SHALL PLACE EMBANKMENTS AND FILLS IN THE HORIZONTAL LAYERS AS SPECIFIED AND COMPACT EACH LAYER TO THE DENSITY SPECIFIED.
- EMBANKMENT SHALL NOT BE CONSTRUCTED WHEN THE EMBANKMENT MATERIAL OR THE FOUNDATION ON WHICH THE EMBANKMENT WOULD BE PLACED IS FROZEN.
- IMMEDIATELY PRIOR TO COMPLETION OF THE EARTHWORK, THE CONTRACTOR SHALL CLEAN THE ENTIRE WORK AREA OF DEBRIS AND FOREIGN MATTER.
- THE MAXIMUM DENSITY OF COMPACTED MATERIAL WILL BE DETERMINED BY AASHTO T-99
- THE CONTRACTOR SHALL COMPACT ALL EMBANKMENTS, FILLS AND BACKFILLS TO A MINIMUM IN PLACE DENSITY OF 95 PERCENT.
- THE CONTRACTOR SHALL WATER THE MATERIALS TO PROVIDE OPTIMUM MOISTURE FOR COMPACTION OF EMBANKMENT AND BACKFILLS. EMBANKMENTS OF BACKFILL MATERIALS SHALL NOT BE PLACED IN FINAL POSITION UNTIL MOISTURE IN EXCESS OF OPTIMUM MOISTURE HAS BEEN REMOVED.
- IF THE SPECIFIED COMPACTION IS NOT OBTAINED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER. THE CONTRACTOR MAY BE REQUIRED TO USE A MODIFIED COMPACTION PROCEDURE OR APPLY ADDITIONAL COMPACTION EFFORT. IF APPROVED MATERIALS MEETING THE SPECIFICATIONS CANNOT BE COMPACTED TO THE REQUIRED DENSITY REGARDLESS OF COMPACTION EFFORT OR METHOD, THE ENGINEER MAY REDUCE THE REQUIRED DENSITY OR DIRECT THE USE OF ALTERNATE MATERIALS. IN NO CASE SHALL EARTHWORK OPERATIONS PROCEED UNTIL THE CONTRACTOR IS ABLE TO COMPACT THE MATERIAL TO THE SATISFACTION OF THE ENGINEER.

## EROSION, SEDIMENTATION & POLLUTION CONTROL PLAN NOTES

- PROJECT/PURPOSE - WITH THE PURPOSE OF MAINTAINING, REHABILITATING, REPLACING, AND UPGRADING THE EXISTING WESTERN EMBANKMENT AT THE SOUTH BASIN OF PORT OF BROOKINGS. THE EXISTING EMBANKMENT WILL BE CLEARED DOWN TO SUITABLE SUBGRADE AT A SLOPE BETWEEN 1.5:1 AND 2:1. A 4 FT DEEP TRENCH WILL BE EXCAVATED AT THE BASE OF THE EMBANKMENT. A 3 FT DEEP LAYER OF AGGREGATE WILL BE PLACED ON TOP OF THE SLOPE AND INTO THE TRENCH
- CONTRACTOR ACTIVITIES - CONTRACTOR ACTIVITIES ARE DESCRIBED AS EXCAVATION AND CLEARING USING A 25T EXCAVATOR. EXCAVATION AND SPREADING AT THE BASE OF THE SLOPE USING A LONG REACH EXCAVATOR. PLACEMENT OF GEOGRID. PLACEMENT OF CRUSHED AGGREGATE FROM FINE GRAVEL TO 2 FT BOULDERS
- SOIL DISTURBING ACTIVITIES - EXCAVATION WILL BE LIMITED TO EXISTING MARINA EDGES
- NON-STORMWATER DISCHARGES - NO DEWATERING, WATER-LINE FLUSHING, PAVEMENT WASH WATERS OR IRRIGATION WATER DISCHARGES ARE PLANNED FOR THIS PROJECT.
- ESTIMATED START DATE FOR CONSTRUCTION - 05/22
- NEAREST SURFACE WATER BODIES - PORT OF BROOKINGS ICE HOUSE INLET IN BASIN 2 (SOUTH BASIN) AND BASIN 1, NEAR DOCK A (NORTH BASIN).
- RECEIVING WATERS - PACIFIC OCEAN
- SPECIAL ENVIRONMENTAL CONSIDERATIONS - SEE SECTION 10
- DESIGNATED EPCM - THE DESIGNATED EROSION AND POLLUTION CONTROL MANAGER (EPCM) WHO WILL ASSURE COMPLIANCE WITH ALL ITEMS IN THIS PLAN IS **TED FITZGERALD**, PORT DIRECTOR, OR HIS DESIGNEE.
- EROSION, SEDIMENTATION AND POLLUTION CONTROL BMPS - BEST MANAGEMENT PRACTICES (BMP) TO BE USED, WHEN APPLICABLE, TO PREVENT POLLUTION RELATED TO CONTRACTOR ACTIVITIES LISTED IN THIS SECTION ARE AS FOLLOWS: A) OFFSITE VEHICLE TRACKING AND DUST PREVENTION - MEASURES WILL BE TAKEN TO PREVENT OFFSITE TRACKING OF MATERIALS, INCLUDING SWEEPING PAVEMENTS, COVERING LOADS AND WETTING SOIL TO PREVENT DUST. THERE WILL BE NO AGGREGATE CONSTRUCTION. B) MATERIAL MANAGEMENT AND SPILL PREVENTION - ALL ON SITE FUELS WILL BE DELIVERED, HANDLED, STORED, USED, AND APPLIED SO AS NOT TO BE RELEASED INTO THE WATERS OF THE STATE/US. FUELING WILL BE ACCOMPLISHED AWAY FROM THE WORK AREA A SPILL CLEANUP KIT WILL BE AVAILABLE IF DEEMED BY THE EPCM TO BE REQUIRED. C) WASTE MANAGEMENT - HANDLING, STORAGE AND DISPOSAL OF SOLID WASTE AND/OR HAZARDOUS WASTE WILL BE DISPOSED INTO SUITABLE LANDFILL OFFSITE D) INSPECTION AND MAINTENANCE - DAILY INSPECTION AND MAINTENANCE FOR ALL CONTROLS INCLUDED IN THE POLLUTION CONTROL PLAN AND THE ESCP WILL BE PERFORMED BY THE EPCM OR HIS DESIGNEE. E) EMPLOYEE AND SUBCONTRACTOR TRAINING - EMPLOYEE AND SUBCONTRACTOR EDUCATION AT A MINIMUM WILL INCLUDES INFORMING PERSONNEL OF THE POSTED LOCATIONS OF THE POLLUTION CONTROL PLAN/EROSION AND SEDIMENT CONTROL PLAN/MSDS'S AND IMPORTANT EMERGENCY PHONE NUMBERS. EDUCATION WILL ALSO INCLUDE INFORMING PERSONNEL OF REVISED MATERIAL MANAGEMENT PROCEDURES FOLLOWING A SPILL F) (CRITERIA 15) PRECONSTRUCTION ACTIVITY - BEFORE ALTERATION OF THE ACTION AREA, FLAG THE BOUNDARIES OF CLEARING LIMITS ASSOCIATED WITH SITE ACCESS AND CONSTRUCTION TO MINIMIZE SOIL AND VEGETATION DISTURBANCE, AND ENSURE THAT ALL TEMPORARY EROSION CONTROLS ARE IN PLACE AND FUNCTIONAL. G) (CRITERIA 16) SITE PREPARATION - DURING SITE PREPARATION, CONSERVE NATIVE MATERIALS FOR RESTORATION, INCLUDING LARGE WOOD, VEGETATION, TOPSOIL AND CHANNEL MATERIALS (GRAVEL, COBBLE AND BOULDERS) DISPLACED BY CONSTRUCTION. WHENEVER PRACTICAL, LEAVE NATIVE MATERIALS WHERE THEY ARE FOUND AND IN AREAS TO BE CLEARED, CLIP VEGETATION AT GROUND LEVEL TO RETAIN ROOT MASS AND ENCOURAGE REESTABLISHMENT OF NATIVE VEGETATION. BUILDING AND RELATED STRUCTURES MAY NOT BE CONSTRUCTED INSIDE THE RIPARIAN MANAGEMENT AREA H) (CRITERIA 17) HEAVY EQUIPMENT - HEAVY EQUIPMENT WILL BE SELECTED AND OPERATED AS NECESSARY TO MINIMIZE ADVERSE EFFECTS ON THE ENVIRONMENT; AND ALL VEHICLES AND OTHER HEAVY EQUIPMENT WILL BE USED AS FOLLOWS:  
 (A) STORED, FUELED AND MAINTAINED IN A VEHICLE STAGING AREA PLACED 150 FEET OR MORE FROM ANY WATER BODY, OR IN AN ISOLATED HARD ZONE SUCH AS A PAVED PARKING LOT.  
 (B.) INSPECTED DAILY FOR FLUID LEAKS BEFORE LEAVING THE VEHICLE STAGING AREA FOR OPERATION WITHIN 50 FEET OF ANY WATER BODY.  
 (C.) STEAM-CLEANED BEFORE OPERATION BELOW ORDINARY HIGH WATER, AND AS OFTEN AS NECESSARY DURING OPERATION TO REMAIN FREE OF ALL EXTERNAL OIL, GREASE, MUD, SEEDS, ORGANISMS AND OTHER VISIBLE CONTAMINANTS.  
 (D.) GENERATORS, CRANES AND ANY OTHER STATIONARY EQUIPMENT OPERATED WITHIN 150 FEET OF ANY WATER BODY WILL BE MAINTAINED AND PROTECTED AS NECESSARY TO PREVENT LEAKS AND SPILLS FROM ENTERING THE WATER I) (CRITERIA 18)

### IN-WATER WORK PERIOD -

ALL WORK WITHIN THE ACTIVE CHANNEL WILL BE COMPLETED IN ACCORDANCE WITH THE OREGON GUIDELINES FOR TIMING OF IN-WATER WORK TO PROTECT FISH AND WILDLIFE RESOURCES (ODFW 2000, OR THE MOST RECENT VERSION).

J) (CRITERIA 21) EMBANKMENT INSTALLATION -  
<TBC>

K) (CRITERIA 24) SUBGRADE PREPARATION -  
<TBC>

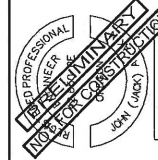
- DEQ 1200-C PERMIT IS REQUIRED.
- UNLESS DIRECTED OTHERWISE, REMOVE CLEARED AND GRUBBED MATERIAL FROM THE SITE AND DISPOSE AT AN APPROVED LOCATION.
- UNLESS OTHERWISE NOTED, THE SAMPLING AND TESTING OF MATERIALS FOR USE ON THE JOBSITE SHALL BE AT THE EXPENSE OF THE CONTRACTOR. ALL TESTING OF MATERIALS AND WORKMANSHIP SHALL BE PERFORMED BY A CERTIFIED TESTER. RESULTS OF THE TESTS SHALL BE SENT DIRECTLY TO THE PROJECT ENGINEER AS WELL AS THE CONTRACTOR, BY THE LABORATORY. LOCATION AND FREQUENCY OF TESTS SHALL BE DESIGNATED BY THE GENERAL CONTRACTOR.
- ALL CUT AND FILL SLOPES SHALL BE MAXIMUM OF 2:1.

### GEOTECHNICAL NOTE

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE PROJECT ENGINEER FOR REQUIRED REMEDIATION. THE CONTRACTOR SHALL COORDINATE WITH THE PROJECT ENGINEER FOR REQUIRED SITE OBSERVATIONS AND TESTING OF ALL FILLS.

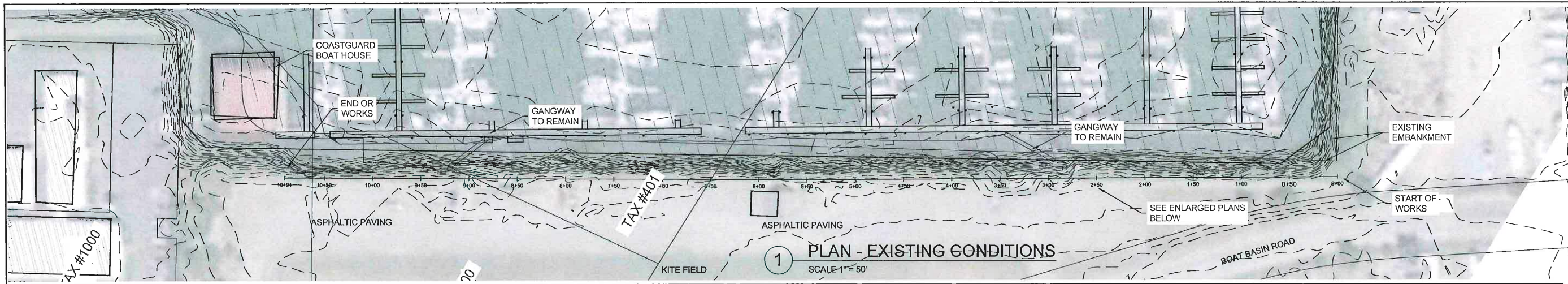
REVISIONS	BY:

Grants Pass \* Jacksonville \* Medford, OR  
 Grants Pass \* Jacksonville \* Grants Pass, OR 97527  
 166330 Lower Harbor Road, Brookings, OR 97415  
 Phone: 541-747-4434 \* Cell: 541-261-9929 \* Fax: 541-737-4488  
[emc@emcengineerscientists.com](mailto:emc@emcengineerscientists.com) <http://www.emcengineerscientists.com>  
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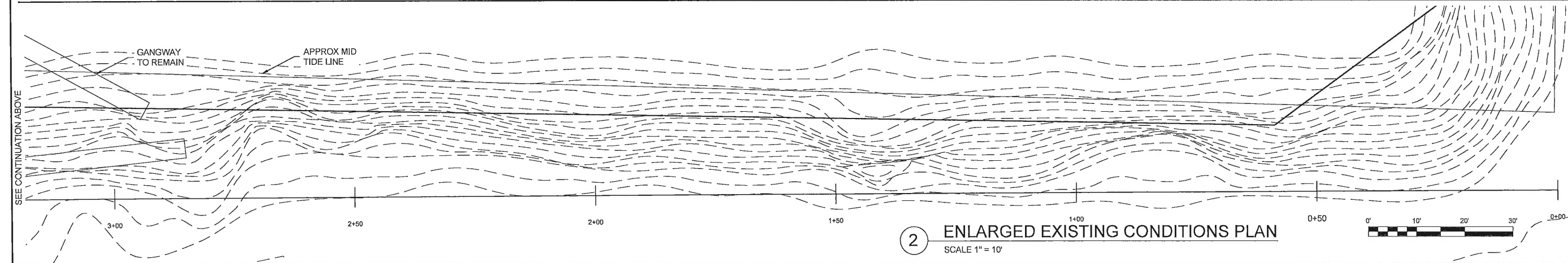
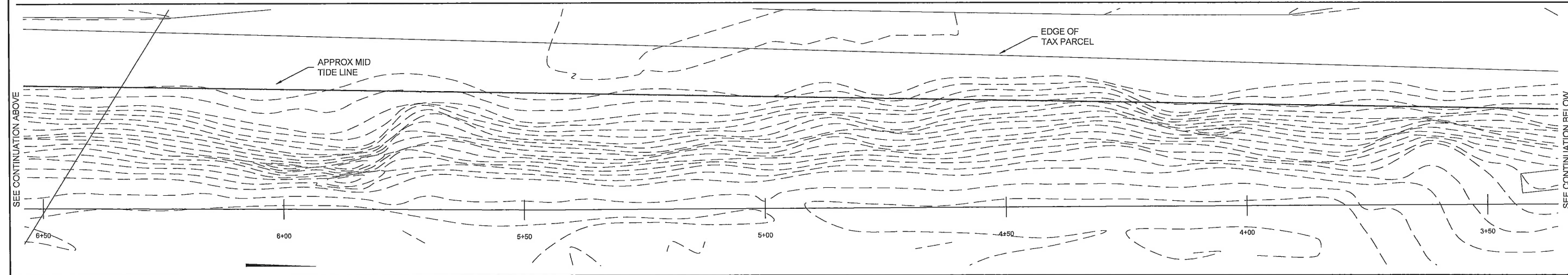
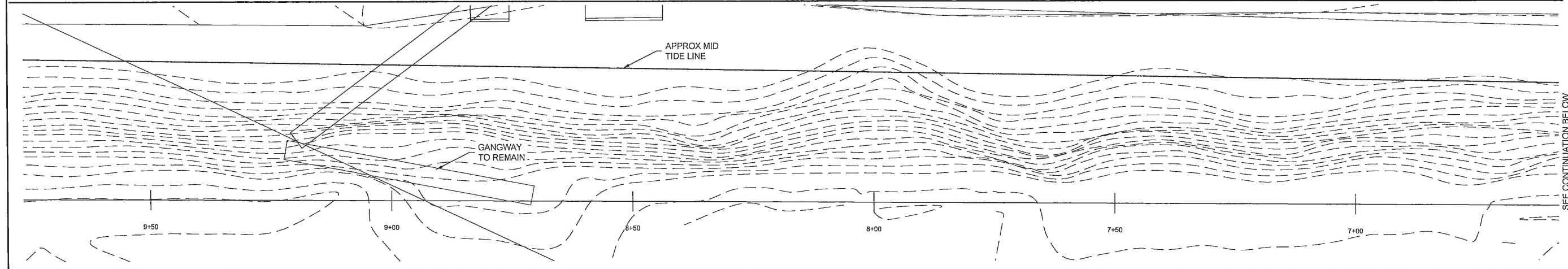


**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**EMBANKMENT ROCK RECONSTRUCTION**

DRAWN BY: CDJ/JW  
 DATE: 4 MAY 2022  
 JOB No: 22-202203  
 SHEET No:  
**C1.1**  
 GENERAL NOTES



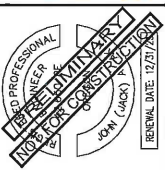
1 PLAN - EXISTING CONDITIONS  
SCALE 1" = 50'



2 ENLARGED EXISTING CONDITIONS PLAN  
SCALE 1" = 10'

REVISIONS	BY:

Grants Pass • Jacksonville • Medford, OR  
 CP Office: 1807 Williams Hwy., Suite 218, Grants Pass, OR, 97527  
 Ph: 541-724-8434 • Cell: 541-261-9929 • Fax: 541-727-5488  
 ems@emcengineerscientists.com • <http://www.emcengineerscientists.com>  
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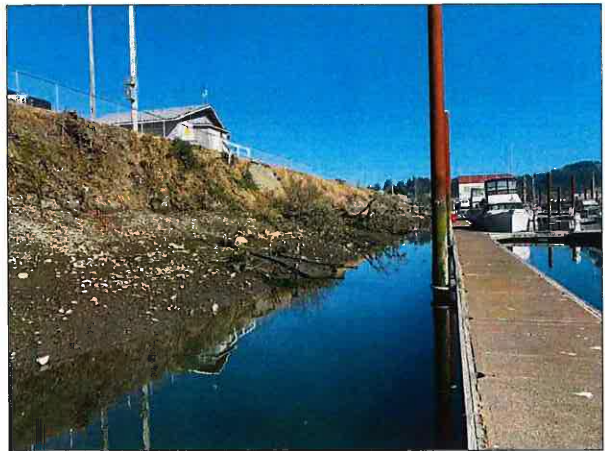


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 EMBANKMENT ROCK RECONSTRUCTION

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 DATE: 4 MAY 2022  
 JOB No: 22-202203  
 SHEET No:

**C2.0**  
 EXISTING  
 CONDITIONS

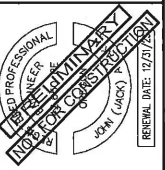
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1 EXISTING EMBANKMENT PHOTOS  
NO SCALE

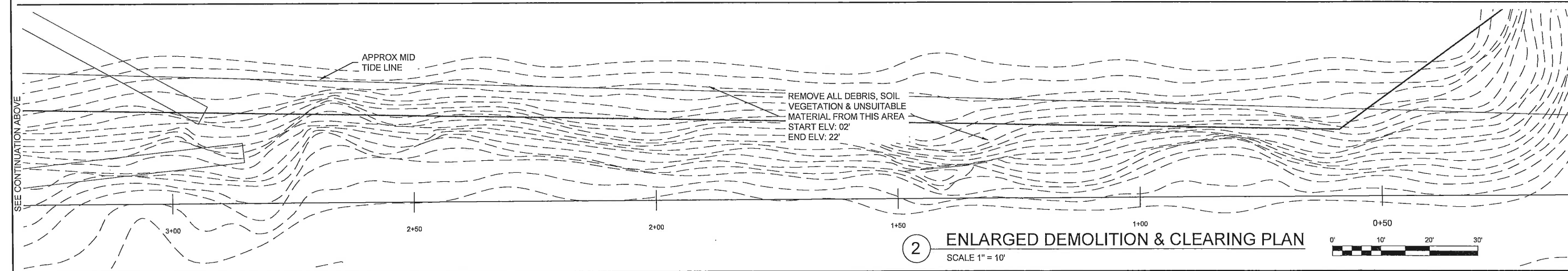
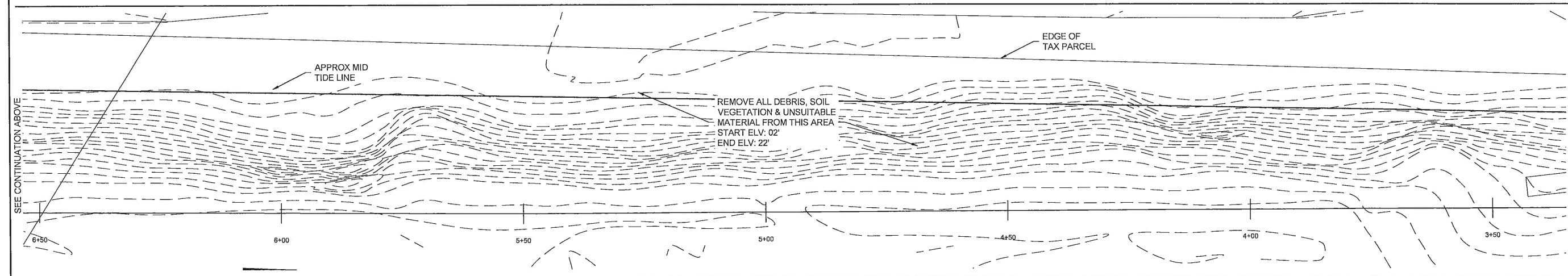
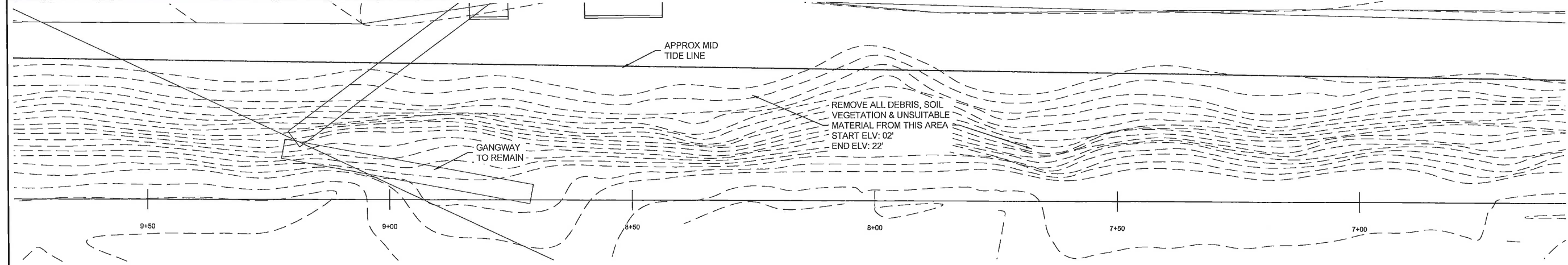
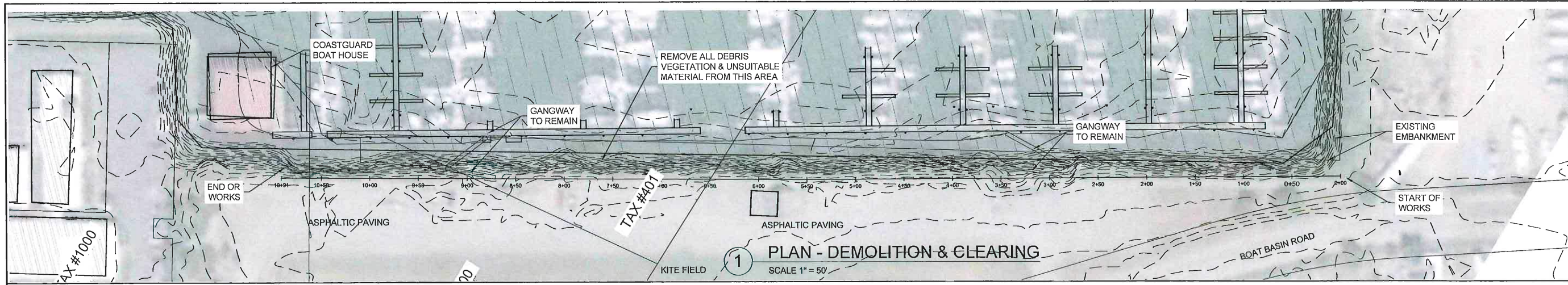
REVISIONS	BY:

Grants Pass \* Jacksonville \* Medford, OR  
 16330 Lower Harbor Road, Brookings, OR 97415  
 Phone: 541-744-8434 \* Cell: 541-261-9929 \* Fax: 541-737-5488  
<http://www.emcplainscientists.com>  
 emc@emcplainscientists.com



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 EMBANKMENT ROCK RECONSTRUCTION

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**C2.1**  
 EXISTING  
 EMBANKMENT  
 PHOTOS

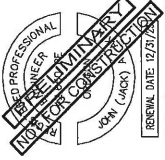


1 PLAN - DEMOLITION & CLEARING  
SCALE 1" = 50'

2 ENLARGED DEMOLITION & CLEARING PLAN  
SCALE 1" = 10'

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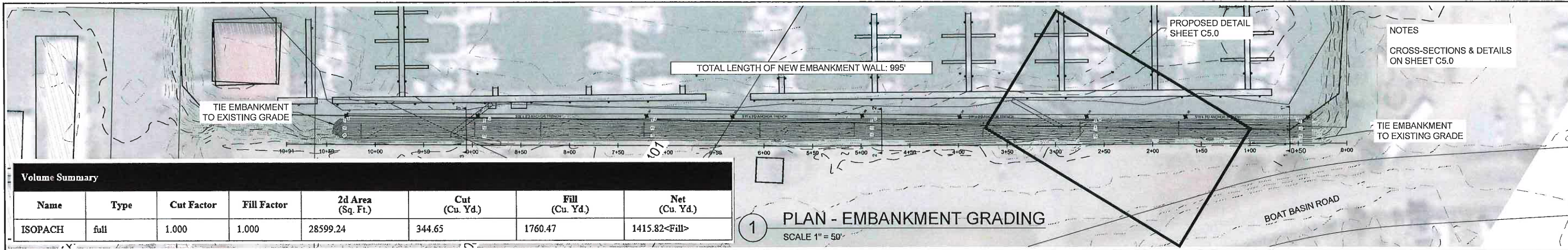
Grants Pass \* Jacksonville \* Medford, OR  
 CP Office: 1867 Williams Hwy., Suite 216, Grants Pass, OR, 97527  
 Ph: 541-764-8434 \* Cell: 541-261-9929 \* Fax: 541-727-5488  
 emc@emcengineerscientists.com http://www.emcengineerscientists.com  
**Engineers/Scientists, LLC**



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**EMBANKMENT ROCK RECONSTRUCTION**

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 JOB No: 22-202203  
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**C3.0**  
 DEMOLITION & CLEARING

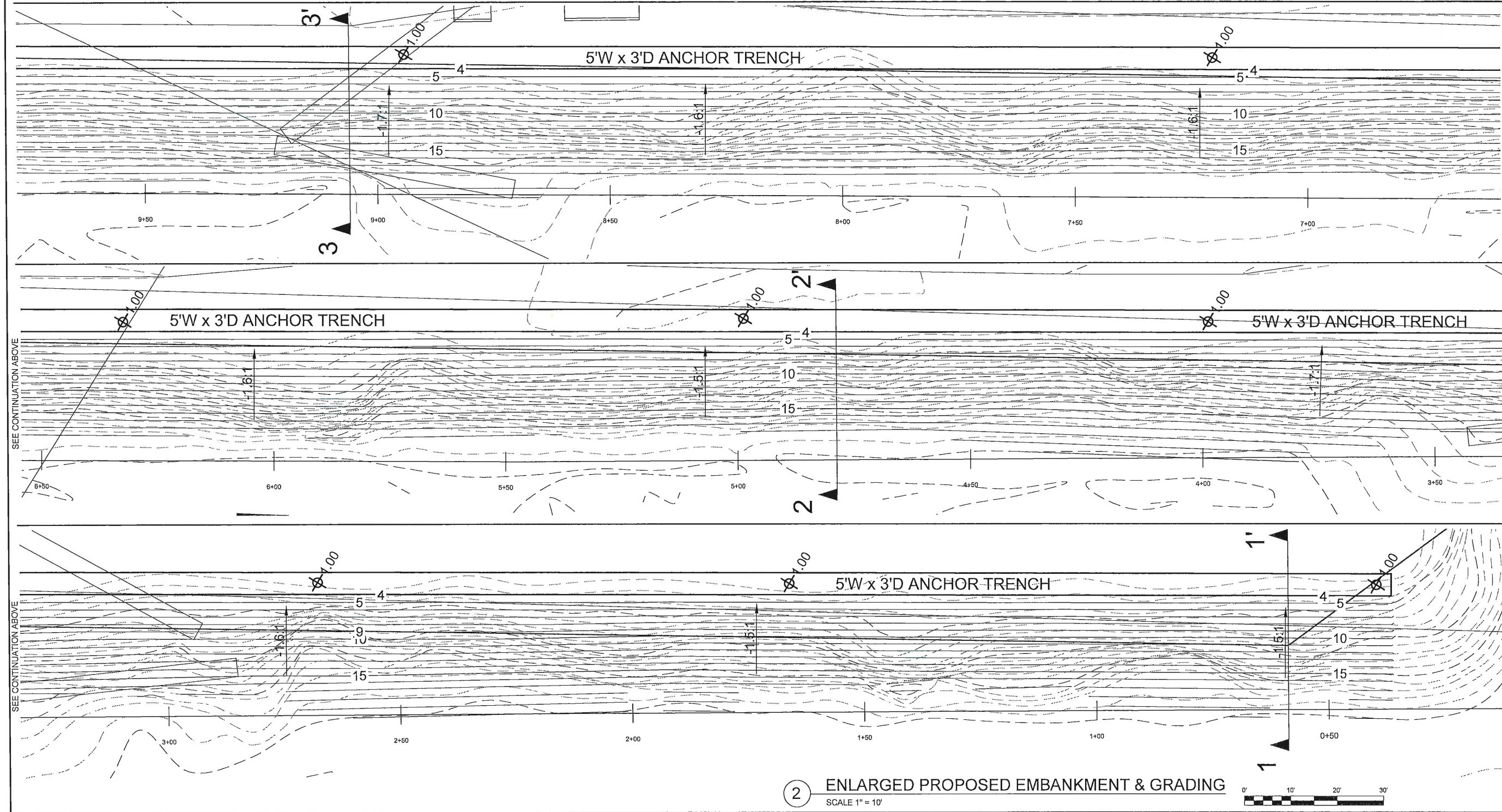




**Volume Summary**

Name	Type	Cut Factor	Fill Factor	2d Area (Sq. Ft.)	Cut (Cu. Yd.)	Fill (Cu. Yd.)	Net (Cu. Yd.)
ISOPACH	full	1.000	1.000	28599.24	344.65	1760.47	1415.82<Fill>

1 PLAN - EMBANKMENT GRADING  
SCALE 1" = 50'



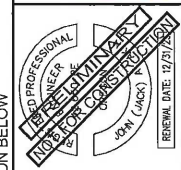
2 ENLARGED PROPOSED EMBANKMENT & GRADING  
SCALE 1" = 10'



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BY:	

Grants Pass \* Jacksonville \* Medford, OR  
 CP Office: 1867 Williams Hwy., Suite 216, Grants Pass, OR, 97527  
 Ph: 541-746-8434 \* Cell: 541-261-9919 \* Fax: 541-727-5488  
 emc@emcengineerscientists.com \* http://www.emcengineerscientists.com



PORT OF BROOKINGS HARBOR  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
 EMBANKMENT ROCK RECONSTRUCTION

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 SHEET No:  
**C4.0**  
 PROPOSED EMBANKMENT

**DESIGN SPECIFICS OF ROCK & CONSTRUCTION**

THE ROCK USED (IF 406 MITIGATION IS APPROVED) FOR THIS PROJECT WILL BE SPECIFIED TO FOLLOW TEST REQUIREMENTS FOUND WITHIN AASHTO 85 (APPARENT SPECIFIC GRAVITY, PERCENT ABSORPTION); ODOT TM 208A (DEGRADATION); AND AASHTO T 104 (SOUNDNESS). ALL ROCK SPECIFIED IN THIS PROJECT MUST BE ANGULAR IN SHAPE, AND THE THICKNESS OF ANY SINGLE ROCK SHALL NOT BE LESS THAN ONE THIRD OF ITS LENGTH. ROUND ROCK WILL NOT BE ACCEPTED UNLESS AUTHORIZED BY EMC. THE ROCK MUST MEET THE GRADATION REQUIREMENTS FOR THE CLASS SPECIFIED, BE FREE FROM OVERBURDEN, SPOILED, SHALE AND ORGANIC MATERIAL. NON-DURABLE ROCK, SHALE OR ROCK WITH SHALE SEAMS IS NOT ACCEPTABLE. CLASS 2000 RIP RAP IS BY DEFINITION COMPRISED OF ROCKS THAT ARE 20% BY WEIGHT OF 1400 POUNDS TO 2000 POUNDS, 30% BY WEIGHT OF 700 TO 1400 POUNDS, 40% BY WEIGHT 40 TO 700 POUNDS AND 0 TO 10% 0 TO 40 POUNDS. EITHER A FILTER BLANKET OF 16 INCH LAYER OF CLASS 50, OR SPECIFIED FILTER FABRIC WILL BE LAID BENEATH THE ROCK.

A CLAMSHELL, ORANGE PEEL BUCKET, SKIP OR SIMILAR APPROVED DEVICE WILL BE USED WHICH WILL TRANSPORT THE RIPRAP MATERIAL TO ITS FINAL DESTINATION. THIS REVETMENT REPAIR IS FOR FLOW ASSUMED TO GENERALLY BE UNIFORM, STEADY AND SUBCRITICAL. HOWEVER, RAPIDLY VARYING, UNSTEADY FLOW CONDITIONS OCCUR OCCASIONALLY, AND EXCESSIVE WAVE ACTION, HYDRAULIC JUMPS AND EXTREME FLOW TURBULENCE CAN OCCUR AT THIS LOCATION. THESE CONDITIONS ARE AMONG THE REASONS FOR THE EXTENT OF PROTECTION PROPOSED. THE LONGITUDINAL EXTENT OF THIS REPAIR SHOULD BE CONTINUOUS FOR A DISTANCE GREATER THAN THE LENGTH THAT IS IMPACTED. THE VERTICAL EXTENT OF PROTECTION REQUIRED FOR THIS REVETMENT INCLUDES DESIGN HEIGHT AND FOUNDATION OR TOE DEPTH. THE DESIGN HEIGHT OF THE RIP RAP INSTALLATION IS TO BE EQUAL TO THE DESIGN HIGH WATER ELEVATION (KING TIDE PLUS STORM SURGE) WITH ADEQUATE FREEBOARD TO ACCOMMODATE WAVE ACTION, SUPER ELEVATION FROM THE CHANNEL BEND, HYDRAULIC JUMP, AND FLOW IRREGULARITIES, PLUS ERRATIC PHENOMENA SUCH AS UNFORESEEN EMBANKMENT SETTLEMENT, ACCUMULATION OF TRASH AND DEBRIS FROM THE RIVER.

SCOUR DEPTH IS ESTIMATED AT ABOUT 4 FEET FROM THE LOWEST ELEVATION IN THE CROSS-SECTION OF THE BASIN AT THIS POINT, UTILIZING THE CONSERVATIVE ASSUMPTION OF A MEDIAN DIAMETER OF BED MATERIAL TO BE ABOUT 0.15 M. RIPRAP THICKNESS FOR CLASS 2000 IS SPECIFIED TO BE AT LEAST A 4 FOOT LAYER.

THE FILTER BENEATH THE RIPRAP AND OVERLYING THE STRUCTURAL FILL IS TO PREVENT THE MIGRATION OF FINE SOIL PARTICLES THROUGH STRUCTURAL VOIDS AND TO DISTRIBUTE THE WEIGHT OF THE ARMORING UNITS (RIPRAP) TO PROVIDE MORE UNIFORM SETTLEMENT, AND ALSO PERMITS RELIEF OF HYDROSTATIC PRESSURES WITHIN THE SOILS.

FOR THE AREAS ABOVE THE WATERLINE AT ANY GIVEN TIME THE FABRIC OR GEOTEXTILE ALSO PREVENTS SURFACE WATER FROM CAUSING EROSION BENEATH THE RIP RAP. IN ADDITION TO TOE CONSIDERATIONS WITH RESPECT TO SCOUR THE FLANKS OF THIS REVETMENT ARE DESIGNED FOR UPSTREAM AND DOWNSTREAM CONDITIONS

**GENERAL CONSTRUCTION, EROSION & CONTROL NOTES**

FINAL BANK SLOPE WILL BE BETWEEN 1V:1.5H AND 1V:2H. BANK PREPARATION WILL CONSIST OF CLEARING DEBRIS AND MINOR GRADING. RIPRAP PLACEMENT WILL BE BY MACHINE PLACING AND HAND PLACING. HAND PLACING WILL BE PERFORMED AS SPECIFIED BY EMC ON STEEPER SIDE SLOPES.

RE-HANDLING OR DRAGGING TO SMOOTH REVETMENT SERVICES TEND TO RESULT IN SEGREGATION AND BREAKAGE OF STONE AND ARE TO BE AVOIDED. STONE WILL NOT BE DROPPED FROM AN EXCESSIVE HEIGHT.

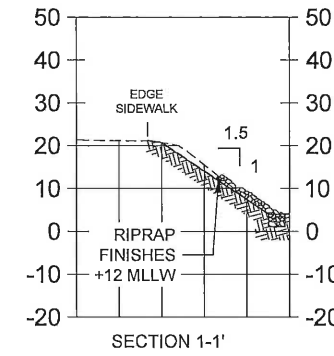
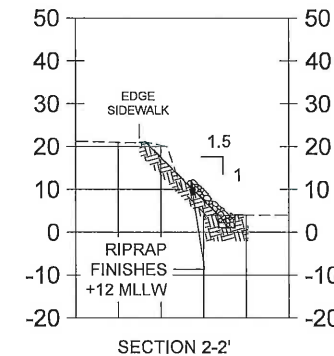
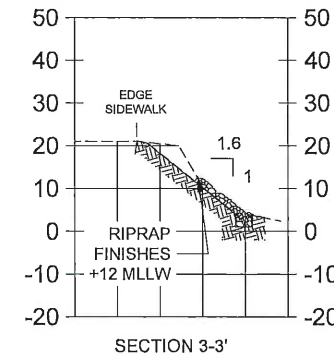
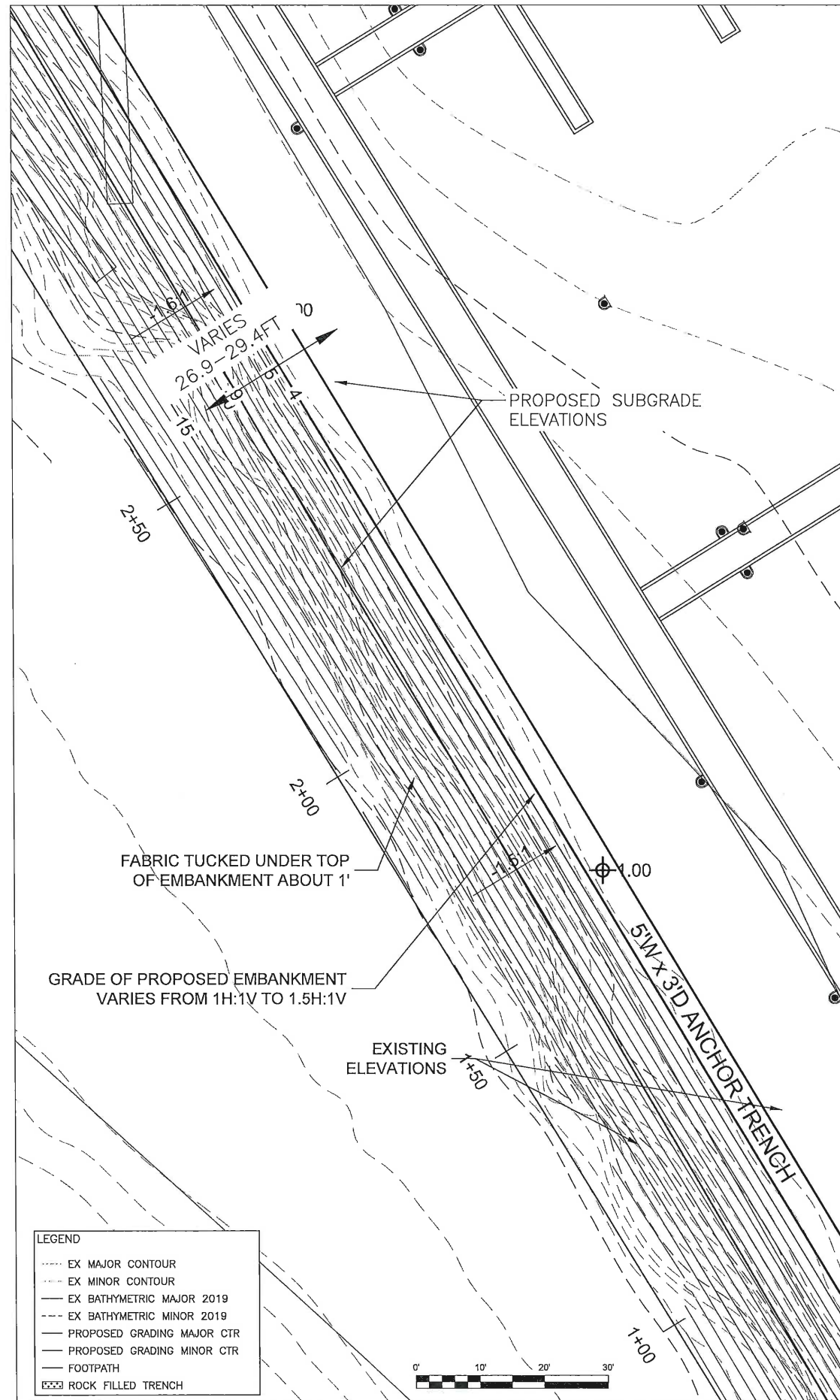
ACTIONS THAT WILL REQUIRE THE USE OF MATERIALS THAT ARE HAZARDOUS OR TOXIC TO AQUATIC LIFE (SUCH AS MOTOR FUEL, OIL, OR DRILLING FLUID), ARE INCLUDED WITHIN THE POLLUTION AND EROSION CONTROL PLAN SECTIONS OF THIS NARRATIVE, TO BE MANAGED BY EMC AND ENFORCED BY THE PORT OF BROOKINGS-HARBOR. THE PLAN INCLUDES PRACTICES TO MINIMIZE EROSION AND SEDIMENTATION ASSOCIATED WITH ALL ASPECTS OF THE PROJECT (E.G., STAGING AREAS, STOCKPILES, GRADING); TO PREVENT DEBRIS FROM DROPPING OR OTHERWISE ENTERING ANY STREAM OR WATERBODY; AND TO PREVENT AND CONTROL HAZARDOUS MATERIAL SPILLS.

EROSION CONTROLS WILL BE MONITORED AND MAINTAINED DAILY DURING THE RAINY SEASON AND WEEKLY DURING THE DRY SEASON AS NECESSARY TO ENSURE CONTROLS ARE PROPERLY FUNCTIONING. IF MONITORING SHOWS THAT THE EROSION CONTROLS ARE INEFFECTIVE AT PREVENTING VISIBLE SEDIMENT DISCHARGE, THE PROJECT WILL STOP TO EVALUATE EROSION CONTROL MEASURES, REPAIRS, REPLACEMENTS OR THE INSTALLATION OF ADDITIONAL EROSION CONTROL MEASURES WILL BE COMPLETED BEFORE THE PROJECT RESUMES.

IF APPLICABLE, MAINTENANCE WILL INCLUDE REMOVAL OF SEDIMENT AND DEBRIS FROM EROSION CONTROLS LIKE SILT FENCES OR HAY BALES ONCE IT HAS REACHED ON-THIRD OF THE EXPOSED HEIGHT OF THE CONTROL. WHENEVER PRACTICAL, NATIVE MATERIALS ARE TO BE LEFT WHERE THEY ARE FOUND AND IN AREAS TO BE CLEARED, VEGETATION IS TO BE CLIPPED AT GROUND LEVEL TO RETAIN ROOT MASS AND ENCOURAGE REESTABLISHMENT OF NATIVE VEGETATION.

HEAVY EQUIPMENT WILL BE SELECTED AND OPERATED AS NECESSARY TO MINIMIZE ADVERSE EFFECTS ON THE ENVIRONMENT (E.G., MINIMALLY-SIZED, LOW PRESSURE TIRES, MINIMAL HARD TURN PATHS FOR TRACKED VEHICLES, TEMPORARY MATS OR PLATES WITHIN WET AREAS OR SENSITIVE SOILS); AND ALL VEHICLES AND OTHER HEAVY EQUIPMENT WILL BE USED AS FOLLOWS:

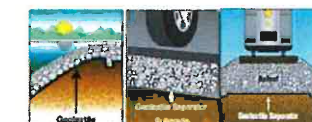
- 1) STORED, FUELED AND MAINTAINED IN A VEHICLE STAGING AREA PLACED 150 FEET OR MORE FROM ANY WATERBODY, OR IN AN ISOLATED HARD ZONE SUCH AS A PAVED PARKING LOT, OR LINED SURFACE;
- 2) INSPECTED DAILY FOR FLUID LEAKS BEFORE LEAVING THE VEHICLE STAGING AREA FOR OPERATION WITHIN 50 FEET OF ANY WATERBODY;
- 3) STEAM-CLEANED BEFORE OPERATION BELOW ORDINARY HIGH WATER, AND AS OFTEN AS NECESSARY DURING OPERATION TO REMAIN FREE OF ALL EXTERNAL OIL, GREASE, MUD, SEEDS, ORGANISMS AND OTHER VISIBLE CONTAMINANTS AND
- 4) GENERATORS, CRANES AND ANY OTHER STATIONARY EQUIPMENT OPERATED WITHIN 150 FEET OF ANY WATERBODY WILL BE MAINTAINED AND PROTECTED AS NECESSARY TO PREVENT LEAKS AND SPILLS FROM ENTERING THE WATER.



CROSS-SECTIONS

**Nonwoven Geotextile**

NTPEP APPROVED - GTX-2013-01-019. US 300NW is a nonwoven needlepunched geotextile made of 100% polypropylene staple filaments. US 300NW resists ultraviolet and biological deterioration, rotting, naturally encountered bases and acids. Polypropylene is stable within a pH range of 2 to 13. US 300NW meets the following M.A.R.V. values except where noted:

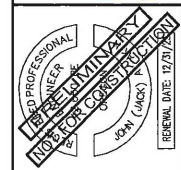


PROPERTY	TEST METHOD	ENGLISH	METRIC
Weight - Typical	ASTM D-5261	12 oz/sy	407 g/sm
Tensile Strength	ASTM D-4632	300 lbs	1,335 N
Elongation @ Break	ASTM D-4632	50%	50%
Nullen Burst*	ASTM D-3786*	580 psi	3,999 kPa
Puncture Strength*	ASTM D-4833*	180 lbs	801 N
CBR Puncture	ASTM D 6211	850 lbs	3,782 N
Trapezoidal Tear	ASTM D-4533	115 lbs	511 N
Apparent Opening Size	ASTM D-4751	100 US Sieve #150	mm
Permeability	ASTM D-4491	1.00 Sec-1	1.00 Sec-1
Water Flow Rate	ASTM D-4491	75 g/min/sf	3,055 l/min/m <sup>2</sup>
UV Resistance @ 500 Hours	ASTM D-4355	70%	70%

ROLL SIZE ROLL DIAMETER AREA WEIGHT		
12.5' x 360'	23.0 in	500 sqs 380 lbs
15' x 300'	25.0 in	500 sqs 380 lbs

REVISIONS	BY:

Grants Pass \* Jacksonville \* Medford, OR  
 CP Office: 1807 Williams Hwy., Suite 216, Grants Pass, OR, 97527  
 PH: 541-764-8434 \* Cell: 541-261-9919 \* Fax: 541-727-5488  
 emc@emcengineerscientists.com http://www.emcengineerscientists.com  
**EMC** - Engineers/Scientists, LLC



**PORT OF BROOKINGS HARBOR**  
 16330 LOWER HARBOR ROAD, BROOKINGS, OR 97415  
**EMBANKMENT ROCK RECONSTRUCTION**

DRAWN BY: CD/JW  
 DATE: 4 MAY 2022  
 JOB No: 22-202203  
 SHEET No:

**C5.0**  
 EMBANKMENT DETAILS

**1 EMBANKMENT DETAILS**  
 SCALE 1" = 10'

# ACTION ITEM – H

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**DATE:** August 17, 2022  
**RE:** Preliminary Wastewater Treatment Plant Design  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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## OVERVIEW

- April 20, 2022, the Board approved Jack Akin/EMC Engineers/Scientists (Port Engineer) to design the wastewater treatment plant to meet regulatory requirements for disposal of crustacean and fish processing only.
- Engineering and permitting for this work are not reimbursable. Federal funding does not cover expenses prior to award.
- It is critical to begin the permitting process prior to EPA funding due to the lengthy permitting process.

## DOCUMENTS

- Administrative Project Timeline, 1 page
- Preliminary Wastewater Treatment Plant Presentation, 27 pages

## COMMISSIONERS ACTION

- **Recommended Motion:**  
Motion to approve Jack Akin/EMC Engineers/Scientists (Port Engineer) to proceed with the wastewater treatment plant preliminary drawings and pursue Oregon Department of Environmental Quality permit with these conceptual drawings.

# Port of Brookings Harbor

## Wastewater Treatment Plant Project Timeline

Commissioner Meetings Highlighted

Dates		Description
August 17, 2022		Board Approval – Design for DEQ Permit Application
August 31, 2022	March 1, 2023	Submit DEQ Permit Application (6-month review)
September 1, 2022	November 1, 2022	Prepare WWTP Equipment Bid Package (Manufacturer)
September 1, 2022	November 1, 2022	Prepare Construction Drawings and Bid Package (Contractor)
October 1, 2022	December 1, 2022	EPA Funding – Contract Review
November 16, 2023		Board Review – WWTP Equipment Bid Package
November 16, 2023		Board Review – Construction Drawings and Bid Package
December 21, 2022		Board Approval – WWTP Equipment Bid Package
December 21, 2022		Board Approval – EPA Funding
December 21, 2022		Board Approval – Construction Drawings and Bid Package
January 1, 2023	April 1, 2023	County Planning and Building & Safety Permitting
January 1, 2022	February 1, 2023	Request for Proposal (RFP) WWTP Equipment (30-days)
February 1, 2023	March 1, 2023	Award & Order WWTP Equipment (6-month delivery)
February 15, 2023		Board Approval – Award WWTP Manufacturer
March 1, 2023	April 1, 2023	Request for Proposal (RFP) WWTP Contractor
April 1, 2023	May 1, 2023	Award WWTP Contractor
April 19, 2023		Board Approval – Award WWTP Contractor
October 2023		Construction Begins
March 2024		WWTP in Operation for 2024 Shrimp Season

Dates are subject to change depending on EPA funding and/or permitting challenges. Schedule has approximately 3 months of float from construction beginning to finishing before 2024 shrimp season.

# Wastewater Treatment

*Processes & Alternatives*

*8/17/2022*

Pacific Seafood is at present out of compliance with federal and state NPDES (National Pollutant Discharge Elimination System) wastewater effluent requirements. Currently, the facility uses a rotary drum screen to treat approximately 500,000 gallons per day prior to discharge, which does little to reduce the incoming BOD (Biological Oxygen Demand), TSS (Total Suspended Solids), and O&G (Oil and Grease).

The plant is not under a permit and is presently operating under agreement with ODEQ (Oregon Department of Environmental Quality). The ODEQ will most certainly issue, upon the plant's DMR (Discharge Monitoring Report) reports, a notice of non-compliance. Those DMR reports (May & June) have been submitted to ODEQ in recent weeks. It is not clear exactly when the NON will be issued. The response by the plant to the NON would be to present a plan to the ODEQ as to how they will get into compliance.

At that time the ODEQ is expected to, hopefully, enter into agreement with Pacific Seafood, based on the Port of Brookings Harbor's ability to provide wastewater treatment. Pacific Seafood has expressed in no uncertain terms that it cannot afford the multi-million dollar price tag for a wastewater treatment plant.

However, the Port of Brookings Harbor has received promise for a \$3.5 million dollar grant to provide wastewater treatment at its facilities. The Port had initially investigated a well-tried aerobic system, but the Port feels that its footprint is too large (220' x 280'), its multiple components and its general configuration is too complex and overbearing in appearance.

Since then, the Port has investigated the available, applicable wastewater treatment approaches that are applied to fish processing, which include DAF, Anaerobic, multi-staged aerobic and membrane technologies.

## **DAF**

DAF (Dissolved Air Flotation) is an alternative clarification process ideal for treating raw water with light particles, such as algae or color-causing organics, and at low temperatures when sedimentation is not as effective. Before the flocculated water enters the clarification chamber, millions of tiny air bubbles are released from the diffuser nozzles to the water, which attach and float the floc particles to the water surface. The air bubbles are generated in a pumped recycle stream by an air compressor and are dissolved in water through a packed-tower-type of saturator. The floated sludge is removed periodically to a desludging trough, and the clarified water flows to the bottom of the clarifying chamber and then to an effluent control weir for collection. The air saturator is used to dissolve air in the water and then generate bubbles to float the floc particles.



Our research has concluded that DAF is at this time the most used technology applied to the treatment of fish processing wastewater. The unit itself, sized for the Brookings Pacific Seafood facility would likely cost about \$500,000. However, with installation costs, programming and auxiliary systems would likely range between \$1.5 - \$2.5 million.

This system is used at the Westport Pacific Seafood facility. We have had multiple interviews with Amy Wentworth, corporate Director of Environmental, Health, & Safety, and Caleb Unema, R/D Manager, Pacific Seafood Value Creation Team.

Recently, following several phone conversations, Amy wrote the following in an 8/04/22 email: “We also have a DAF at our Westport facility that processes crab and shrimp. One of the biggest challenges we face is retaining DAF operators who are trained and capable to operate the DAF. It’s a dirty job and requires mechanical, technical, and chemical knowledge. DAFs run best when they’re treating consistent flows, too, and the difference in pollutants from processing crab vs shrimp are astronomical. Crab is very “clean” and shrimp is very “dirty” and each species uses a vastly different amount of water. So, the DAF is not great because it’s difficult to maintain optimum operating parameters when the flows and pollutants vary so frequently. It is possible for a DAF to be adjusted daily to the wastewater quality and quantity, but it requires a higher level of technical ability than most of our operators have (that we can retain).”

We also interviewed Caleb Unema via phone on 8/06/22. He is the person most familiar in the company with DAF at Pacific Seafood, its advantages and its disadvantages. He cites as advantages of the DAF to be 1. Widely used and accepted technology; 2. Small footprint and 3. Relatively inexpensive.

He cites disadvantages to be 1. Requires high level of expertise to operate and 2. Serious operational difficulties occur regularly due to the inconsistent nature of shrimp/crab processing.

In his opinion, the disadvantages greatly outweigh the advantages, to the extent that he stated, somewhat “tongue-in-cheek” that DAF stands for, in his mind, “Do Not Go After it for your Facility”. When asked “So why are you using DAF?”, Caleb responded that they don’t have the space for other preferred technologies. We conveyed to Caleb our interest in membrane and anaerobic treatment, he concurred that such would be his choice, but that Pacific Seafood, Westport does not have the necessary space.

Caleb did express advice that we watch out for the fouling of nano-membranes via over-exposure to oils and greases. Also, Pacific Seafood is already financially committed to its existing system. They presently have two DAF units on site. One of them is presently operational, and the other is “mothballed”.

## **Anaerobic Flotation Reactor (AFR)**

**Process** (refer to the numbered illustration below)

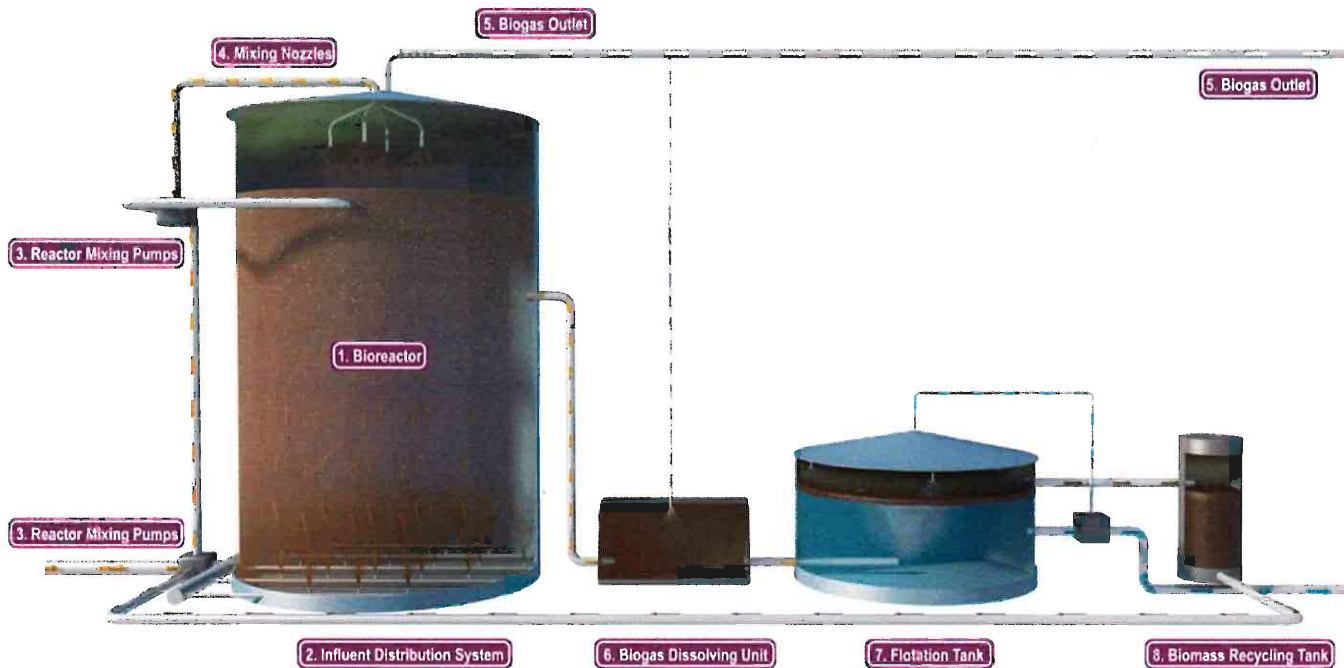
Industrial wastewater is pumped to the Bioreactor (1) through the Influent Distribution System (2) where it is thoroughly mixed with the reactor's anaerobic biomass to achieve optimal contact between biomass and organic compounds to be degraded.

The Bioreactor is hydraulically mixed by continuously pumping (3) the reactor contents to the influent distribution system and to the nozzles (4) in the upper part of the reactor. This exceptionally efficient mixing system ensures maximum conversion of the organic compounds, including fats and proteins, into valuable methane or biogas.

The biogas is collected in the head-space where it leaves the reactor (5) to be used as green energy in boilers, gas engines or upgraded to pipeline quality Renewable Natural Gas (RNG).

Following anaerobic digestion in the Bioreactor, the biomass solids are separated from the treated water through the Dissolved Biogas Flotation (DBF) process. In the DBF process, the effluent is continuously pumped into a Biogas Dissolving Unit (6), in which biogas is pumped and dissolved in the solution. This pressurized stream is then released in center of the Flotation Tank (7) at the bottom. Returning the stream to nearly atmospheric pressure causes the biogas to come out of solution and form small bubbles that attach to the biomass flocs. As the flow travels towards the tank's periphery, the flocs float to the surface where they compress into a flotation layer that is collected in a trough and flows towards the Biomass Recycling Tank (8). From this tank the biomass is pumped back to the Bioreactor. The treated water below the flotation layer leaves the flotation tank by means of a pump or valve and can be discharged or post treated.

The AFR is specifically designed for the treatment of wastewater containing high concentrations of fats, oil and grease (FOG) and other biodegradable compounds such as proteins and starches. The AFR will reliably and efficiently convert these organic compounds. The COD is converted into valuable biogas, while producing a high quality treated effluent.



The budget price for listed scope of supply is \$ 4.5 Million (FOB jobsite). Additionally, with the costs associated with site development and preparation, installation, controls, biogas development into useful energy, and startup, we estimate the total budget would be closer to \$6 million. Our evaluation of this system is that, first, despite its demonstrated applicability and effectiveness, environmental advantages, relative ease of operation and low sludge production, this system is just too expensive, and far beyond the Port's budget.

## **MEMBRANE ALTERNATIVE**

However, we continued to look closely at the Pacific Seafood wastewater, and noted that membrane screening could reduce the volume of wastewater to be treated, whether via anaerobically or otherwise, from 500,000 gpd to less than 5% of that, estimated to be about 15,000 GPD. The permeate from the membrane would consist of highly concentrated, 3% wastewater sludge.

This process includes two additional unit operations, a prefiltration system to remove fine particulate followed by a three stages-in-series filtration system loaded with superfiltration membrane elements. This process will not require in-line chemical dosing during operation, thereby lowering operating costs as compared with conventional treatment systems.

In the proposed process, the pre-treatment filter system proposed consists of a two-stage filtration due to the high feed solids concentration. The first stage is a single, 10.75" OD, self-cleaning RVZ filter with internal scraper device. The second stage is a single, 40" OD, self-cleaning RVM filter with internal vacuum cleaning system.

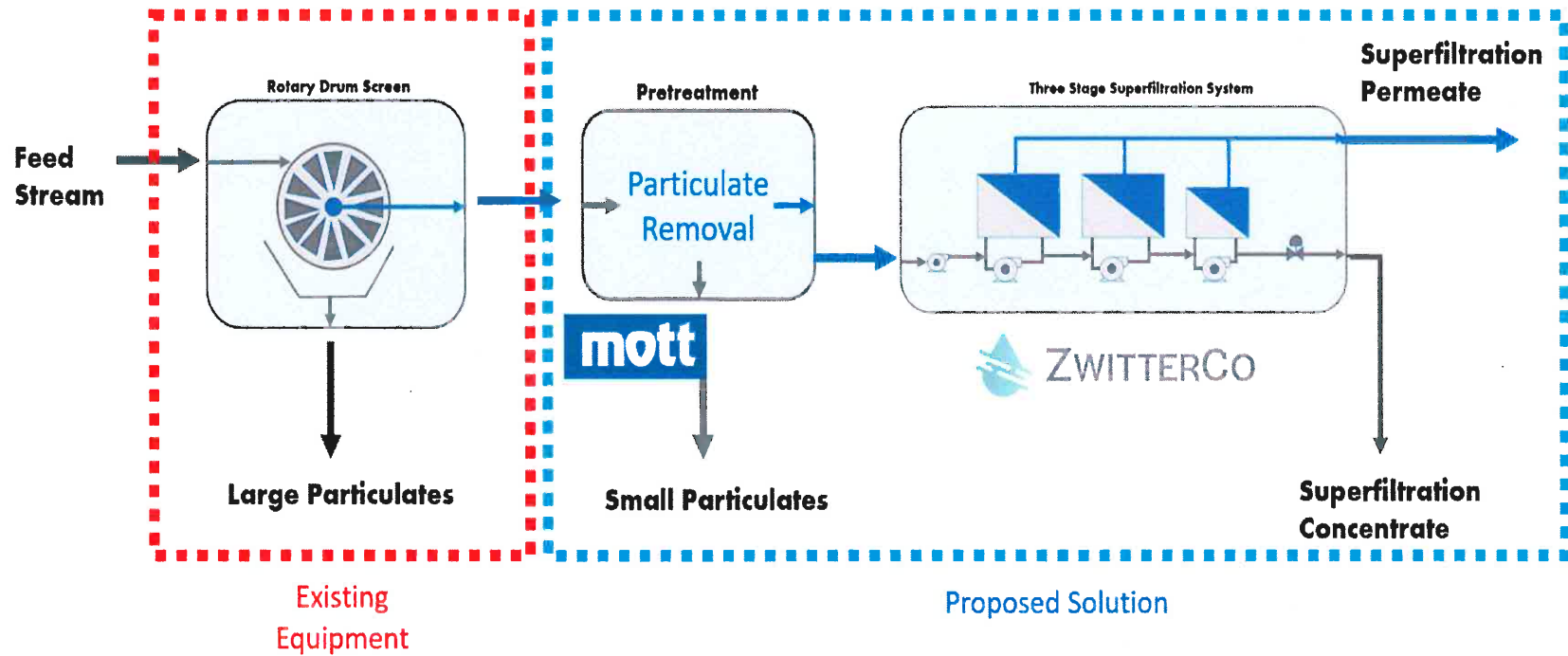
The second step is a stages-in-series filtration system utilizing superfiltration membrane elements. In this system, three membrane stages are cascaded so that feed enters the first stage and is partially concentrated. Exiting the first stage, the partially concentrated solution feeds into the second stage where it is further concentrated, before entering the third stage where it reaches its final concentration. The concentrate exits the system through a bleed valve on the third stage.

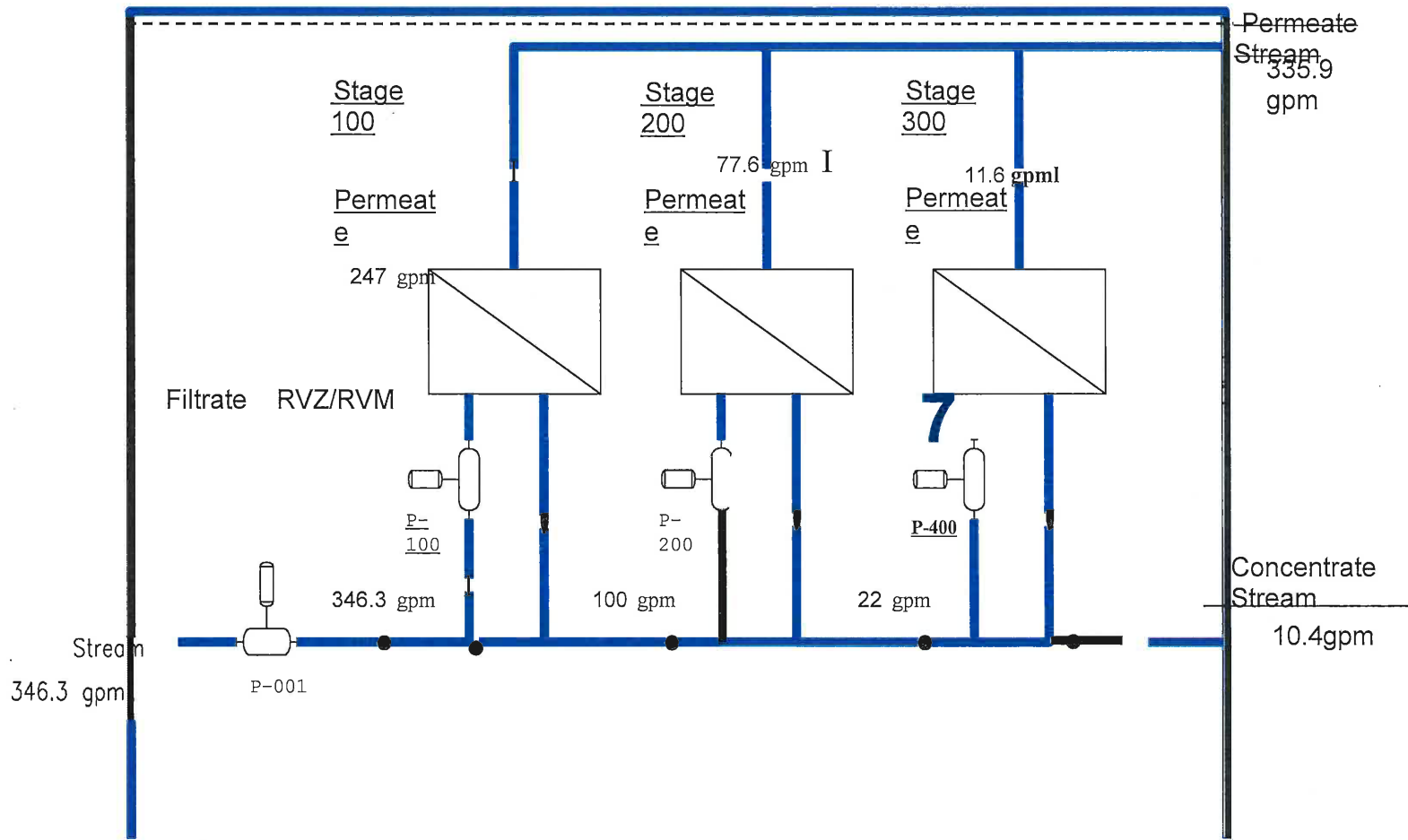
Each stage would contain a recirculation pump, valves, and instruments to control the crossflow and optimize stage and system performance. The third stage's concentrate control valve modulates the bleed rate and the subsequent ratio of feed that is converted to permeate ("Permeate Recovery"), which is expected to be at or above 97% on a volumetric basis.

The superfiltration membrane elements will deliver separation performance that will meet the facility's discharge limits (Note: permeate quality metrics and the system design basis are contingent on additional pilot testing).

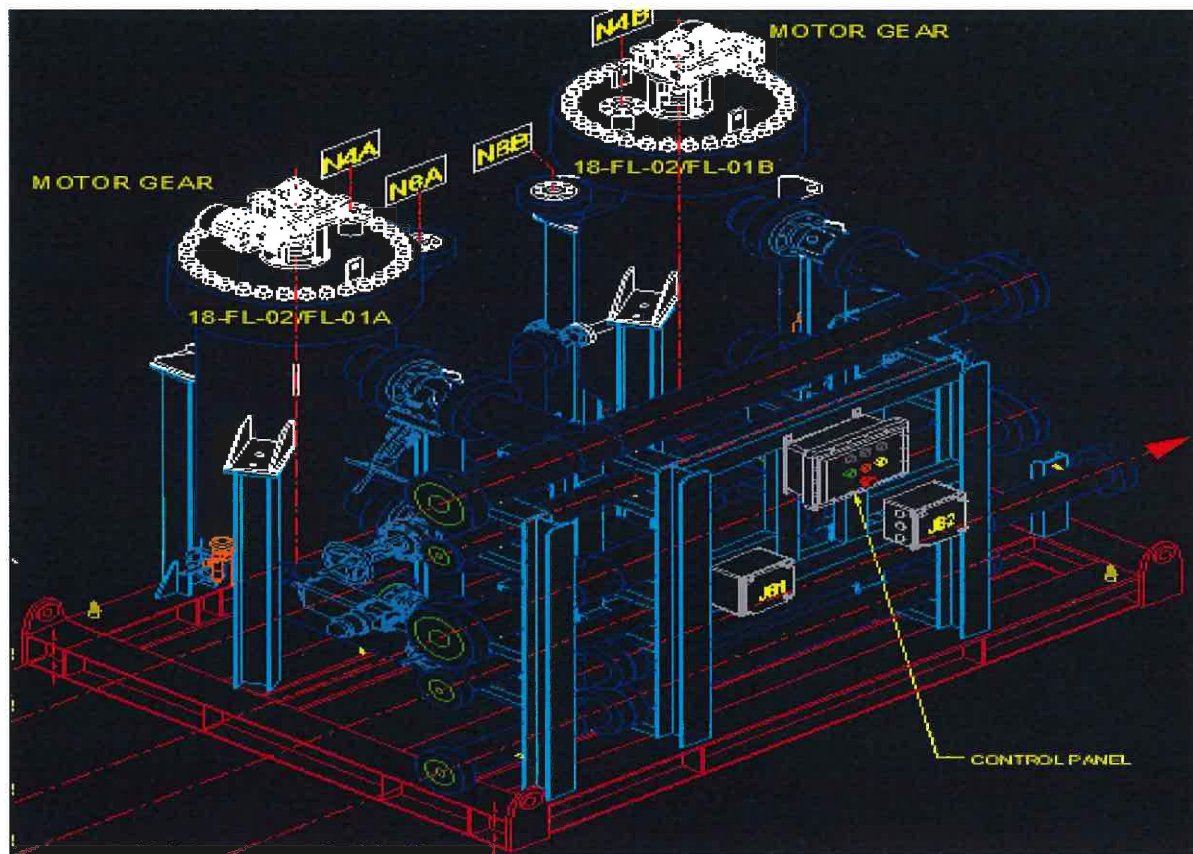


# Proposed Process Flow Diagram for Pacific Seafoods





130



The RVZ module with the 10" filter is approximately 8' deep x 6' wide x 6' tall, including the vessel, controls, and piping. The RVM module with the 40" filter is approximately 12' deep by 8' wide x 10' tall, including the vessel, controls, and piping. These two modules presume the feed pressure is sufficient such that a pump is not required.

**This membrane system would have thus reduced the wastewater to be treated from the nominal 350,000 GPD to a concentrated 15,000 GPD. The recommended alternatives to handling this sludge, are:**

*Alternative 1. treat the remaining waste sludge via AFR, reduced in size and cost;*

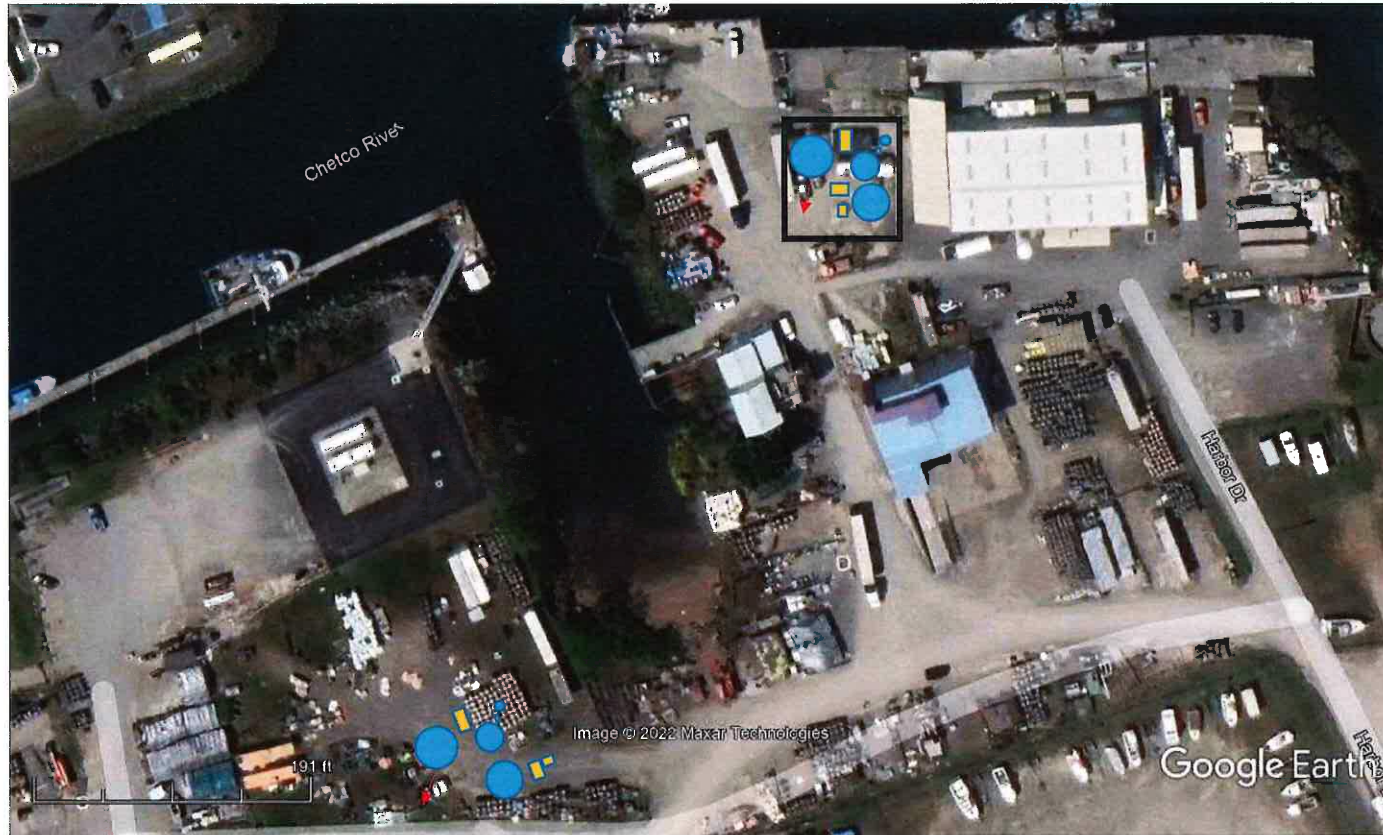
*Alternative 2. After solidifying membrane permeate (from 3% to 15% solids) via screw auger/plate press, routinely deliver the sludge to solid waste. This would result in one truck or less per day delivery. From there, alternative beneficial waste uses (e.g. composting) would be investigated.*

*Alternative 3. Direct membrane permeate into 42,000 gallon multistaged aerobic treatment system. Separated inorganic sludge will be minimized.*

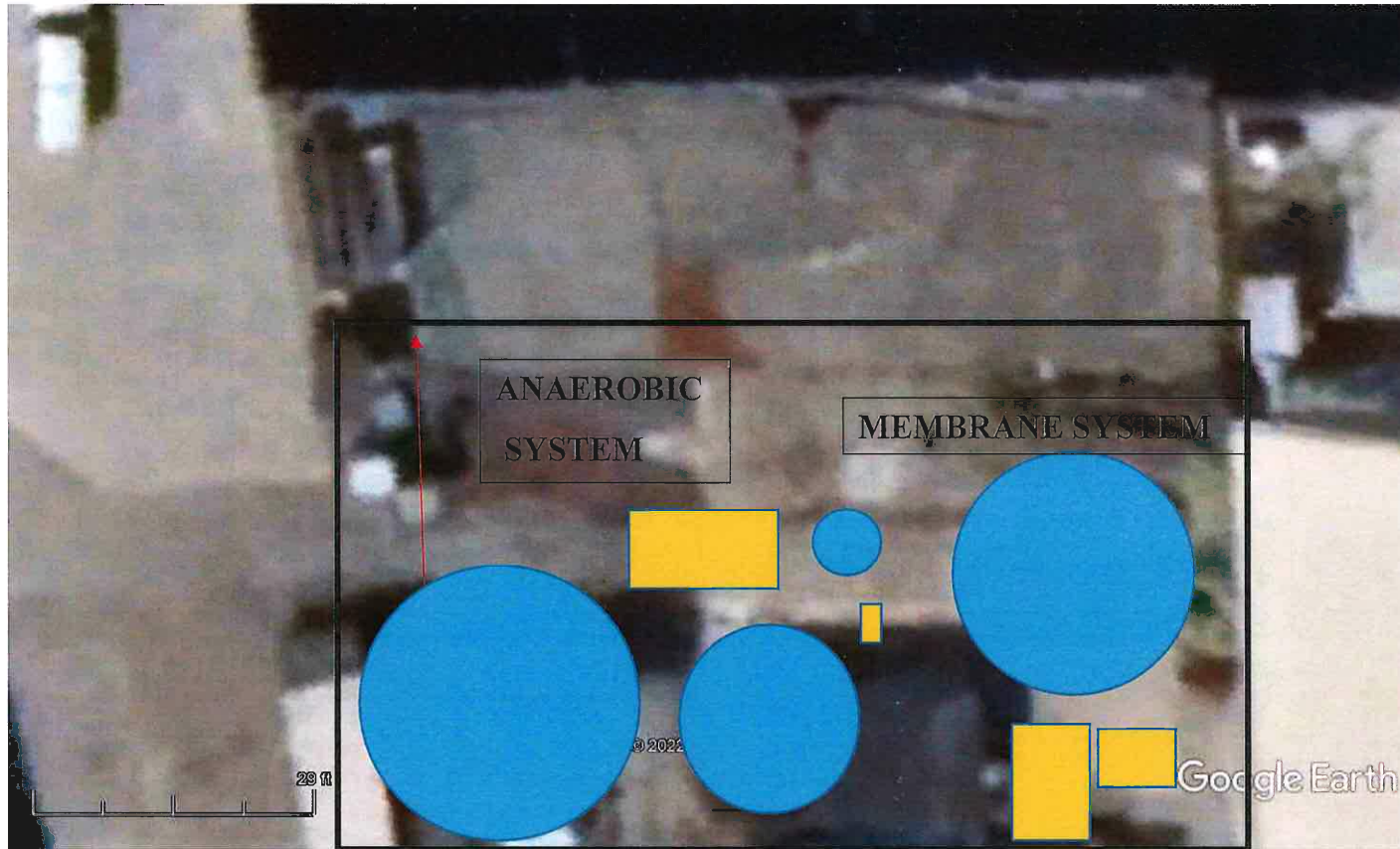
## **Operations**

This is a wastewater system, and so of course on-site management is required. However, membrane system management is relatively simple. System checks are necessary every day. Expertise requirements are limited to a technically-oriented individual. Daily time requirements are estimated to be from 4 – 6 hours, after a reasonable training/orientation period.

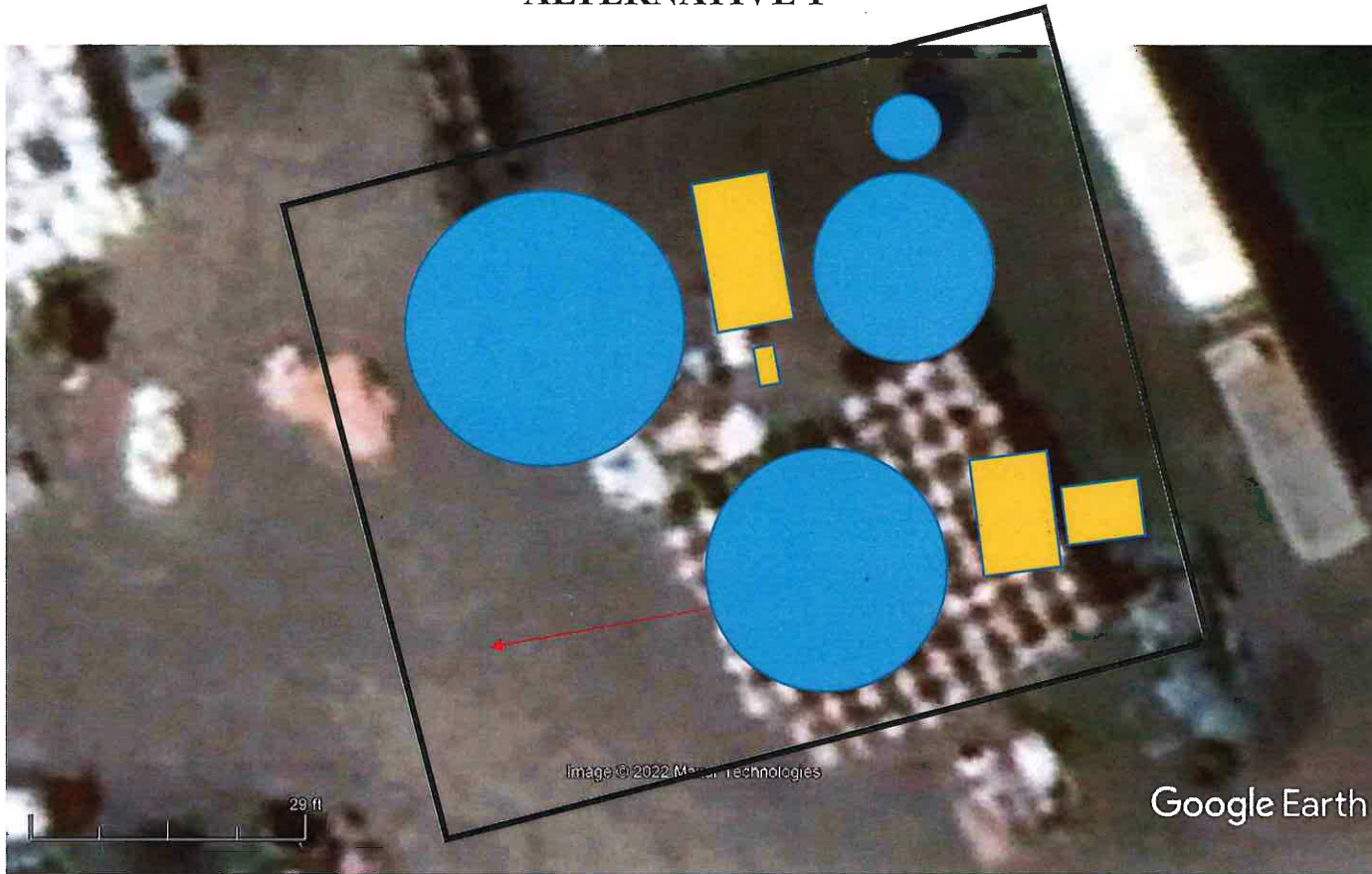
# ALTERNATIVE 1



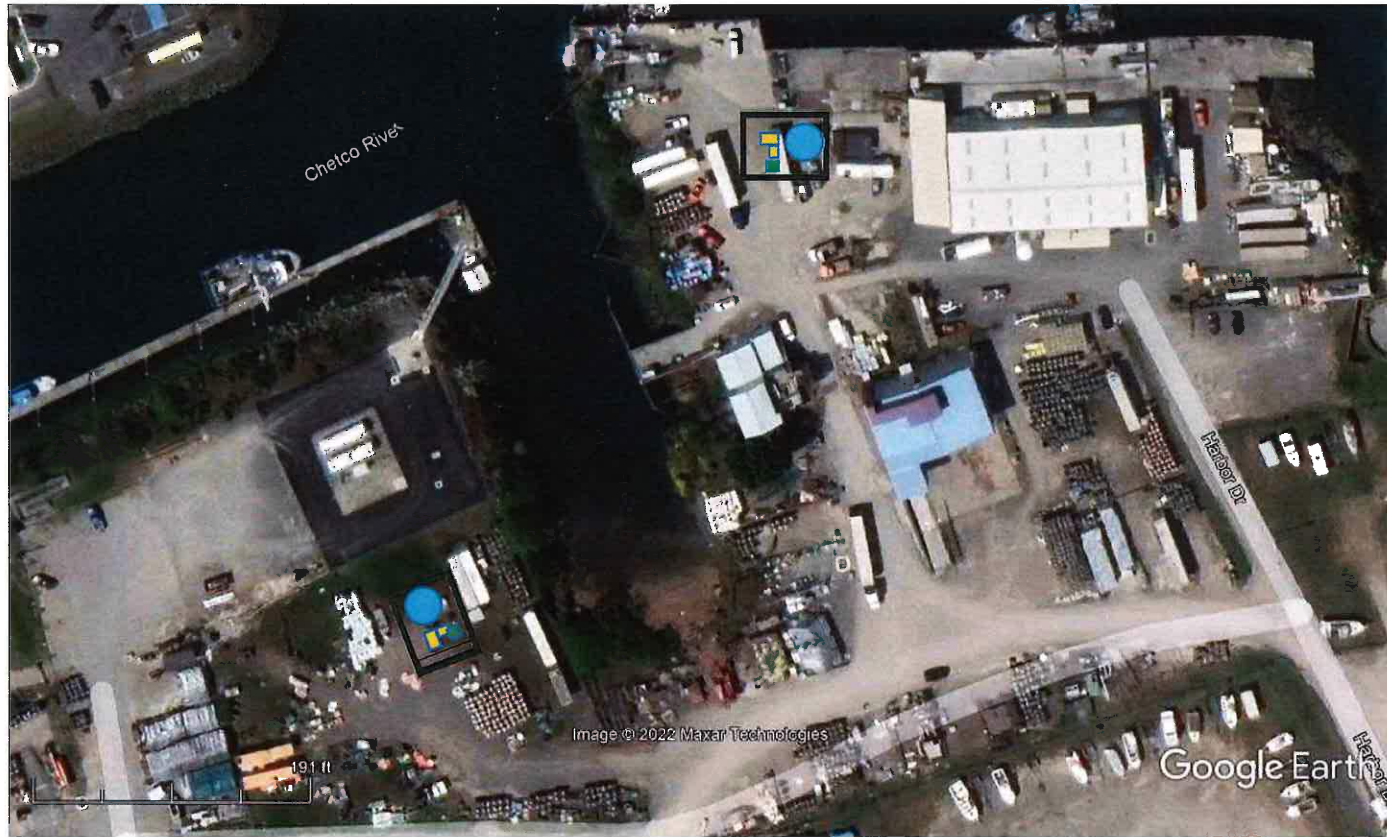
# ALTERNATIVE 1



# ALTERNATIVE 1

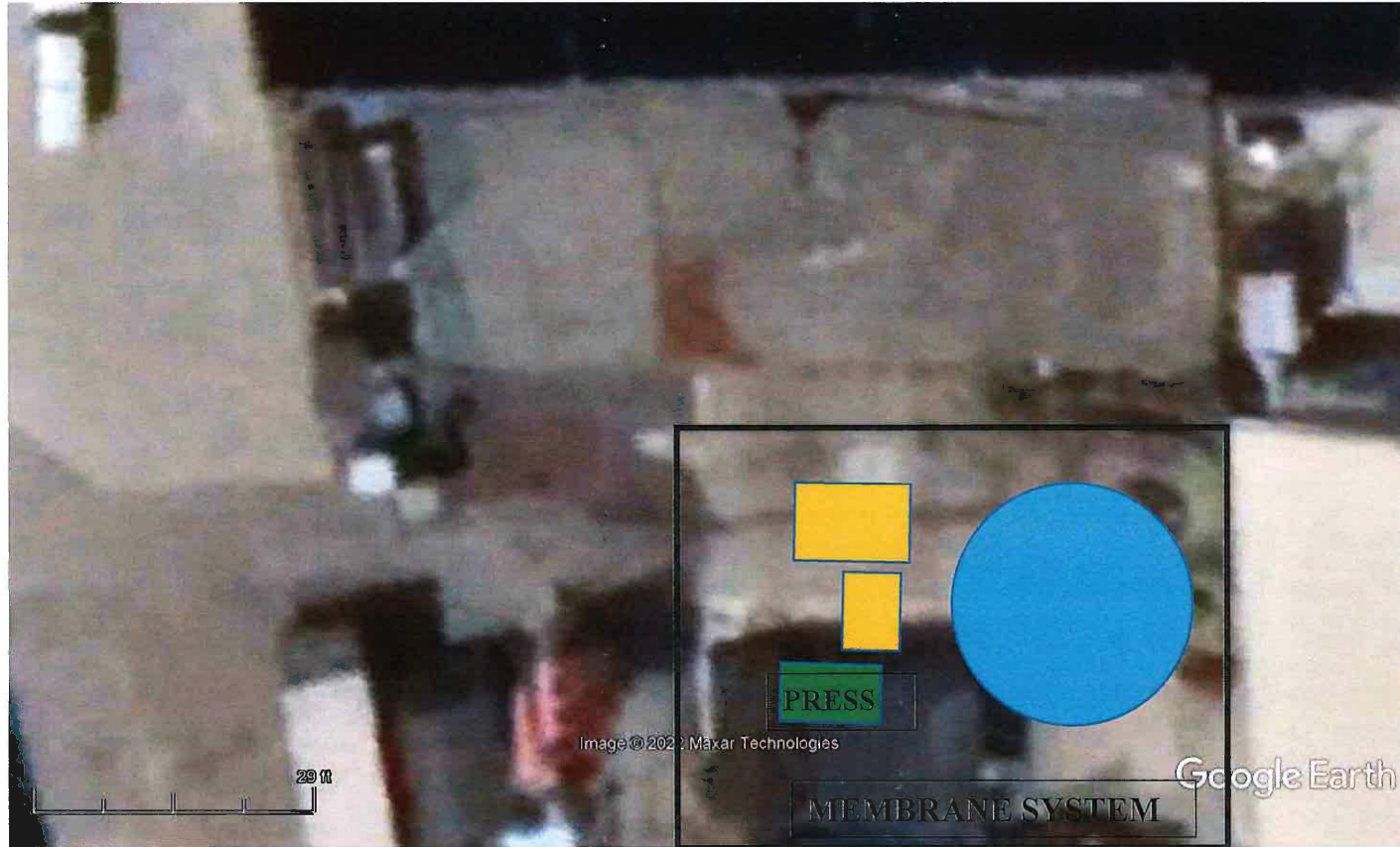


## ALTERNATIVE 2

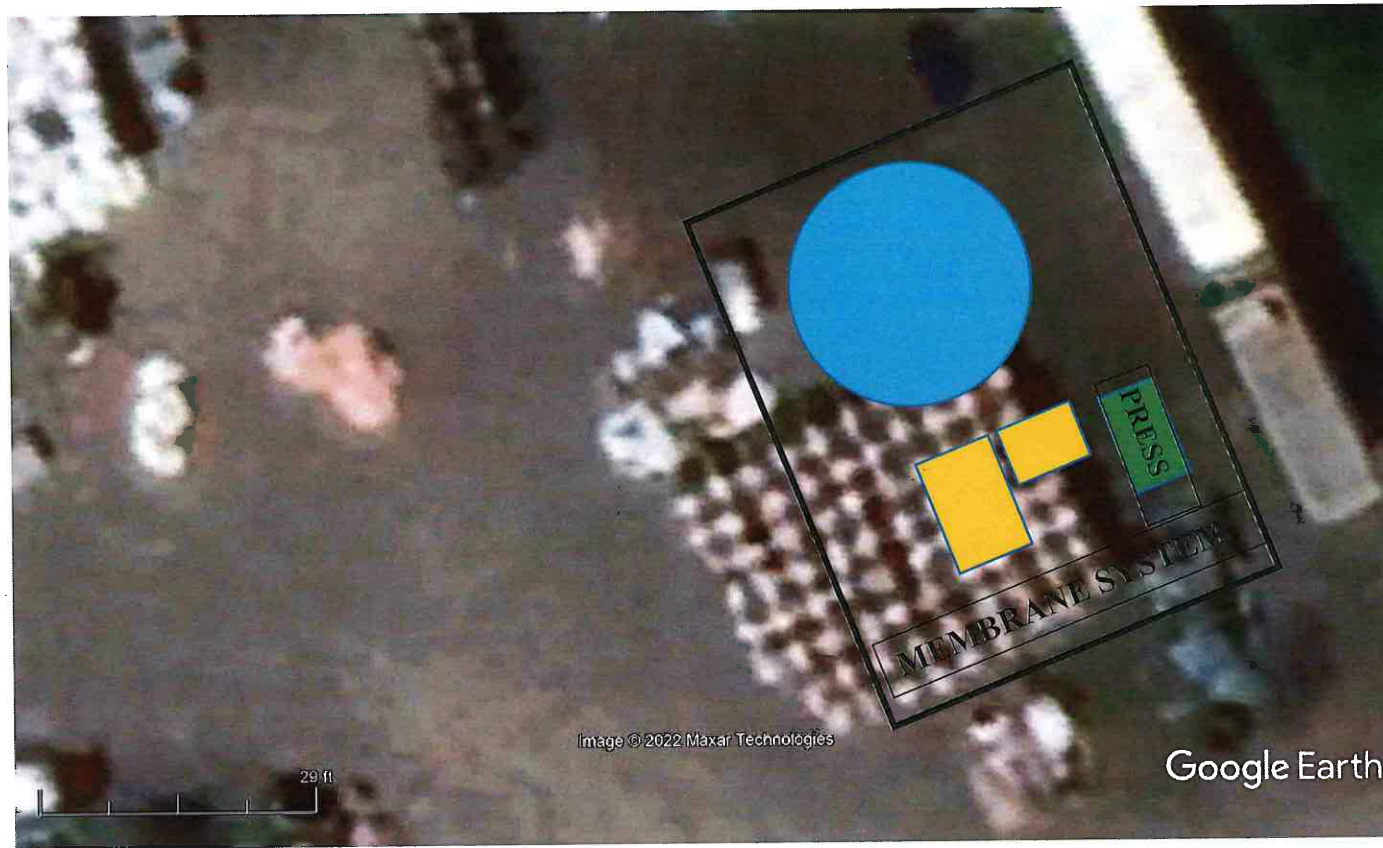




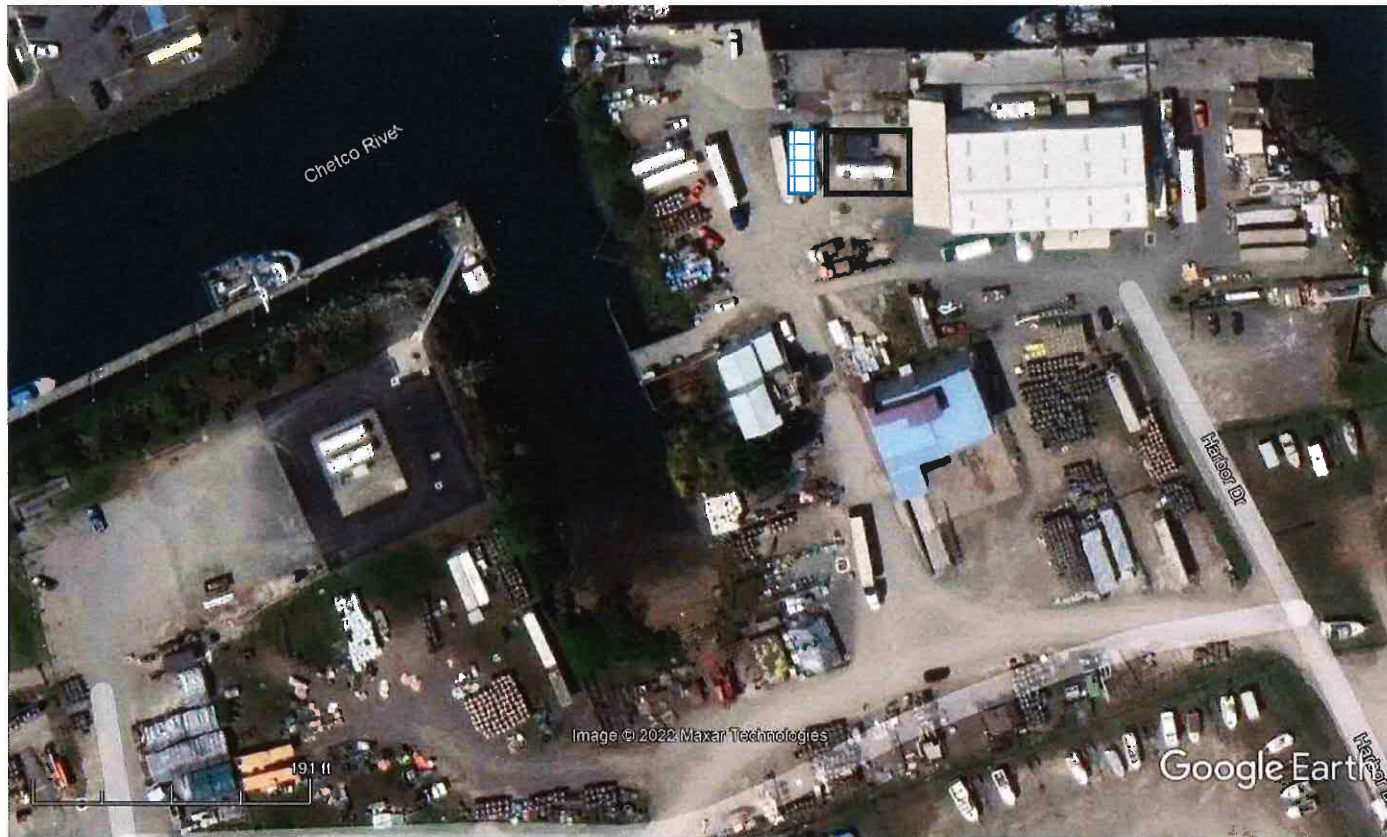
## ALTERNATIVE 2



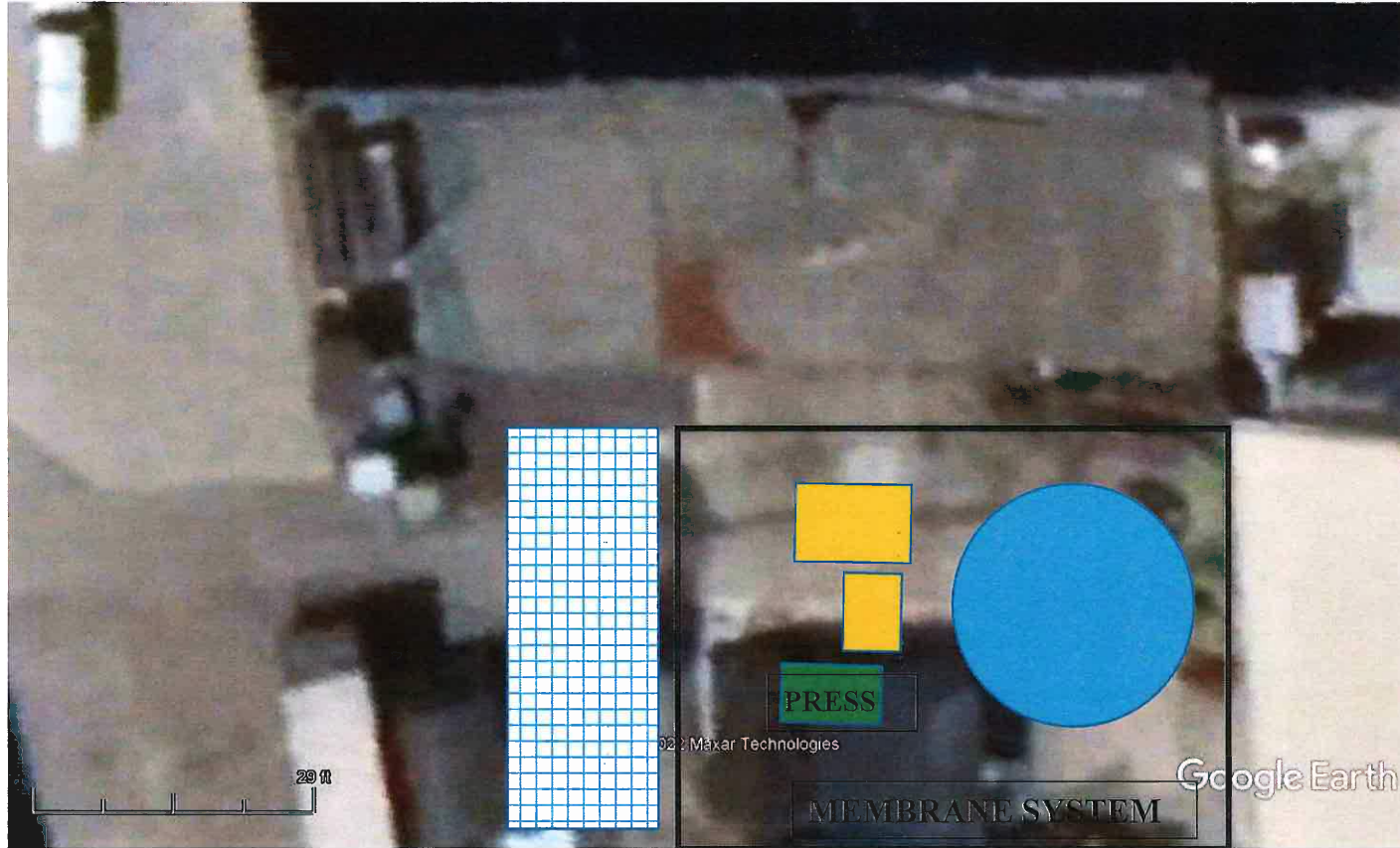
## ALTERNATIVE 2



ALTERNATIVE 3



### ALTERNATIVE 3



### ALTERNATIVE 3



## The Permitting Process

There has been some confusion as to the type of permit that would be selected by the ODEQ for this project. On 8/4/22 we received a final deliberation from the Department, stating “I have copied federal definitions for a POTW (Publicly Owned Treatment Works) below for your information. Typically, POTWs are sewage treatment plants but the federal regulations allow for a POTW to treat “industrial wastes of a liquid nature” (green highlight below).”

40 CFR 403.3 defines a POTW as the following: *The term Publicly Owned Treatment Works or POTW means a treatment works as defined by section 212 of the Act, which is owned by a State or municipality (as defined by section 502(4) of the Act). This definition includes any devices and systems used in the storage, treatment, recycling and reclamation of municipal sewage or industrial wastes of a liquid nature. It also includes sewers, pipes and other conveyances only if they convey wastewater to a POTW Treatment Plant. The term also means the municipality as defined in section 502(4) of the Act, which has jurisdiction over the Indirect Discharges to and the discharges from such a treatment works.*

Therefore, with the type of permit defined, the following is required to obtain a permit:

1. EPA Form 1
2. EPA Form 2A for a POTW or EPA Form 2D for a privately-owned treatment works.
3. Permit application fees – This will be dependent on projected wastewater flows and type of discharge (e.g., only seafood processing waste or other waste streams).
4. DEQ Land Use Compatibility Statement – This form needs to be signed by Curry County indicating that the proposed construction and operation of a wastewater treatment plant is consistent with the local acknowledged comprehensive plan.
5. Mixing zone study – Details are dependent on the proposal as DEQ has shared its concerns with you and the Port about the current discharge outfall in use by Pac Seafood.
6. Ambient data – This is to be determined by DEQ when more is known about the proposal and discharge location. This data is needed to determine water quality-based effluent limits.
7. DEQ may also require supplemental application information once the initial application information has been reviewed.

# INFORMATION ITEM – A

---

**DATE:** August 17, 2022  
**RE:** Boat Yard Dustless Blasting  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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## OVERVIEW

- Port's Best Management Practices prohibits sandblasting.
- Best Management Practices also states as new environmentally safe products become available which minimizes the need for scraping and sanding their use will be encouraged.
- Dustless blasting technology is now available.
- The Port staff would like to update the Best Management Practices and DEQ Stormwater Pollution Control Plan to allow dustless blasting in the boat yard repair area.
- Port staff is also recommending installing infiltration pit for the filtered pressure water disposal. Estimated pit size 18 feet x 6 feet and 3 feet deep. The pressure wash system is doing a great job cleaning the water but meeting the DEQ benchmarks to release it back into the water will not be achievable without adding more expensive metal filtration system.

## DOCUMENTS

- Dustless Blasting general information and pictures, 11 pages
- Best Management Practices page 5, 1 page
- Boat Wash Water "Before" and "After" Test Results, 3 pages





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5 MIN READ

The BEST Way to Remove Powder Coating  
(<https://www.dustlessblasting.com/blog/powder-coating-dustless-blasting-vs.-chemical-stripping?hsLang=en>)

May 24, 2018 Dustless Blasting

Powder coating is well known for its adhesion and...



DUSTLESS BLASTING FOR

# Marine Maintenance

Since dust is a by-product, only simple containment is needed to keep you and water pollutant-free.

**Remove antifouling paint, marine growth, and more.**



01:19

### **Suppress the Dust**

Others can continue working nearby while blasting is being performed.

### **Easy Containment**

Reduced dust means easier containment and cleanup.

### **Safe on All Surfaces**

Steel, aluminum or fiberglass... you won't warp or damage the substrate.

### **Powerful Removal**

Remove antifouling paint, epoxy, heavy marine growth, or open blisters in the gelcoat.





## CASE STUDY

### Remove antifouling paint

Antifouling paint needed to be removed from this 54' Viking yacht, so that a new layer of antifouling paint could be applied. Many blisters had also developed in the fiberglass gelcoat, which needed to be opened up for repair.

“ Marine maintenance has never been this easy or this profitable.

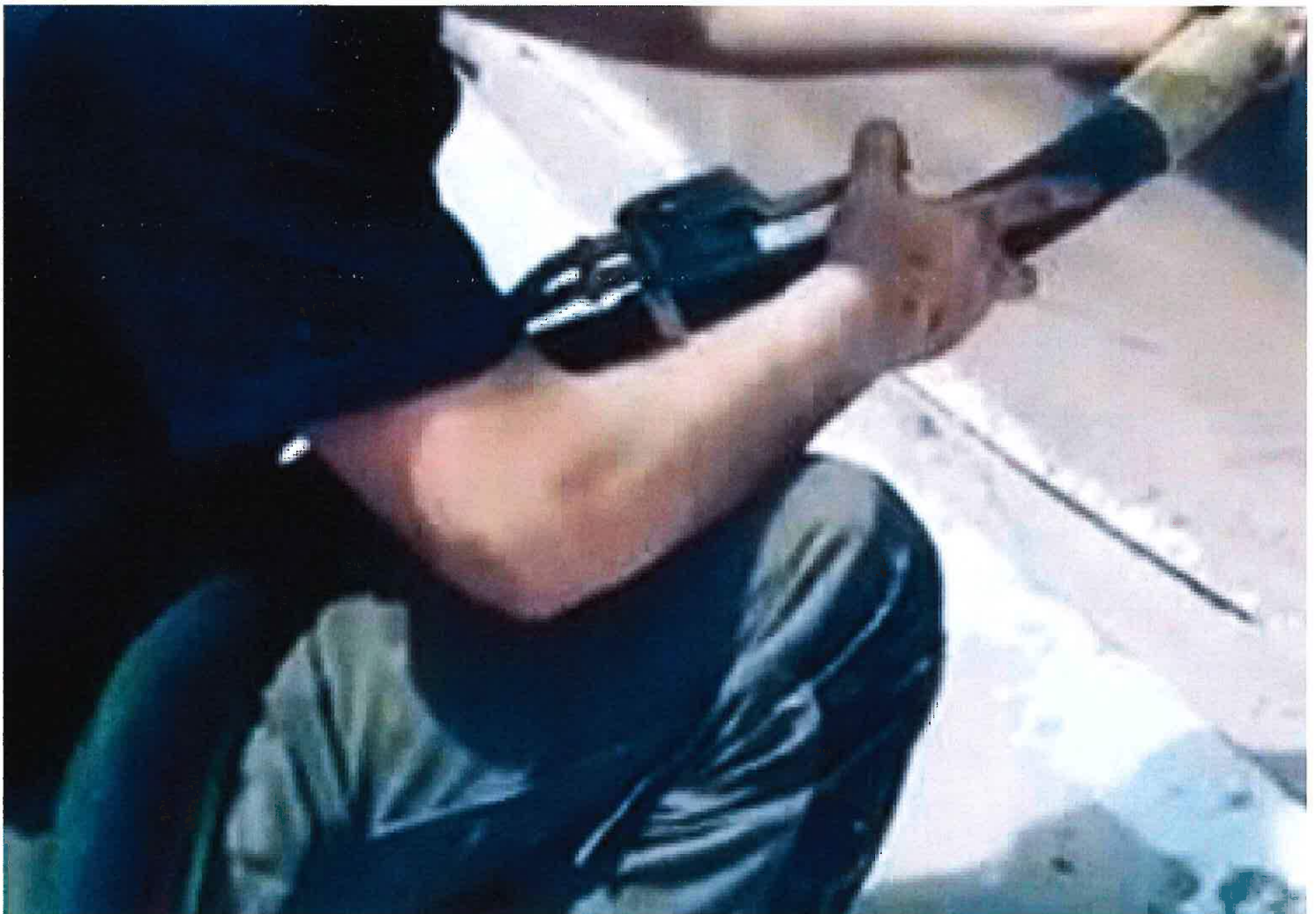
[SEE THE CASE STUDY](#)

“

Please note that actual products may vary from the images shown.

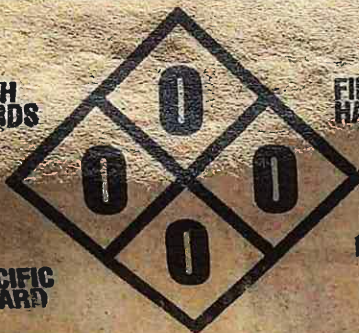
*Your safety is important to us. Videos shown are filmed in a controlled environment for illustrative purposes only. Always follow your operations manual, maintain equipment properly and wear all applicable PPE. Equipment is capable of extreme pressurization. Improper use can result in serious injury or death.*

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**THIS BAG CONTAINS LESS THAN 1% FREE SILICA MATERIALS**

**HEALTH  
HAZARDS**



**FIRE  
HAZARDS**

**SPECIFIC  
HAZARD**

**4 SEVERE  
3 SERIOUS  
2 MODERATE**

**REACTIVITY**

**1 SLIGHT  
0 MINIMAL**

**Contains crushed glass (65997-17-3) 99%-100%**

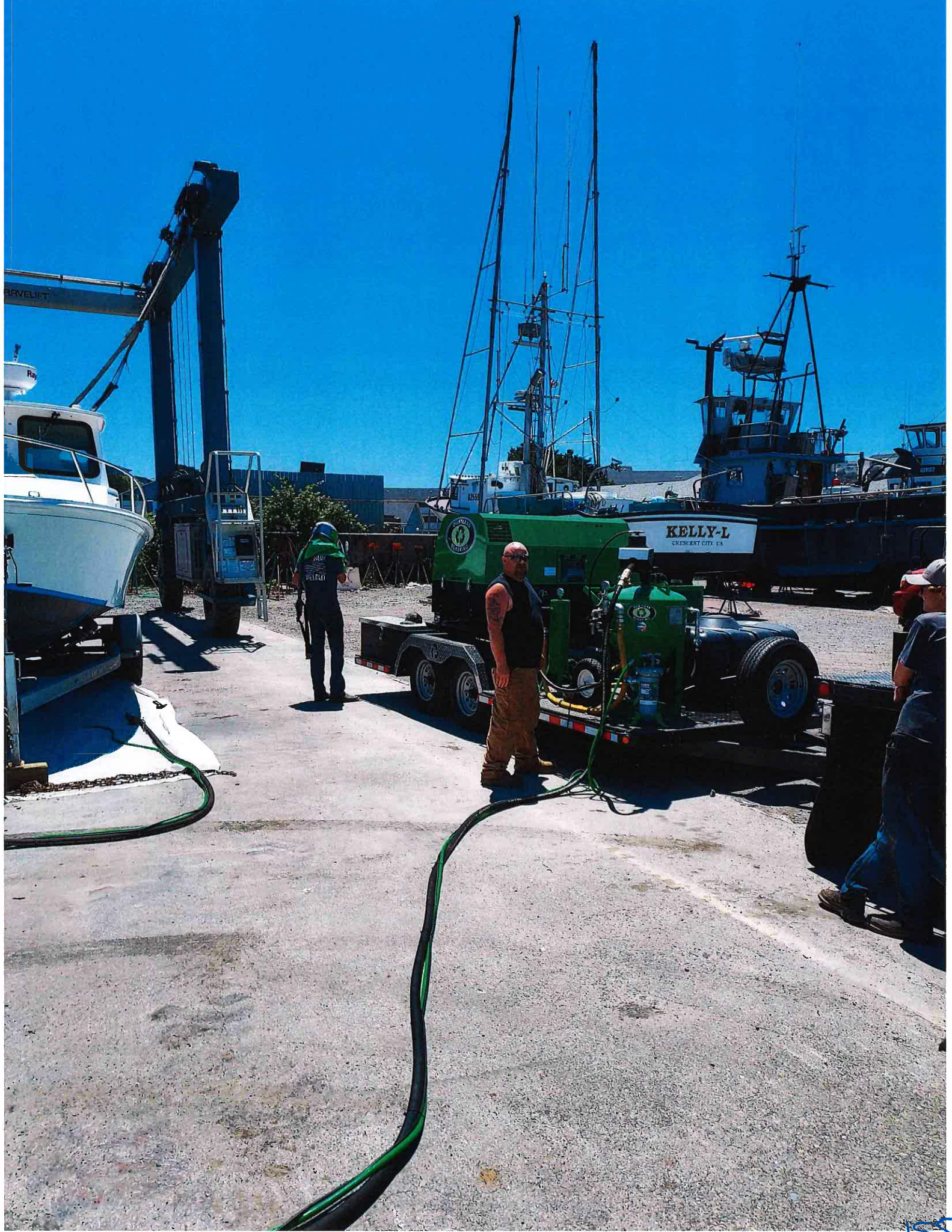
**This product has a low health risk by inhalation. Treat as a nuisance dust. BLACK BEAUTY® GLASS abrasives are low dusting, chemically inert, contain less than 1% free silica, and are not causally linked with any disease including silicosis. This material is not a recognized carcinogen or co-carcinogen. Mechanical irritation may occur if proper protective equipment is not worn.**

**CONSULT CORRESPONDING SDS FOR FURTHER INFORMATION AND INSTRUCTIONS  
FOR EMERGENCY CALL 855-393-9889 ACCESS CODE 13793**









**BMP 1.2 – Scraping and Sandblasting:**

The purpose of this BMP is to minimize the risk of discharge of paint or varnish residues and contaminated blast grit to the environment, either by direct discharge to water, conveyance via storm water or conveyance by air.

- 1) Sandblasting is prohibited.
- 2) Scraping should only be conducted in designated vessel work areas.
- 3) Vessels in gravel work areas must sit on a tarp to prevent dust, paint chips and other materials from accumulation in the soil or sand.
- 4) Hand sanding is allowable, but all power sanders must have a dust collection unit attached.
- 5) Work areas must be kept clean of debris and grit from scraping and sanding operations so that runoff and wind will not carry any waste into the water.
- 6) Tarps and sheeting should be used in the gravel lot to collect debris and spent materials for appropriate disposal. All vessel work areas must be cleaned at the end of each work day.
- 7) As new, environmentally safe products become available which minimize the need for scraping and sanding, their use will be encouraged.
- 8) Scrapings and debris should be stored under cover in a manner that minimizes contact with processed water or storm water. Scrapings may be classed as a special waste or hazardous waste if soluble metals or antifoulant chemicals are present in large amounts.

**BMP 1.3 – Vessel Painting:**

The purpose of this BMP is to minimize the risk of a discharge of paint, solvents, and associated materials to the environment by either airborne or waterborne mechanisms.

- 1) The use of non-toxic, high bonding, easily cleaned hull coatings is encouraged, more alternative coatings are anticipated to become available as the technology advances.
- 2) Painting and varnishing of vessels in water should be generally limited to the interior surfaces and to “bright work”, where paint materials and spills can be contained and prevented from entering the water.
- 3) PAINTING USING AREOSOLS OR SPRAY EQUIPMENT IS PROHIBITED.
- 4) The bottom edges of tarps and plastic sheeting should be weighted to keep them in place.
- 5) Drip pans, tarps, and sheeting should be used to contain droppings and spilled material.
- 6) The mixing of paints and solvents should be carried out in locations and under conditions such that no spill enters State waters.
- 7) Drip pans or other protective devices should be used for all paint mixing, solvent transfer, or equipment cleanup operations unless the operations are conducted in controlled areas away from storm drains, surface waters, shorelines, piers, docks or floats.
- 8) Paint and solvent mixing, brush cleaning and similar activities should not be conducted on open floats or on structures over water, but should be done in an on-shore work area. Paints mixed in a separate work area and transferred to an outdoor work area for

# Sample Information

Sample ID:	22202194	Collectors Name:	Gary Dehlinger
Address of Source:	16330 Lower Harbor Road	Sample Point:	Boat Wash - Before
Project Name:	Port of Brookings Harbor	Source:	Boat Wash
Received Date:	07/08/2022	Treatment System:	None

## Results of Chemical Analysis

Sample Notes:	Boat Wash - Before		Collection Date:		07/08/22 8:14 AM				
Contaminant	Method	LOQ	RESULTS	Units	EPA Limit	Date Analyzed	Analyst	ID	Data Flags
Copper	EPA 200.7	0.006	<b>18.5367</b>	mg/L		07/13/22 12:07 pm	PVS	AA	
Lead	SM 3113 B	0.01	<b>0.0203</b>	mg/L		07/12/22 10:48 am	PVS	AC	
Total Suspended Solids	SM 2540-D	1.0	<b>230.00</b>	mg/L		07/08/22 10:05 am	PVS	AD	
Zinc	EPA 200.7	0.06	<b>1.4976</b>	mg/L		07/13/22 12:07 pm	PVS	AE	
Aluminum	EPA 200.7	0.04	<b>1.3946</b>	mg/L		07/13/22 12:07 pm	PVS	AF	

**DEFINITIONS AND DATA FLAGS**

- |   |  |
|---|--|
| <p>A Analysis is covered under ORELAP scope of Accreditation</p> <p>AA Analysis is covered under ISO scope of Accreditation</p> <p>C Sample did not meet acceptance criteria</p> <p>H Analysis performed outside method hold time</p> <p>ID Subsample identifier for each Sample number</p> <p>M Matrix Spike recovery is out of control limits due to matrix interference<br/>The LCS was in acceptance limits showing the analysis is in control and the data is acceptable</p> | <p>E Estimated Value</p> <p>LOQ Reporting Limit</p> <p>N/A Not Applicable</p> <p>ND None Detected</p> <p>S Sample Outsourced</p> |
|---|--|

**Results Color Key**

**White - No EPA Limit**

**Low Risk  
within EPA Limit**

**Medium Risk**

**High Risk  
Exceeds EPA Limit**

**Call the Lab to Discuss**

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## Sample Information

Sample ID:	22202195	Collectors Name:	Gary Dehlinger
Address of Source:	16330 Lower Harbor Road	Sample Point:	Boat Wash - After
Project Name:	None Provided	Source:	Boat Wash - After
Received Date:	07/08/2022	Treatment System:	None

## Results of Chemical Analysis

Sample Notes:	Boat Wash - After		Collection Date:		07/08/22 8:25 AM				
Contaminant	Method	LOQ	RESULTS	Units	EPA Limit	Date Analyzed	Analyst	ID	Data Flags
Copper	EPA 200.7	0.006	2.7677	mg/L		07/13/22 12:10 pm	PVS	AA	
Lead	SM 3113 B	0.01	ND	mg/L		07/12/22 10:48 am	PVS	AC	
Total Suspended Solids	SM 2540-D	1.0	46.00	mg/L		07/08/22 10:05 am	PVS	AD	
Zinc	EPA 200.7	0.06	1.5218	mg/L		07/13/22 12:10 pm	PVS	AE	
Aluminum	EPA 200.7	0.04	0.5013	mg/L		07/13/22 12:10 pm	PVS	AF	

**DEFINITIONS AND DATA FLAGS**

- |   |  |
|---|--|
| <p>A Analysis is covered under ORELAP scope of Accreditation</p> <p>AA Analysis is covered under ISO scope of Accreditation</p> <p>C Sample did not meet acceptance criteria</p> <p>H Analysis performed outside method hold time</p> <p>ID Subsample identifier for each Sample number</p> <p>M Matrix Spike recovery is out of control limits due to matrix interference<br/>The LCS was in acceptance limits showing the analysis is in control and the data is acceptable</p> | <p>E Estimated Value</p> <p>LOQ Reporting Limit</p> <p>N/A Not Applicable</p> <p>ND None Detected</p> <p>S Sample Outsourced</p> |
|---|--|

**Results Color Key**

White - No EPA Limit

Low Risk  
within EPA Limit

Medium Risk

High Risk  
Exceeds EPA Limit

Call the Lab to Discuss

158

**1200-Z NPDES Monitoring Requirements**

You must monitor for the pollutants in the table below. If discharge to a Category 5: 303(d) listed receiving water for pH, total copper, total lead, total zinc and/or E. coli, the table below will not include statewide or sector-specific benchmarks for those pollutants. Exceedance of impairment monitoring may escalate to a water quality-based effluent limit during this permit cycle. Please read Schedule A.13 and Schedule C carefully. Tier 2 geometric mean evaluations are required annually. Please read Schedule A.12 carefully.

Georegion	Pollutant	Statewide Benchmark	Unit	Frequency
Marine Waters	Total Copper	0.025	mg/L	Four times per year
Marine Waters	Total Lead	1.10	mg/L	Four times per year
Marine Waters	Total Zinc	0.46	mg/L	Four times per year
Marine Waters	pH	6.0 - 9.0	s.u.	Four times per year
Marine Waters	TSS	100	mg/L	Four times per year
SIC code of Industrial Activity	Pollutant	Sector-specific Benchmark	Units	Frequency
4493	Total Aluminum	1.10	mg/L	Four times per year
2092	N/A	N/A	N/A	N/A
<b>Receiving Water</b> LLID: 1242700420450 AUID: 100278 River Mile: 0.15	<b>Pollutant</b>	<b>Impairment Concentration</b>	<b>Units</b>	<b>Frequency</b>
Chetco River	N/A	N/A	N/A	N/A
Technology-based Effluent Limit	Pollutant	Numeric Effluent Limit	Units	Frequency
N/A	N/A	N/A	N/A	N/A

## INFORMATION ITEM – B

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**DATE:** August 17, 2022  
**RE:** Boat Shop & More Lease  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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### OVERVIEW

- The Port received a letter from Kenny Francis requesting to terminate the lease for the boat repair area with the Port before the end of the term. The initial term was three years, November 1, 2020 to October 31, 2023.
- If accepted, the boat repair area would return to Port management and rates would need to be re-established.

### DOCUMENTS

- Boat Shop & More Letter Dated July 26, 2022, 2 pages
- Boat Shop & More Lease, 13 pages

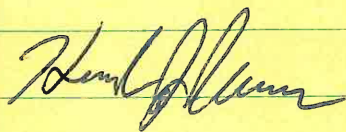


# To The Port of Breeking

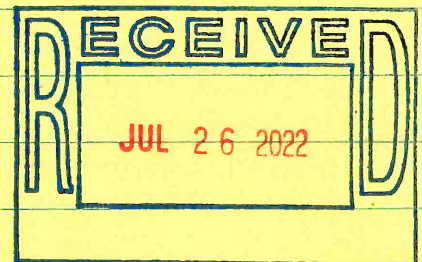
I Kenneth J Francis owner of "Boat Shop and More LLC" would like to Terminate my lease on the Boat Yard only part of my lease.

After running the yard for almost 2 years I have found that there is only \$500 to 600 per month in profits to be made. Unfortunately for me it is not worth the liability to continue.

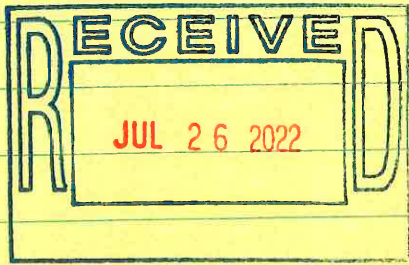
Kenneth J Francis  
16060 Lower Harbor Road  
Breeking, OR 97415  
541 412-7400 office  
541 373-1793 cell



7-26-2022



19987.00 Gross Profit 01/01/2022 to 07/01/2022  
10509.80 Lease 01/01/2022 to 07/01/2022  
1773.43 Power 01/01/2022 to 07/01/2022  
791.00 Insurance 01/01/2022 to 07/01/2022  
761.98 Tarps & Trash 01/01/2022 to 07/01/2022  
692.63 Credit Card Charges :025% 01/01/2022 to 07/01/2022  
1740.00 Yard Labor 01/01/2022 to 07/01/2022 87hr @ 20 per hr  
16268.84 Gross expense  
3718.16 net Profit 01/01/2022 to 07/01/2022



**COMMERCIAL LEASE AGREEMENT  
BOAT SHOP AND MORE, LLC**

This lease agreement is made and entered into at Brookings, Oregon, effective the 1st day of November, 2020, by and between the **Port of Brookings Harbor**, an Oregon special district (referred to herein as the "Landlord") and **Boat Shop and More, LLC** (hereinafter referred to as "Tenant").

1. **Leased Premises.** Landlord hereby leases to Tenant the following described property located in the Port of Brookings Harbor on the terms and conditions stated herein:

- a. Approximately 5,265 square-feet of warehouse-shop building space, (the Leased Premises, as described in Exhibit "A"), located at 16060 Lower Harbor Road, Brookings, Oregon.
- b. Approximately 18,763 square-feet of bare ground space, (the Leased Premises, as described in Exhibit "B"), located at 16060 Lower Harbor Road, Brookings, Oregon.
- c. Approximately 6,995 square-feet of bare ground space, (the Leased Premises, as described in Exhibit "C"), located at 16060 Lower Harbor Road, Brookings, Oregon.

2. **Lease Term and Base Rental Rate.**

a. **Initial Term.** The initial term of this lease is three (3) years commencing November 1, 2020 and continuing through October 31, 2023.

b. **Base Rental Rate.** The base rental rate for the Leased Premises is Three Thousand Three Hundred Seventy-Six and 89/100 Dollars (\$3,376.89) per month, as calculated below, payable on the first day of each month commencing November 1, 2020. The base rental rate is the combined rate of:

1. The warehouse-shop building space consisting of 5,265 square feet of property at a reduced rent due to the condition of the building at approx. \$0.25 per square foot per month, for a total of One Thousand Three Hundred Sixteen and 15/100 Dollars (\$1,316.25) per month.

2. The bare ground space consisting of 18,763 square feet of property for a vessel service area at approx. \$.08 per square foot per month, for a total of One Thousand Five Hundred One and 04/100 Dollars (\$1,501.04) per month.

3. The bare ground space consisting of 6,995 square feet of property for a vessel service or storage area at approx. \$.08 per square foot per month, for a total of Five Hundred Fifty-Nine and 60/100 Dollars (\$559.60) per month.

4. **Reduced Rent for Two Months.** Tenant and the Port will need two months to remove equipment and materials from each warehouse-shop. In consideration of the amount of equipment and materials being relocated, no base rent will be due for the first two months.

5. Boat haul-outs or lifts from Port 50-metric ton travel lift will be billable to the Port per established rates, subject to change from time to time.

c. **Option to Renew.** Upon termination of the initial term of this lease, Landlord grants to Tenant the option to renew this lease in whole or in part of the Leased Premises, for three (3) additional three (3) year term at terms and conditions to be negotiated,

provided that: (a) Tenant is not in default of this lease at the time the option is exercised; (b) Landlord does not need the ground for its own use; and (c) Landlord is otherwise satisfied with Tenant's use of the Leased Premises during the initial term. The parties agree to negotiate in good faith with respect to the renewal terms and conditions on terms at least as favorable as those offered to any other tenant of Landlord at the time.

- d. **Notice of Intent.** Tenant shall notify the Landlord in writing ninety (90) days prior to expiration of the lease of Tenant's intent to exercise all or any portion of Tenant's option to extend the lease. Failure to provide such notice is a default and a material breach of the lease and Landlord may terminate the lease on the expiration date and retake possession of the Leased Premises with or without process of law.

### 3. Base Rent Payment.

- a. **Annual Adjustment.** Tenant must pay the base rent for the Leased Premises and any additional rent provided herein without deduction or offset. The base rent will increase annually, on each anniversary of the lease commencement for the second and each subsequent year, according to the Consumer Price Index for All Urban Consumers (CPI-U). The base rent increase will be for the total amount of the base rent due. Base rent includes all prior percentage increases. In the event that the CPI-U is negative, the base rent will remain the same, it will not increase or decrease.
- b. **Proration.** Rent for any partial month during the lease term will be prorated to reflect the number of days during the month that Tenant actually occupied the Leased Premises.
- c. **Additional Rent.** Additional rent means any other sums payable by Tenant to Landlord under this lease. At the end of the initial lease term, a new base rent will be established.
- d. **Fees and Charges.** Should any rent or other payment required of Tenant by this lease not be paid within 10 days after it is due, a late charge of 1.5% per month (18% per annum) will be assessed. In the event any suit or action is instituted to collect any amount owed on this account, the undersigned applicant agrees to pay any reasonable attorney's fees, collection agency fees and any other costs associated with such action. A \$50.00 fee will be assessed on any returned payment.

- 4. **Lease Consideration/Security Deposit.** Upon execution of the lease, Tenant's base rent is due the first day of the month of the lease term for which rent is payable. Tenant is required to pay a security deposit in a sum equal to one month's base rent. Landlord may apply the security deposit to pay the cost of performing any obligation that Tenant fails to perform within the time required by this lease, but such application by Landlord shall not be the exclusive remedy for Tenant's default. If the security deposit is applied by the Landlord, Tenant must on demand pay the sum necessary to replenish the security deposit to its original amount. To the extent not applied by Landlord to cure defaults by Tenant, the security deposit will be returned to Tenant upon termination of this lease, or, by mutual agreement between Landlord and Tenant, applied against the rent payable for the last month of the term.

- 5. **Use.** Tenant may use the Leased Premises for boat services, repairs or refurbishing (excluding sand or metal blasting and spray painting) and boat storage for seaworthy boats and specialty auto work and for no other purpose without Landlord's written consent. In connection with its use of the Leased Premises, Tenant must, at its sole expense, promptly comply with all applicable laws, ordinances, rules and regulations of any public authority, including those of the Port of Brookings Harbor, and not unreasonably annoy, obstruct or interfere with the rights of other tenants of the Port of Brookings Harbor, wherever located. Tenant must not create or maintain any nuisance or any objectionable fumes, noise, or vibrations while using the Leased Premises.

6. **Equipment.** Tenant may install in the Leased Premises only such equipment as is customary for the intended *use* and must not overload the floors or electrical circuits of the Leased Premises or alter the plumbing or wiring of the Leased Premises, without the prior written consent of Landlord. Landlord must approve, in advance, the location and manner of installing any electrical, heat generating or communication equipment or exceptionally heavy articles. Any equipment installed by Tenant will remain Tenant's property and must be installed and operated at Tenant's expense. Any air conditioning required because of heat generating equipment or special lighting installed by Tenant must also be installed and operated at Tenant's expense.
7. **Sign.** No signs, awnings, antennas, or other apparatus may be positioned as to be visible from outside the Leased Premises without Tenant obtaining Landlord's prior written approval as to design, size, location, and color. All signs installed by Tenant must comply with Landlord's standards for signs, and all applicable codes and signs and sign hardware must be removed upon termination of this lease with the sign location restored to its former state unless Landlord elects to retain all or any portion thereof.
8. **Utilities and Services.** Landlord will furnish all utilities up to the Leased Premises and Tenant will be directly responsible for any and all electrical charges or fees for electrical service and must make arrangements to be billed directly from the local electric co-op (Coos-Curry Electric Cooperative, Inc.). Tenant must also make the necessary arrangements to have a meter installed in the name of Tenant for billing purposes. Water and Sewer usage will be billed separately. Tenant must comply with all government laws or regulations regarding the use or reduction of use of utilities on the Leased Premises. Tenant is responsible for all waste generated by the business and disposal of the waste. Unless caused by Landlord's negligence or intentional act, the interruption, limitation, curtailment, or rationing of services or utilities may not be deemed an eviction or disturbance of Tenant's use and possession of the Leased Premises, render Landlord liable to Tenant for damages, or relieve Tenant from performance of Tenant's obligations under this lease. Landlord must take all reasonable steps to correct any interruption in service.
9. **Maintenance and Repair – Tenant's Obligations**
- a. Tenant is at all times during the term of this lease, and at Tenant's sole cost and expense, obligated to keep the entire of the Leased Premises and every part thereof in good condition and repair; excepting ordinary wear and tear and damage to the Leased Premises by earthquake, act of God, or the elements. Landlord has no obligation and has made no promise to alter, remodel, improve, repair, decorate, or paint the Leased Premises or any part thereof. Landlord does have the right to erect scaffolding and other apparatus necessary for the purpose of making repairs, and Landlord will have no liability for interference with Tenant's use because of repairs and installations. Tenant will have no claim against Landlord for any interruption or reduction of services or interference with Tenant's occupancy, and no such interruption or reduction shall be construed as a constructive or other eviction of Tenant.
- b. Tenant will be responsible for any repairs necessitated by Tenant's breach of this lease or the negligent or intentional acts of Tenant, its agents, employees, and invitees, excepting repairs that would otherwise be the responsibility of Landlord under Section 10 or Section 15.
- c. Tenant is responsible for all other repairs to the Leased Premises which Landlord is not required to make under Section 10 or Section 15.

- d. If Tenant fails to perform Tenant's obligations under this Section 9 or under any other Section of this lease, Landlord may enter upon the affected portion of the Leased Premises after ten (10) days' prior written notice to Tenant (except in case of emergency, in which no notice shall be required), perform such obligations on Tenant's behalf and put the Leased Premises in good order, condition and repair, and the cost thereof together with interest thereon at the maximum rate then allowable by law will be due and payable as additional rent to Landlord together with Tenant's next base rent installment.
- e. On the last day of the term hereof, or upon any sooner termination, Tenant must surrender the Leased Premises to Landlord in the same condition as received, ordinary wear and tear excepted, clean and free of debris. Any damage or deterioration of the Leased Premises will not be deemed ordinary wear and tear if the same could have been prevented by commercially reasonable maintenance practices. Tenant shall leave the air-lines, power panels, electrical distribution systems, lighting fixtures, space heaters, air conditioning, plumbing and fencing which were on the Leased Premises prior to the commencement of the lease, in good operating condition.

**10. Maintenance and Repair - Landlord's Obligations.** The following will be the responsibility of Landlord:

- a. Provide adequate means of ingress and egress to the Leased Premises.
- b. Provide access to a water supply and electricity.
- c. Repair and maintenance of existing exterior water, sewage, and electrical services up the point of entry to the Leased Premises.
- d. Repair and maintain any structural element of the building that does not meet the definition of Major Damage as provided in Section 15, with respect to the Leased Premises.

**11. Alterations.** Tenant must not make any alterations, additions, or improvements to the Leased Premises without Landlord's prior written consent. Any such additions, alterations, or improvements, except for removable machinery and trade fixtures, will at once become part of the realty and belong to the Landlord. Landlord may at its option require that Tenant remove any alterations and restore the Leased Premises to the original condition upon termination of this lease. Landlord will have the right to approve the contractor used by Tenant for any work on the Leased Premises, and to post notices of non-responsibility in connection with any work being performed by Tenant in the Leased Premises.

**12. Indemnity.**

- a. Tenant may not allow any liens to attach to the Leased Premises or Tenant's interest in the Leased Premises as a result of its activities. In the event that a materialman, mechanic's, or other lien is filed, or a claim of lien is made for work claimed to have been done for Tenant, Landlord will have the option in its sole discretion to require Tenant to post a Surety Bond within ten (10) days at Tenant's expense or to pay and discharge the lien. Tenant agrees to reimburse Landlord promptly upon demand. These Landlord remedies are not exclusive as Landlord has other remedies as provided by law including requiring Tenant to pay for Landlord's attorney's fees and costs relating to any such lien.
- b. Except as otherwise stated herein, Tenant hereby waives all claims against Landlord for damage to any property or injury, illness, or death of any person in, upon, or about the Leased Premises arising at any time and from any cause whatsoever other than by reason of the willful act of Landlord, its officers, employees, invitees, licensees or agents. Tenant must defend, indemnify and hold Landlord harmless from any and all

claims or liability for damage to any property or injury, illness, or death of any person (a) occurring in or on the Leased Premises or any part thereof arising at any time and from any cause whatsoever other than by reason of the willful act of Landlord, its officers, employees, invitees, licensees or agents; or (b) occurring in, on, or about any part of the Leased Premises when such damage, injury, illness, or death was caused by the act, negligence, omission, or fault of Tenant, its agents, servants, employees, invitees, or licensees. Except as otherwise stated herein, Landlord will have no liability to Tenant because of loss or damage caused by the acts or omissions of other tenants of Landlord, or by third parties. The provisions of this paragraph will survive the termination of this lease with respect to any damage, injury, illness, or death occurring prior to such termination.

13. **Insurance.** During the initial term of this lease and any extension thereof, Tenant must comply with the following insurance requirements:
- a. **General Liability.** Tenant must carry commercial general liability insurance at least as broad as ISO Form CG 00 01 covering CGL on an "occurrence" basis, including products and completed operations, property damage, bodily injury and personal and advertising injury with limits no less than \$2 million per occurrence. If a general aggregate limit applies, either the general aggregate limit shall apply separately to this project location (ISO CG 25 03 or 25 04) or the general aggregate limit shall be twice the required occurrence limit.
  - b. **Property.** Tenant must carry property insurance against all risk of loss to any tenant improvement or betterments, at full replacement cost with no coinsurance penalty provision.
  - c. **Workers' Compensation.** If Tenant has employees, Tenant must carry workers' compensation insurance as required by State law and Employer's Liability Insurance with limits of no less than \$1 million per accident for bodily injury or disease.
  - d. **Excess Coverage.** If Tenant maintains broader coverage and/or higher limits than the minimums shown above, Landlord will be entitled to the broader coverage and/or the higher limits maintained by Tenant. Any available insurance proceeds in excess of the specified minimum limits of insurance and coverage will be available to Landlord.
  - e. **Additional Insureds.** The Port of Brookings Harbor, its officers, officials, employees, and agents are to be covered as additional insureds on the CGL policy with respect to liability arising out of work or operations performed by or on behalf of the Tenant including materials, parts, or equipment furnished in connection with such work or operations. General liability coverage can be provide in the form of an endorsement of the lessee's insurance (at least as broad as ISO Form CG 20 10).
  - f. **Certificates of Coverage.** Tenant must furnish certificates of insurance to Port's General Manager, P.O. Box 848, Brookings, Oregon 97415 certifying the existence of such insurance no later than five (5) days prior to commencement of this lease. Each insurance policy required by this clause must be endorsed to state that coverage will not be suspended, voided, canceled, or reduced in coverage or limits or not renewed without fourteen (14) days advance written notice to the Landlord and Landlord's agent, if any, and a renewal certificate must be furnished at least 14 days prior to the expiration of any policy.
  - g. **Primary Insurance.** The insurance required herein will be primary and without right of contribution from other insurance that may be in effect and without subordination. Any other insurance carried by the Landlord is excess. The insurance policies must be underwritten by a company licensed in the state of Oregon, and carry a minimum Best's rating of "A-VI" or better.
  - h. **Lapse of Policy.** If Tenant's policies lapse or are canceled at any time during the term of this Contract, Landlord will have the right to immediately terminate Tenant's lease until such insurance requirements have been fully satisfied by Tenant. Tenant will be responsible to

Landlord, and must reimburse and hold Landlord harmless for any bodily injury, fire or property damage not covered by Tenant's insurance.

14. **Exemption of Landlord from Liability.** Tenant hereby agrees that Landlord will not be liable for injury to Tenant's business or any loss of income therefrom or for damage to the goods, wares, merchandise or other property of Tenant, Tenant's employees, invites, customers, or any other person in or about the Leased Premises or the Port, nor will Landlord be liable for injury to the person of Tenant, Tenant's employees, agents or contractors, whether such damage or injury is caused by or results from fire, steam, electricity, gas, water or rain, or from the breakage, leakage, obstruction or other defects of pipes, wires or lighting fixtures, or from any other cause, whether said damage or injury results from conditions arising upon the Leased Premises or upon other premises of the Port, or from other sources or places and regardless of whether the cause of such damage or injury or the means of repairing the same is inaccessible to Tenant. Landlord will not be liable for any damages arising from any act or neglect of any other tenant, occupant or user of the Port, nor from the failure of Landlord to enforce the provisions of any other lease of the Port.
15. **Major Damage.** Major damage means damage by fire or other casualty to the Leased Premises that causes the Leased Premises or any substantial portion of the Leased Premises to be unusable. In the event that major damage occurs without negligence or willful misconduct of Tenant or its employees, agents, or licensees, then either Landlord or Tenant may elect to terminate this lease by providing written notice to the other party within thirty (30) days after the occurrence of the damage. If this lease is not terminated following major damage, or if damage occurs that is not major damage, Landlord must promptly restore the Leased Premises to the condition existing just prior to the damage, with the exception of damage to Tenant improvements. Restoration of any Tenant improvements or alterations installed by Tenant, and the costs thereof, will be the responsibility of the Tenant. Rent will be reduced from the date of damage until the date restoration work being performed by the Landlord is substantially complete, with the reduction to be in proportion to the area of the Leased Premises not useable by Tenant.
16. **Waiver of Subrogation.** Tenant will be responsible for insuring its personal property and trade fixtures located on the Premises and any alterations or Tenant improvements it has made to the Premises. Neither Landlord nor Tenant will be liable to the other for any loss or damage caused by any of the risks that are or could be covered by a standard all risk insurance policy with the extended coverage endorsement, or for any business interruption. There may be no subrogated claims by one party's insurance carrier against the other party arising out of any loss.
17. **Eminent Domain.** If a condemning authority takes title by eminent domain or by agreement in lieu thereof to the entire Leased Premises or a portion sufficient to render the Leased Premises unsuitable for Tenant's use, then either party may elect to terminate this lease effective on the date that possession is taken by the condemning authority; provided, however, that a condition to the exercise by Tenant of such right to terminate will be that the portion of the Leased Premises taken must be of such extent and nature as to substantially handicap, impede, or impair Tenant's use of the balance of the Leased Premises for the purpose intended. Rent will be reduced for the remainder of the term in an amount proportionate to the reduction in area of the Leased Premises caused by the taking. All condemnation proceeds will belong to Landlord, and Tenant will have no claims against Landlord or the condemnation award because of the taking.
18. **Assignment and Subletting.** This lease binds and inures to the benefit of the parties, their respective heirs, successors, and assigns, provided that Tenant may not assign its interest



under this lease or sublet all or any portion of the Leased Premises without first obtaining Landlord's consent in writing. This provision applies to all transfers by operation of law including but not limited to mergers and changes in control of Tenant. No assignment may relieve Tenant of its obligation to pay rent or perform other obligations required by this lease and no consent to one assignment or subletting may be deemed consent to any further assignment or subletting. Landlord may not unreasonably withhold or delay its consent to any assignment, or to subletting, accepting that the proposed Tenant has been approved by Landlord in writing. Tenant will pay any costs incurred by Landlord in connection with a request for assignment or subletting, including reasonable attorney's fees.

**19. Default.**

a. Any of the following constitute a default by Tenant under this lease:

1. Tenant's failure to pay rent or any other charge under this lease within ten (10) days after it is due, or failure to comply with any other term or condition within twenty (20) days following written notice from Landlord specifying the noncompliance. If such noncompliance cannot be cured within the 20-day period, this provision will be satisfied if Tenant commences corrective action within such period and thereafter proceeds in good faith and with reasonable diligence to effect compliance as soon as possible. Time is of the essence of this lease.
2. Tenant's insolvency, business failure or assignment for the benefit of its creditors. Tenant's commencement of proceedings under any provision of any bankruptcy or insolvency law or failure to obtain dismissal of any petition filed against it under such laws within the time required to answer, or the appointment of a receiver for Tenant's property.
3. Assignment or subletting by Tenant in violation of this lease.
4. Vacation or abandonment of the Leased Premises for more than three (3) months without the written consent of Landlord.
5. If this lease is levied upon under any attachment or execution and such attachment or execution is not vacated within ten (10) days.

**20. Remedies for Default.** In case of default as described in Section 19 above, Landlord will have the right to the following remedies, which are intended to be cumulative and in addition to any other remedies provided under applicable law.

- a. Landlord may terminate the lease and reenter, retake possession of the Leased Premises, and remove any persons or property by legal action or by self-help with the use of reasonable force and without liability for damages. Following such retaking of possession, efforts by Landlord to relet the Leased Premises will be sufficient if Landlord follows its usual procedures for finding tenants for the Leased Premises at rates not less than the current rates for other comparable space on Port property. If Landlord has other vacant space available, prospective tenants may be placed in such other space without prejudice to Landlord's claim to damages to loss of rentals from Tenant.
- b. Landlord may recover all damages caused by Tenant's default, which include an amount equal to rent lost because of the default and all attorney's fees and costs. Landlord may sue periodically to recover damages as they occur throughout the lease term, and no action for accrued damages will bar a later action for damages subsequently accruing. Landlord may elect in any one action to recover accrued damages plus damages attributable through the remaining term of the lease. Such damages will be measured by the difference between the rent under this lease and the reasonable rental value of

the Leased Premises for the remainder of the term, discounted to the time of judgment at the prevailing interest rate on judgments.

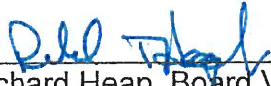


- c. Landlord may make any payment or perform any obligation that Tenant has failed to perform, in which case Landlord will be entitled to recover from Tenant upon all demand all amounts so expended plus interest from the date of the expenditure at the rate of one and one-half percent (1.5%) per month. Any such payment or performance by Landlord will not waive Tenant's default.
21. **Regulations.** Landlord will have the right (but not the obligation) to make, revise, and enforce commercially reasonable regulations or policies consistent with this lease for the purpose of promoting safety, order, economy, cleanliness, and good service to all tenants of the Landlord, provided that if Landlord passes a regulation or policy that interferes with Tenant's quiet enjoyment or unreasonably interferes with Tenant's use of the Leased Premises, then Tenant may terminate this lease. All such regulations and policies must be complied with as if part of this lease.
22. **Access.** During times, other than normal business hours, Tenant's officers and employees or those having business with Tenant may be required to identify themselves or show passes in order to gain access to the Leased Premises. In such event, Landlord will have no liability for permitting or refusing to permit access to anyone. With reasonable notice to Tenant, Landlord will have the right to enter upon the Leased Premises at any time by passkey or otherwise to determine Tenant's compliance with this lease, to perform necessary services, maintenance and repairs to the Leased Premises, or to show the Leased Premises to any prospective tenant or purchasers. Except in cases of emergency, such entry will be with at least 24 hours prior notice and at such times and in such manner as to minimize interference with the reasonable business use of the Leased Premises by Tenant.
23. **Notices.** Notices to the parties relating to the lease must be in writing, effective when delivered, or if mailed, effective on the second day following mailing, postage prepaid, to the address for the party stated in this lease or to such other address as either party may specify by notice to the other. Notice to Tenant may always be delivered to the Leased Premises. Rent will be payable to Landlord at the same address and in the same manner, but will be considered paid only when received.
24. **Subordination.** This lease will be subject and subordinate to any mortgages, deeds of trust, or land sale contracts (hereafter collectively referred to as encumbrances) now existing against the Leased Premises. At Landlord's option this lease will be subject and subordinate to any future encumbrance hereafter placed against the Leased Premises (including the underlying land) or any modifications of existing encumbrances. Tenant must execute such documents as may reasonably be requested by Landlord or the holder of the encumbrance to evidence this subordination.
25. **Transfer of Premises.** If the Leased Premises is sold or otherwise transferred by Landlord or any successor, Tenant will attorn to the purchaser or transferee and recognize it as the landlord under this lease, and, provided the purchaser assumes all obligations hereunder, the Landlord (transferor) will have no further liability hereunder.
26. **Estoppel.** Either party will within twenty (20) days after notice from the other party execute, acknowledge and deliver to the other party a certificate reciting: whether or not this lease has been modified and is in full force and effect; whether there are any modifications or alleged breaches by the other party; the dates to which rent has been paid in advance, and the amount of any security deposit or prepaid rent; and any other facts that may be reasonably requested.

Failure to deliver the certificate within the specified time will be conclusive upon the party of whom the certificate was requested that the lease is in full force and effect and has not been modified except as may be represented by the party requesting the certificate. If requested by the holder of any encumbrance or any ground lessor, Tenant will agree to give such holder or lessor notice of and an opportunity to cure any default by Landlord under this lease.

27. **Attorney's Fees.** In the event, any action, suit, or other proceeding is instituted by either party to this lease to enforce any provision of this lease or any matter arising therefrom or to interpret any provision of this lease, the prevailing party will be entitled to an award of reasonable attorney's fees and costs of suit, including expert witness fees. In the event, any such action, suit, or other proceeding is appealed to any higher court or courts, the prevailing party will be entitled to an award of reasonable attorney's fees and costs for prosecuting or defending such appeal or appeals, in addition to the reasonable attorney's fees and costs in the lower court, or courts.
28. **Quiet Enjoyment.** Landlord warrants that so long as Tenant complies with all material terms of this lease, Tenant is entitled to peaceable and undisturbed possession of the Leased Premises free from any eviction or disturbance by Landlord. Landlord will have no liability to Tenant for loss or damages arising out of the acts of other tenants of Port property or third parties, nor any liability for any reason which exceeds the value of its interest in the Leased Premises.
29. **Complete Agreement.** This lease and the attached exhibits constitute the entire agreement of the parties and supersede all prior written and oral agreements and representations. Neither Landlord nor Tenant is relying on any representations other than those expressly set forth herein. Any modification to this lease must be in writing and signed by both parties.
30. **Nonwaiver.** Waiver by either party of strict performance of any provision of this lease may not be deemed a waiver of or prejudice of the party's right to require strict performance of the same provision in the future or of any other provision.
31. **Real Property Taxes.**
  - a. **Payment of Taxes.** Tenant must pay all real and personal real property taxes, if any, applicable to Tenant's portion of the use and possession of the Leased Premises.
  - b. **Additional Improvements.** Tenant will be responsible for paying any increase in real property tax specified in the Tax Assessor's records and work sheets caused by additional improvements placed upon the Leased Premises by Tenant or by Landlord for use by Tenant.
  - c. **Definition of "Real Property Tax".** As used herein, the term "real property tax" includes any form of real estate tax or assessment, general, special, ordinary or extraordinary, and any license fee, commercial rental tax, improvement bond or bonds, levy or tax (other than inheritance, personal income or estate taxes) imposed on the Port or any portion thereof by any authority having the direct or indirect power to tax, including any city, county, state or federal government, or any school, agricultural, sanitary, fire, street, drainage or other improvement district thereof.
32. **Severability.** The invalidity of any provision of this lease as determined by a court of competent jurisdiction, may in no way affect the validity of any other provisions herein.
33. **Time of Essence.** Time is of the essence with respect to the obligations to be performed under this lease.

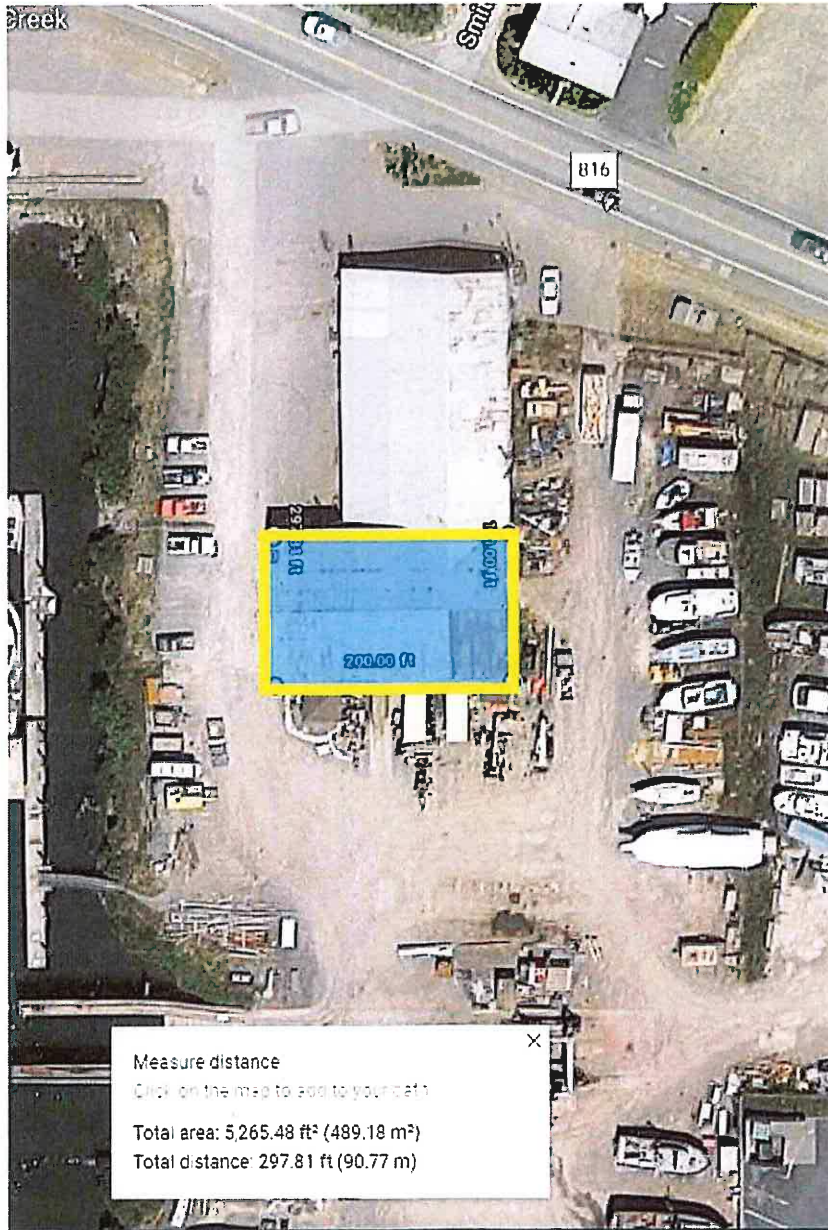
- 34. **Security Measures.** Each party acknowledges that they have no obligation whatsoever to provide guard service or other security measures for the benefit of the other party or their property. Each party assumes full responsibility for the protection of itself, its agents and invitees and its property from acts of third parties. Nothing herein contained prevents Landlord, at Landlord's sole option from providing security protection for the Port or any part thereof.
- 35. **No Warranties.** The Leased Premises are leased "as-is" and in their current condition as of the first day of the lease term. No warranties, express or implied, are provided by Landlord regarding the condition or fitness for purpose of the Leased Premises.
- 36. **Parking.** Landlord does not assign any specific parking spaces to Tenant under this lease. Tenant and Tenant's employees and invitees are permitted to use any un-restricted Port public parking areas.
- 37. **Headings.** The headings in this lease are for the convenience of the parties only and are not to be used in the interpretation of its provisions.

IN WITNESS, WHEREOF, the duly authorized representatives of the parties have executed this lease as of the last date written below.

<b>PORT OF BROOKINGS HARBOR, Landlord</b>	<b>BOAT SHOP AND MORE, LLC Tenant</b>
Dated: <u>10-21-2020</u>	Dated: <u>10-21-2020</u>
By: <u></u> Richard Heap, Board Vice-President	By: <u></u> Name: Kenneth Francis Its: Owner
ATTEST: <u></u> Sharon Hartung, Board Secretary / Treasurer	
Mailing Address: P.O. Box 848 Brookings, OR 97415  Phone: 541-469-2218	Mailing Address: <u>98125 North Bank Chatoe River Rd</u> <u>Brookings OR 97415</u> Phone: 541-373-1793

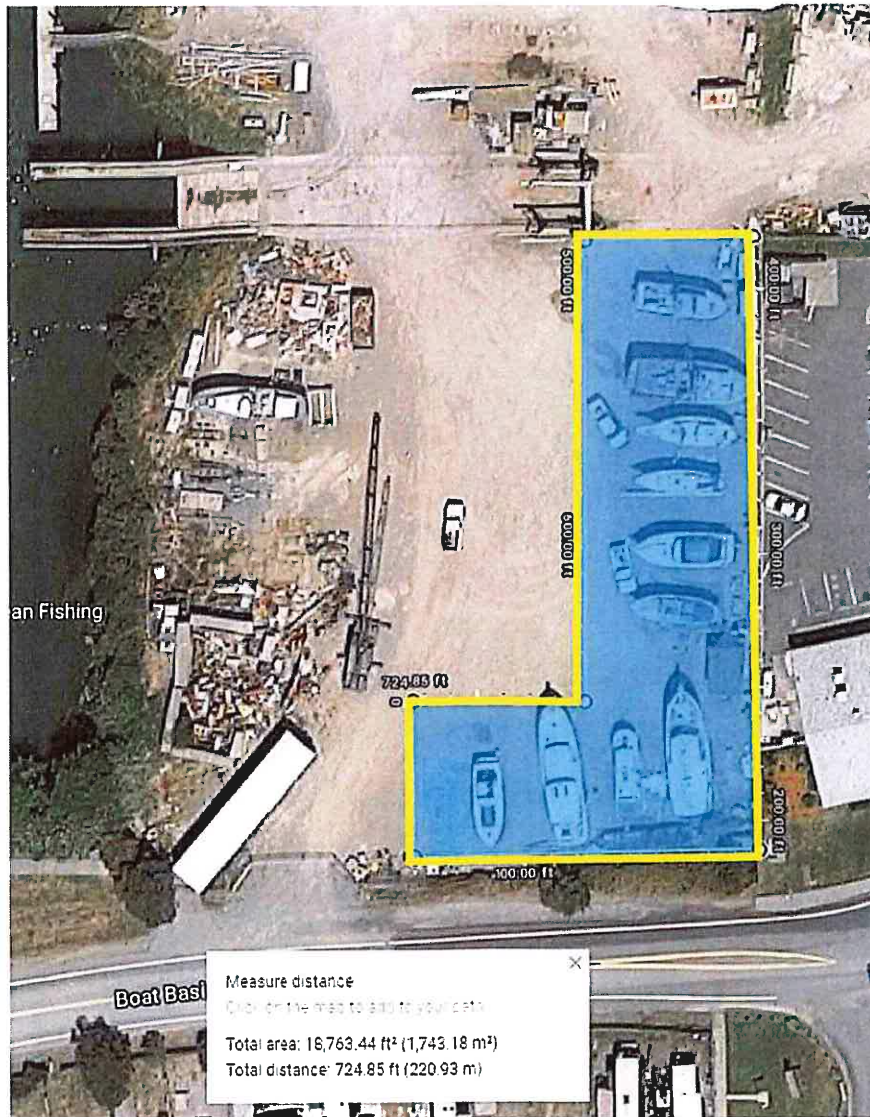
# Port of Brookings Harbor Exhibit A

Warehouse-Shop Area at 16060 Lower Harbor Road = 5,265 square feet.



# Port of Brookings Harbor Exhibit B

Boat Service Area at 16060 Lower Harbor Road = 18,763 square feet.



# Port of Brookings Harbor Exhibit C

Boat Service / Storage Area at 16060 Lower Harbor Road = 6,995 square feet.



## INFORMATION ITEM – C

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**DATE:** August 17, 2022  
**RE:** USFWS Completes Sea Otter Study, Outlines Next Steps  
**TO:** Honorable Board President and Harbor District Board Members  
**ISSUED BY:**

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### OVERVIEW

- Port received information from Jodie Delavan, Public Affairs Officer, USFWS – Oregon Fish and Wildlife Office regarding the Sea Otter Study.

### DOCUMENTS

- Email from Jodie Delavan, Public Affairs Officer, USFWS – Oregon Fish and Wildlife Office, 3 pages



**From:** Port Office <info@portofbrookingsharbor.com>  
**Sent:** Wednesday, July 27, 2022 9:47 AM  
**To:** Gary Dehlinger  
**Subject:** Fwd: Subject: FYI: USFWS Completes Sea Otter Study, Outlines Next Steps

----- Forwarded message -----

**From:** Delavan, Jodie <jodie\_delavan@fws.gov>  
**Date:** Wed, Jul 27, 2022 at 9:34 AM  
**Subject:** Subject: FYI: USFWS Completes Sea Otter Study, Outlines Next Steps  
**To:**

Good morning,

Under a directive from Congress, the U.S. Fish and Wildlife Service evaluated the feasibility of reintroducing sea otters to their historical range along the West Coast of the contiguous United States. The Service focused the assessment on northern California and Oregon, where potential sea otter reintroduction would have the greatest conservation value. The assessment is available to the public and partners, today [at this link](#).

In the assessment, the Service concludes that a reintroduction of sea otters to their historical range in northern California and Oregon would provide substantial benefits to the species. As a keystone species, sea otters play a fundamental role in the ecological health of nearshore ecosystems. Sea otters eat sea urchins and other marine grazers, which helps keep kelp forests and seagrass beds in balance. Their presence in the ocean enhances biodiversity, increases carbon sequestration by kelp and seagrass, and makes the ecosystem more resilient to the effects of climate change. Additionally, reintroduction could increase the genetic diversity of recovering sea otter populations and contribute to the conservation of the threatened southern sea otter.

There is no specific proposal or timeline for any reintroductions at this time. Having concluded that the reintroduction of sea otters is feasible and provides significant conservation benefits to the species and the ecosystem, however, if consideration of reintroduction continues, the Service recommends the following next steps:

- Convene a series of structured decision-making workshops to engage key stakeholders and explore reintroduction options that might present an acceptable level of risk to all parties and increase the likelihood of success.
- Develop criteria for selection and evaluation of potential reintroduction sites; criteria should include consideration of socioeconomic and biological information.
- Initiate a targeted socioeconomic impact study that considers the full range of both negative and positive effects of sea otter reintroduction, including ecosystem services and other nonmonetary values, once potential reintroduction sites are identified.
- Develop plans for pilot studies, small-scale experimental reintroductions, or both to assess the viability of using surrogate-reared southern sea otter pups or wild captured sea otters.
- Integrate population growth and expansion models for California and Oregon to forecast outcomes assuming interactions between reintroduced populations.

If there is a move to formally propose the reintroduction of sea otters, the Service would initiate a public review process under the National Environmental Policy Act. For more information, please see a copy of the press release with Frequently Asked Questions: <https://www.fws.gov/press-release/2022-07/fish-and-wildlife-service-completes-sea-otter-study-outlines-next-steps>. The news release is also included below.

Please do not hesitate to reach out if you have any additional questions.

Best,  
Jodie

**Jodie Delavan**

Public Affairs Officer

USFWS - Oregon Fish and Wildlife Office

2600 SE 98th Ave., Ste. 100

Portland, Oregon 97266

Phone: (503) 231-6984

Website: <https://fws.gov/office/oregon-fish-and-wildlife>

Connect with us on [Facebook](#)

## **Fish and Wildlife Service Completes Sea Otter Study, Outlines Next Steps**

PORTLAND, Oregon – Under a directive from Congress, the U.S. Fish and Wildlife Service evaluated the feasibility of reintroducing sea otters to their historical range along the West Coast of the contiguous United States. The Service focused the assessment on northern California and Oregon, where potential sea otter reintroduction would have the greatest conservation value.

The Service’s assessment indicates reintroduction is feasible, but it does not provide a recommendation as to whether sea otter reintroduction should take place. Additional information and stakeholder input would be needed to help inform any future reintroduction proposal if the initiative moves forward.

Sea otters once lived across the north Pacific Rim from the northern islands of Japan to Baja California. By 1911, this heavily hunted species was nearly extinct and survived in only a few small disjunct populations. After slow population recovery and past reintroduction efforts, sea otters now again inhabit portions of their historical range. However, sea otters remain absent from the contiguous Pacific Coast, with the exception of central California, one island in southern California, and the northern coast of Washington. Sea otters in California are listed as threatened under the Endangered Species Act.

As a keystone species, sea otters play a fundamental role in the ecological health of nearshore ecosystems. Sea otters eat sea urchins and other marine grazers, which helps keep kelp forests and seagrass beds in balance. Their presence in the ocean enhances biodiversity, increases carbon sequestration by kelp and seagrass, and makes the ecosystem more resilient to the effects of climate change. Additionally, reintroduction could increase the genetic diversity of recovering sea otter populations and contribute to the conservation of the threatened southern sea otter.

“If sea otters are reintroduced to northern California and Oregon, it would benefit both otters and the nearshore marine ecosystem,” said Craig Rowland, acting state supervisor for the Service’s Oregon office. “Additional work is needed to evaluate the possible impacts of a potential reintroduction as well as measures to offset these

impacts. While we anticipate an overall socioeconomic benefit to coastal communities, we also recognize that some local shellfish fisheries could be affected.”

In the consideration of sea otter reintroduction, the Service’s assessment recommends stakeholder engagement in the identification of potential reintroduction sites and small-scale experimental reintroductions as next steps. If there is a move to formally propose the reintroduction of sea otters, the Service would initiate a public review process under the National Environmental Policy Act.

The Elakha Alliance, an Oregon nonprofit group, separately evaluated the feasibility of bringing sea otters back to the Oregon Coast. The Service partly funded this study. The feasibility assessment by the Service is intended to be read as a companion to the Elakha Alliance study, but it expands the geographic area under consideration in response to the request from Congress.

For more information and a copy of our report, please visit: <https://www.fws.gov/project/sea-otter-feasibility-assessment>

The mission of the U.S. Fish and Wildlife Service is working with others to conserve, protect, and enhance fish, wildlife, plants, and their habitats for the continuing benefit of the American people. For more information on our work and the people who make it happen, visit [www.fws.gov](http://www.fws.gov). Connect with our Facebook page at [www.facebook.com/usfws](http://www.facebook.com/usfws), follow our tweets at [www.twitter.com/usfwshq](http://www.twitter.com/usfwshq), our YouTube Channel at <http://www.youtube.com/usfws> and download photos from our Flickr page at <http://www.flickr.com/photos/usfwshq>

-FWS-

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**April Sachanowski**  
**Port of Brookings Harbor**  
**16330 Lower Harbor Road**  
**PO Box 848**  
**Brookings, OR 97415**  
**(541) 469-2218**  
**Fax: (541) 359-3999**